



*City of*  
**Whittlesea**

# **ATTACHMENTS**

## **OF ORDINARY COUNCIL MEETING**

**HELD ON**

**TUESDAY 7 FEBRUARY 2017**

**AT 6.30PM**



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# PLANNING APPLICATION NO. 715935

Aerials: Apr 2016



Subject Land



Medium Density Housing



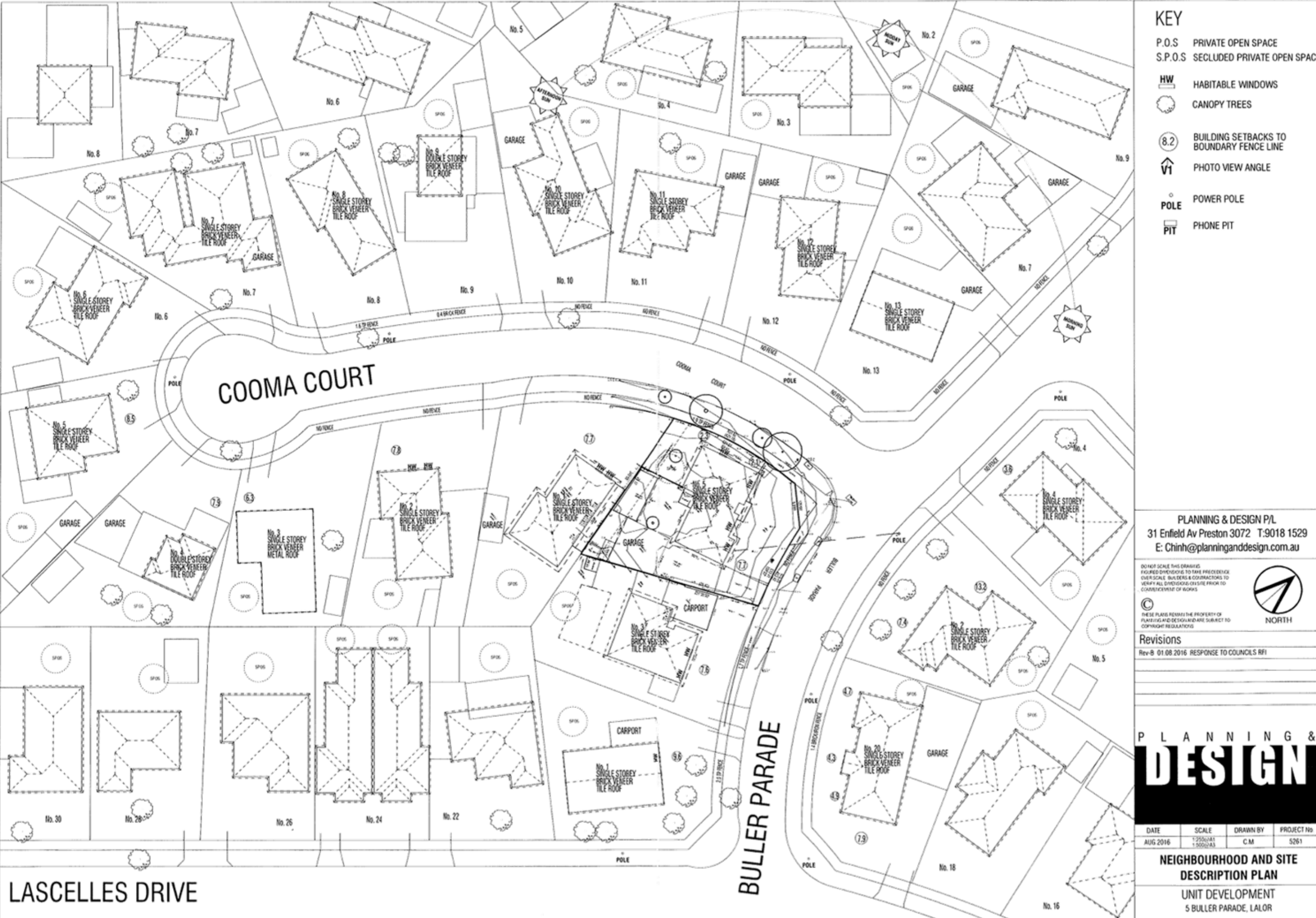
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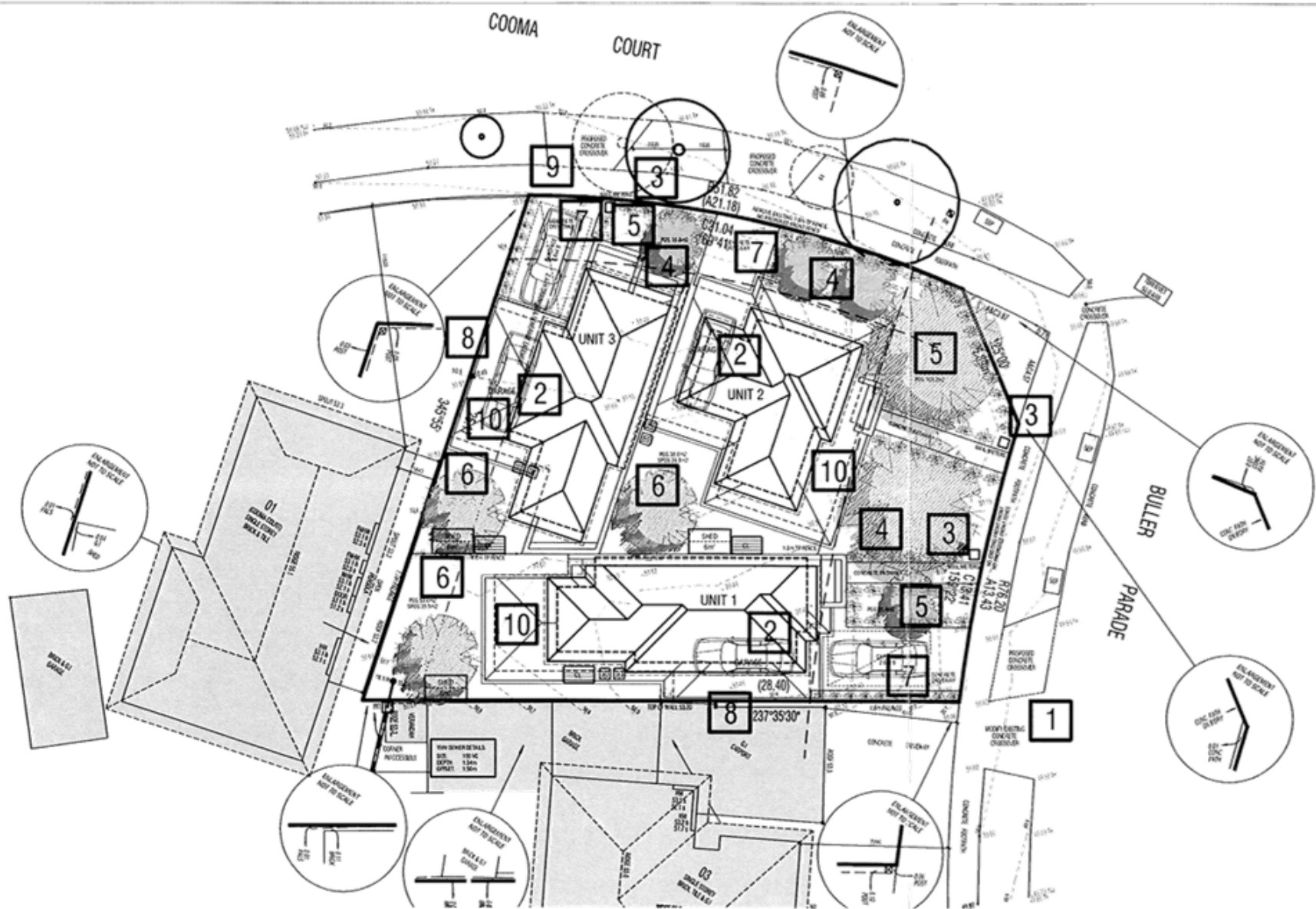
City of  
Whittlesea

## DEVELOPMENT ASSESSMENT REPORT









DESIGN RESPONSE

- 1 EXISTING CROSSOVER TO BE MODIFIED TO SERVICE UNIT 1.
- 2 PROPOSED VEHICLE STORAGE FOR ALL UNITS ARE LOCATED BEHIND THE FRONT DWELLING TO HIDE THE DOMINANCE OF CAR PARKING STRUCTURES FROM THE STREET-SCAPE.
- 3 NEW MAILBOXES FOR ALL UNITS.
- 4 PROPOSED FRONT STREET SETBACK TO DEVELOPMENT IS SYMPATHETIC TO THE ADJOINING NEIGHBOURS.
- 5 LARGE LAWN AREA PROVIDES SPACE FOR LANDSCAPING.
- 6 OPEN SPACES TO UNITS ARE ORIENTED TOWARDS NORTH PROVIDING FUTURE RESIDENCES WITH EXCELLENT SOLAR ACCESS AND NORTH LIGHT. OPEN SPACE ON SITE FOR EACH DWELLING IS DISTRIBUTED TO THE REAR AND THROUGHOUT THE SITE. THE DEVELOPMENT WILL PROVIDE SUFFICIENT PRIVATE OPEN SPACE FOR THE REASONABLE RECREATION, SERVICE AND STORAGE NEEDS OF RESIDENTS. THE PRIVATE OPEN SPACES FOR ALL DWELLINGS ARE LOCATED OFF LIVING AREAS.
- 7 LANDSCAPING LOCATION TO PROVIDE SOFT BUFFER AND SCREENING BETWEEN THE ADJOINING PROPERTIES.
- 8 ANY PROPOSED WALLS TO BE BUILT TO THE BOUNDARY ARE LOCATED AWAY FROM EXISTING HABITABLE ROOM WINDOWS AND OPEN SPACES.
- 9 PROPOSED CROSSOVER ALONG COOMA COURT TO PROVIDE ACCESS TO UNIT 2 AND 3.
- 10 FIRST FLOOR LEVEL OF DWELLINGS SETBACK FROM SIDE BOUNDARIES TO REDUCE OVERSHADOWING AND VISUAL BULK TO ADJOINING PROPERTIES. UPPER LEVEL FOOTPRINT IS OFFSET WITHIN THE GROUND FLOOR ENVELOPE TO REDUCE VISUAL BULK AND CREATE A MORE GRADUAL TRANSITION BETWEEN THE SINGLE STOREY AND TWO-STOREY BUILDING FORM.



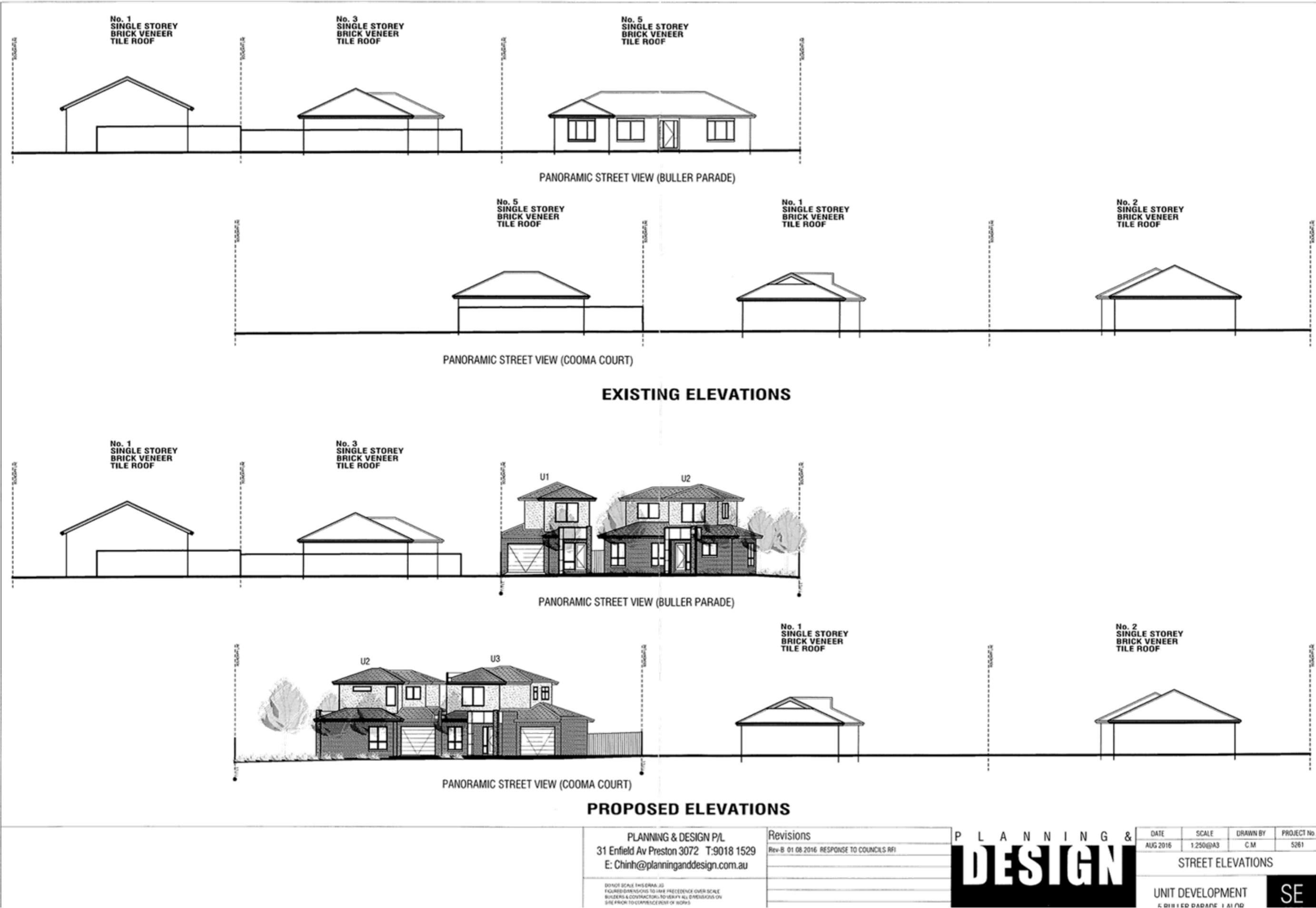
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31 Enfield Av Preston 3072 T:9018 1529  
E: Chinh@planninganddesign.com.au

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BUILDERS & CONTRACTORS TO VERIFY ALL DIMENSIONS ON  
SITE PRIOR TO COMMENCEMENT OF WORKS

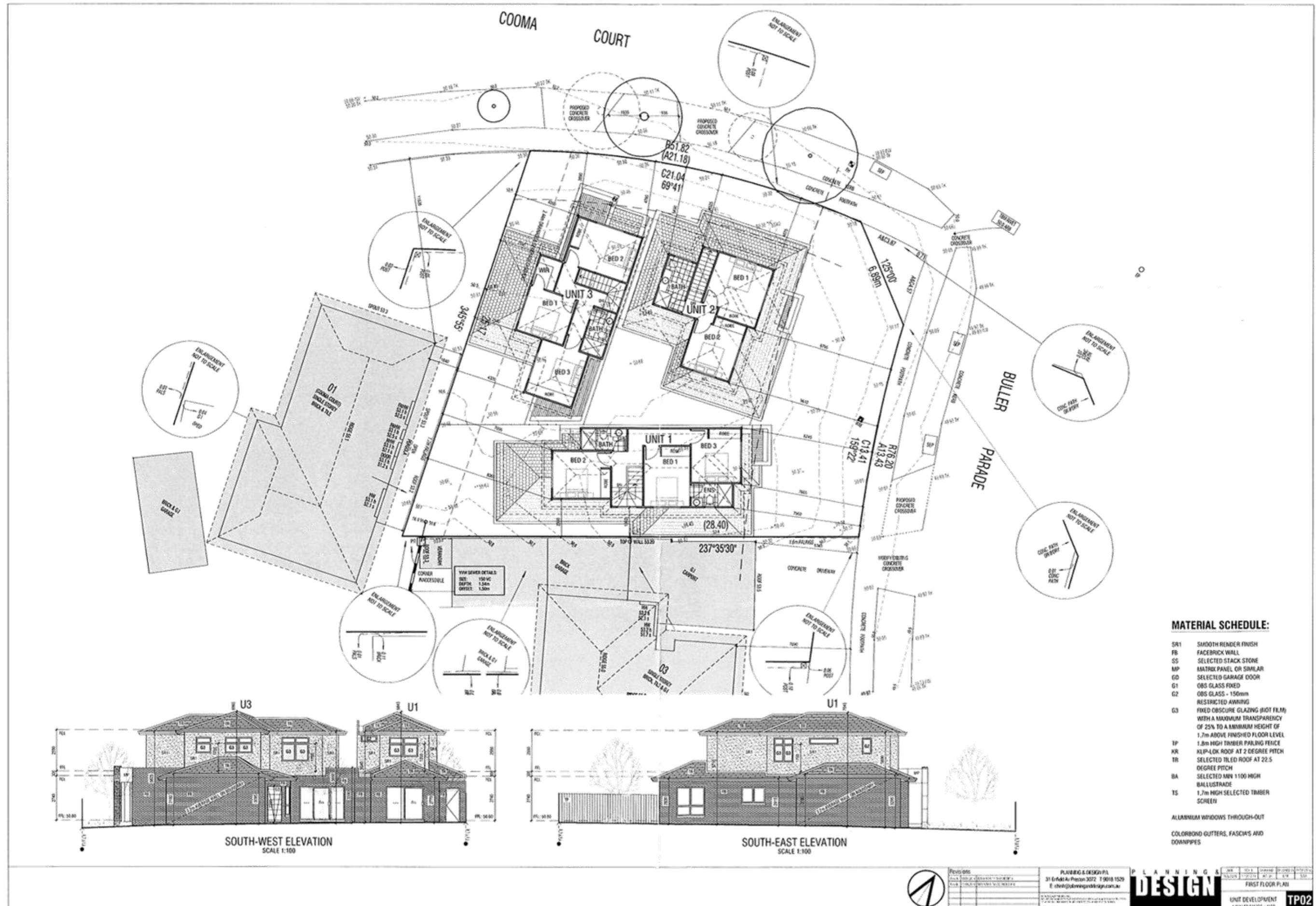
Revisions
Rev-B 01 08 2016 RESPONSE TO COUNCIL'S RFI

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**DESIGN**

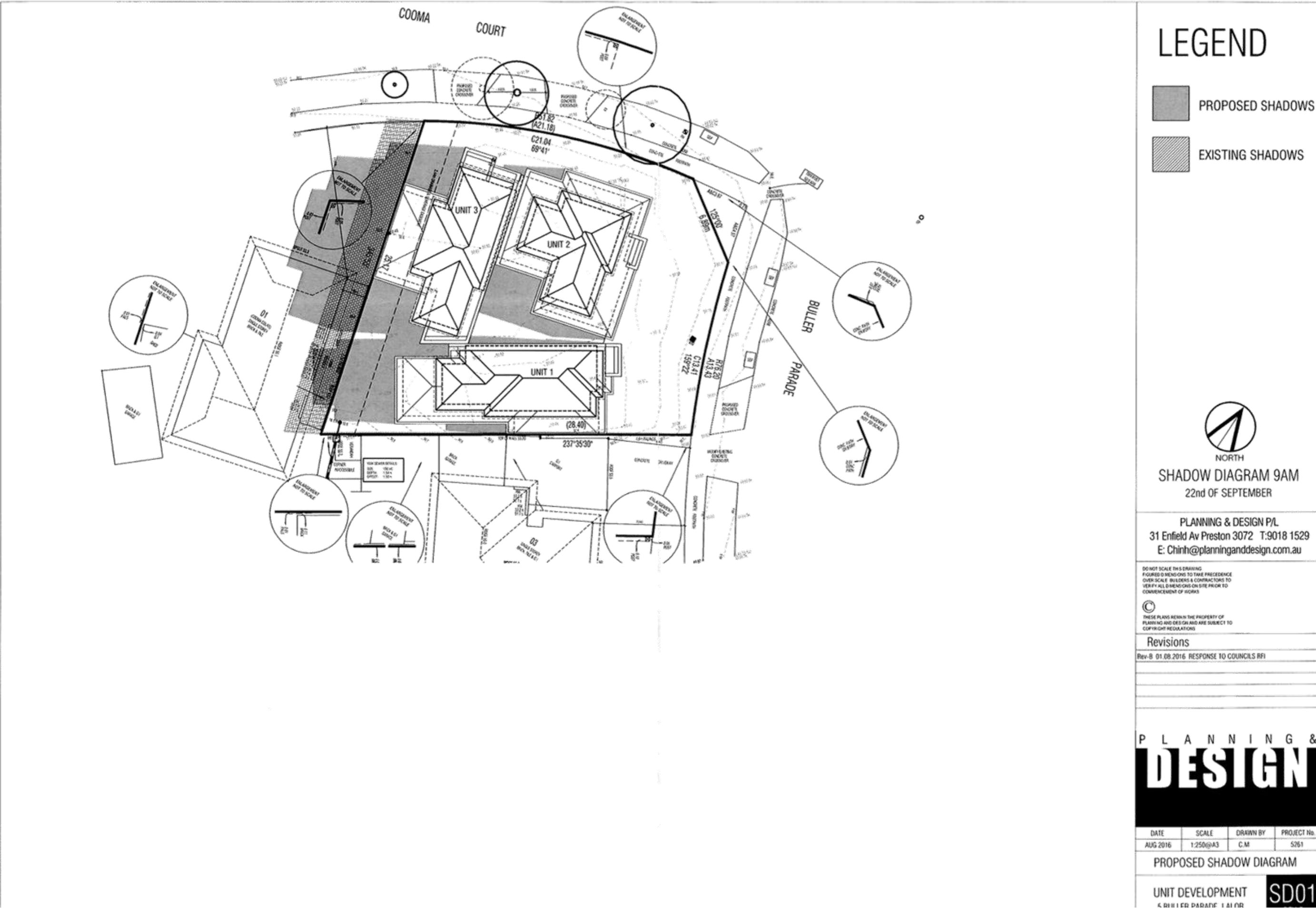
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UNIT DEVELOPMENT			DR

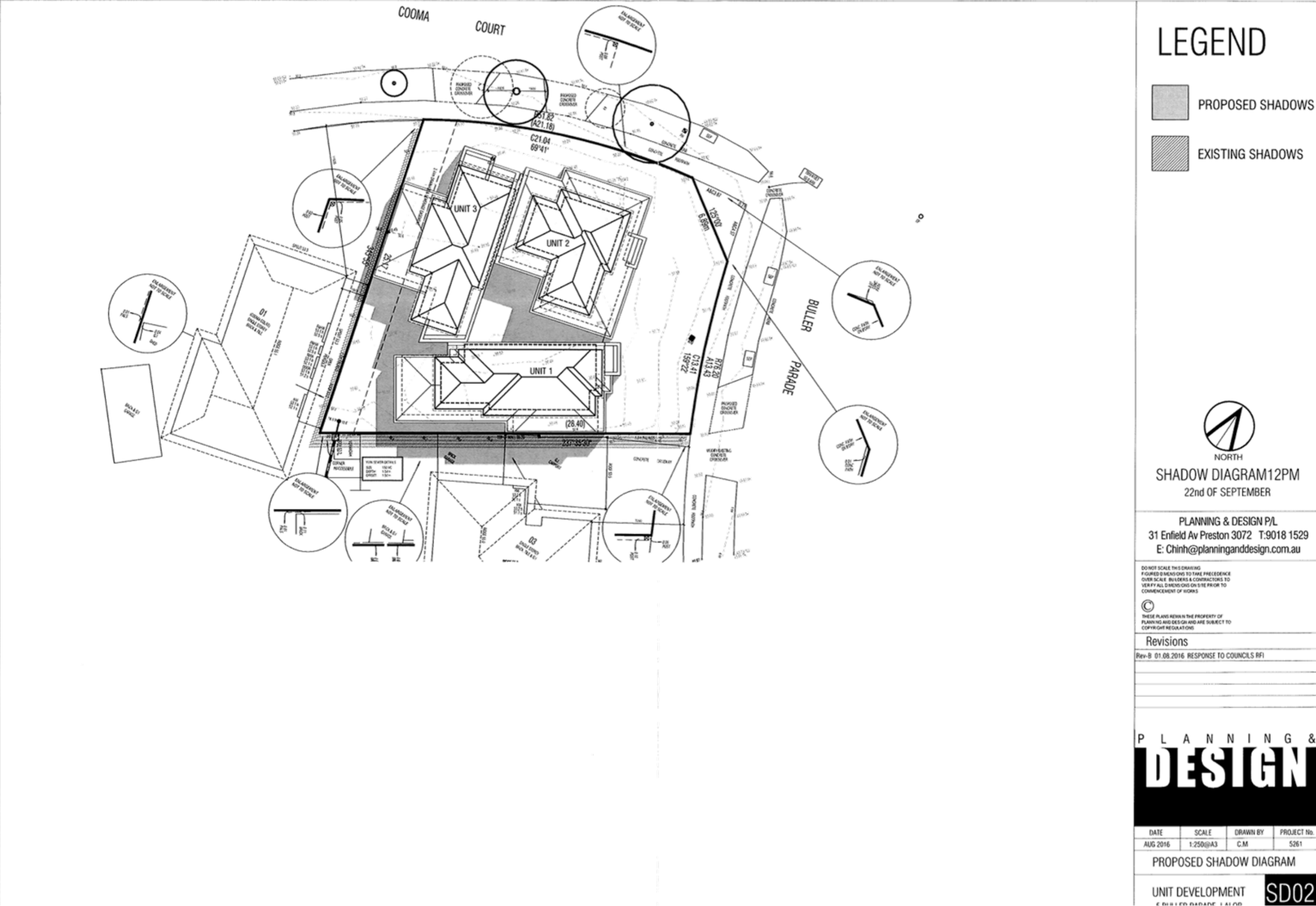


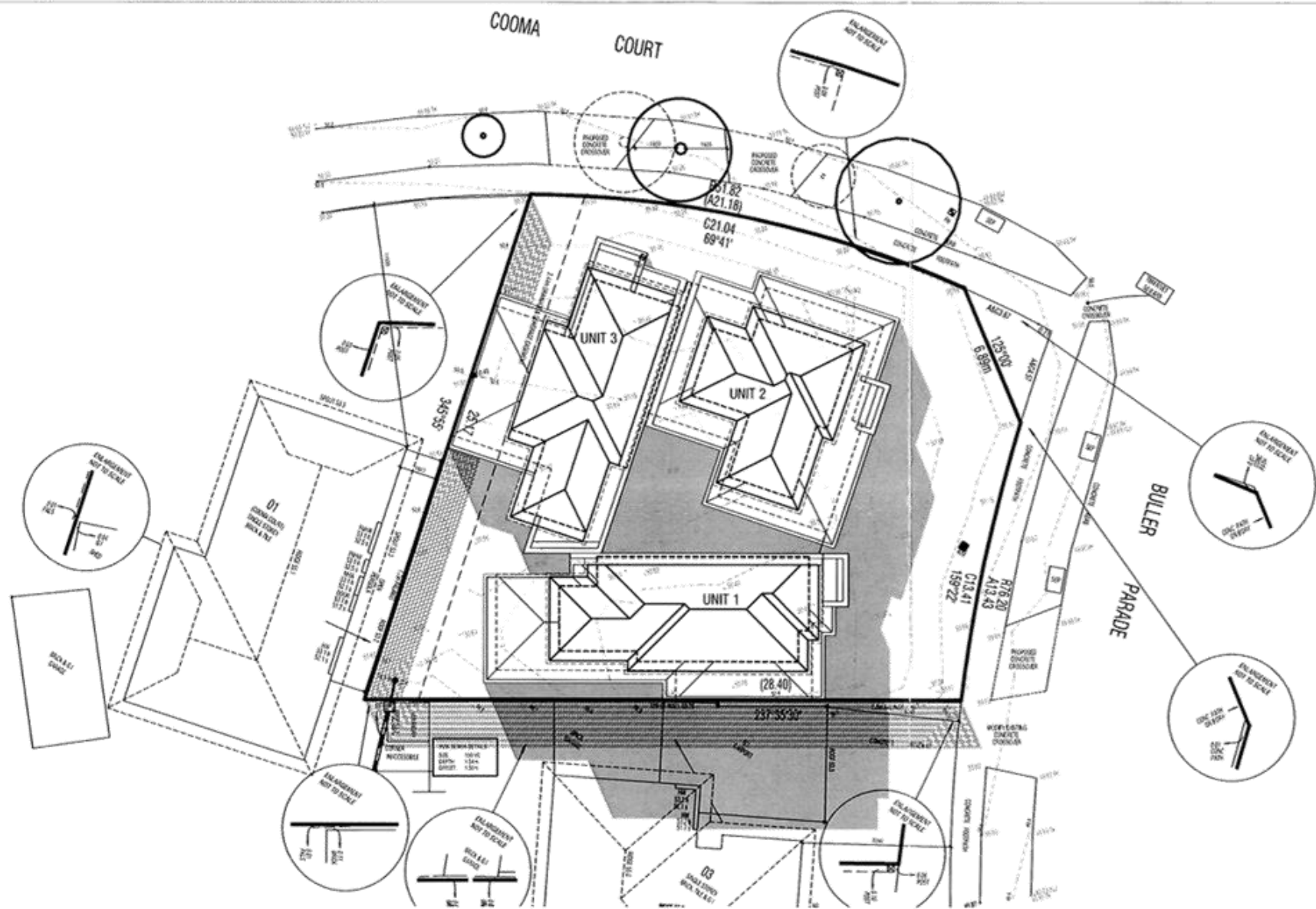












LEGEND

- PROPOSED SHADOWS
- EXISTING SHADOWS



SHADOW DIAGRAM 3PM  
22nd OF SEPTEMBER

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Revisions

Rev-B 01.08.2016 RESPONSE TO COUNCIL'S RF1

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**DESIGN**

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PROPOSED SHADOW DIAGRAM

UNIT DEVELOPMENT

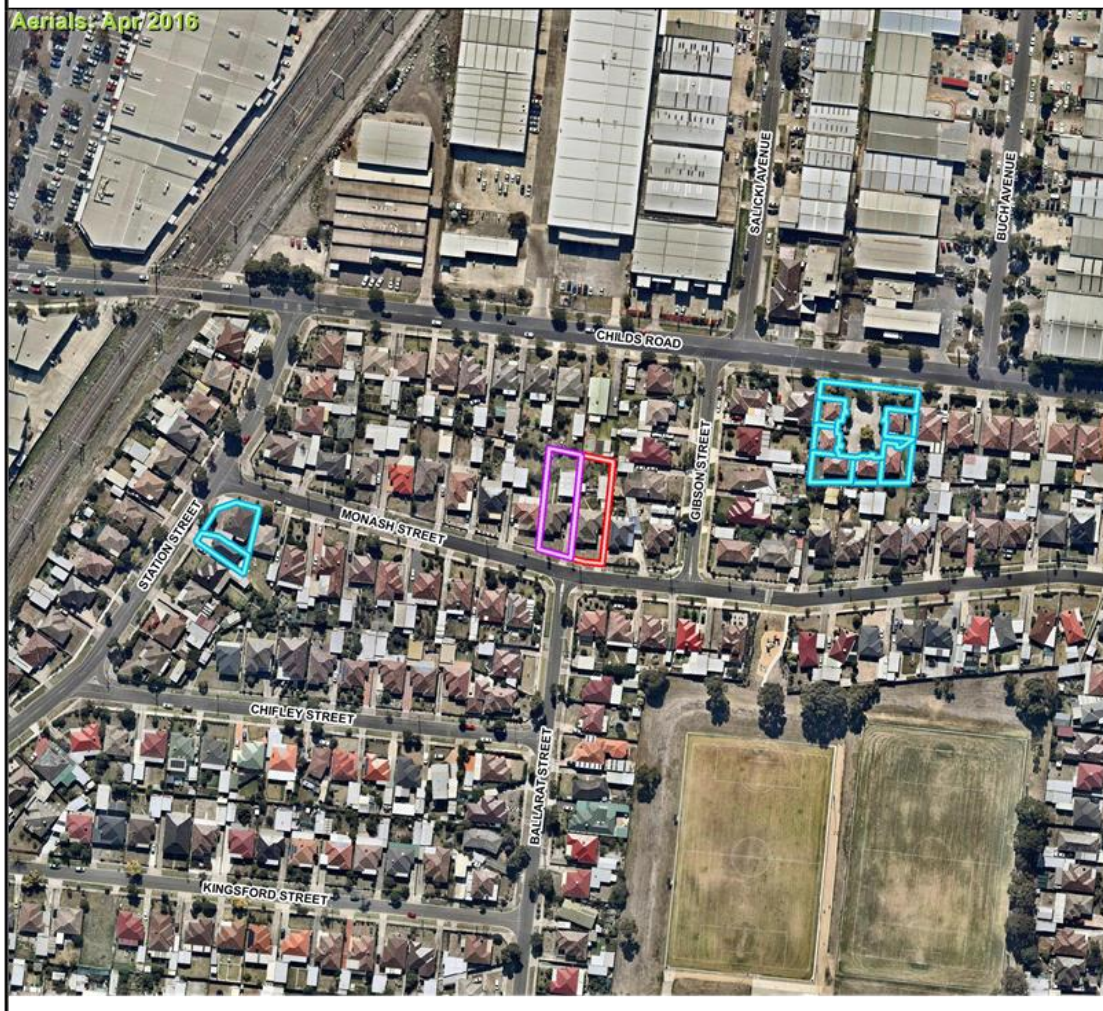
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# PLANNING APPLICATION NO. 716225

Aerials: Apr/2016



Subject Land



Medium Density Housing

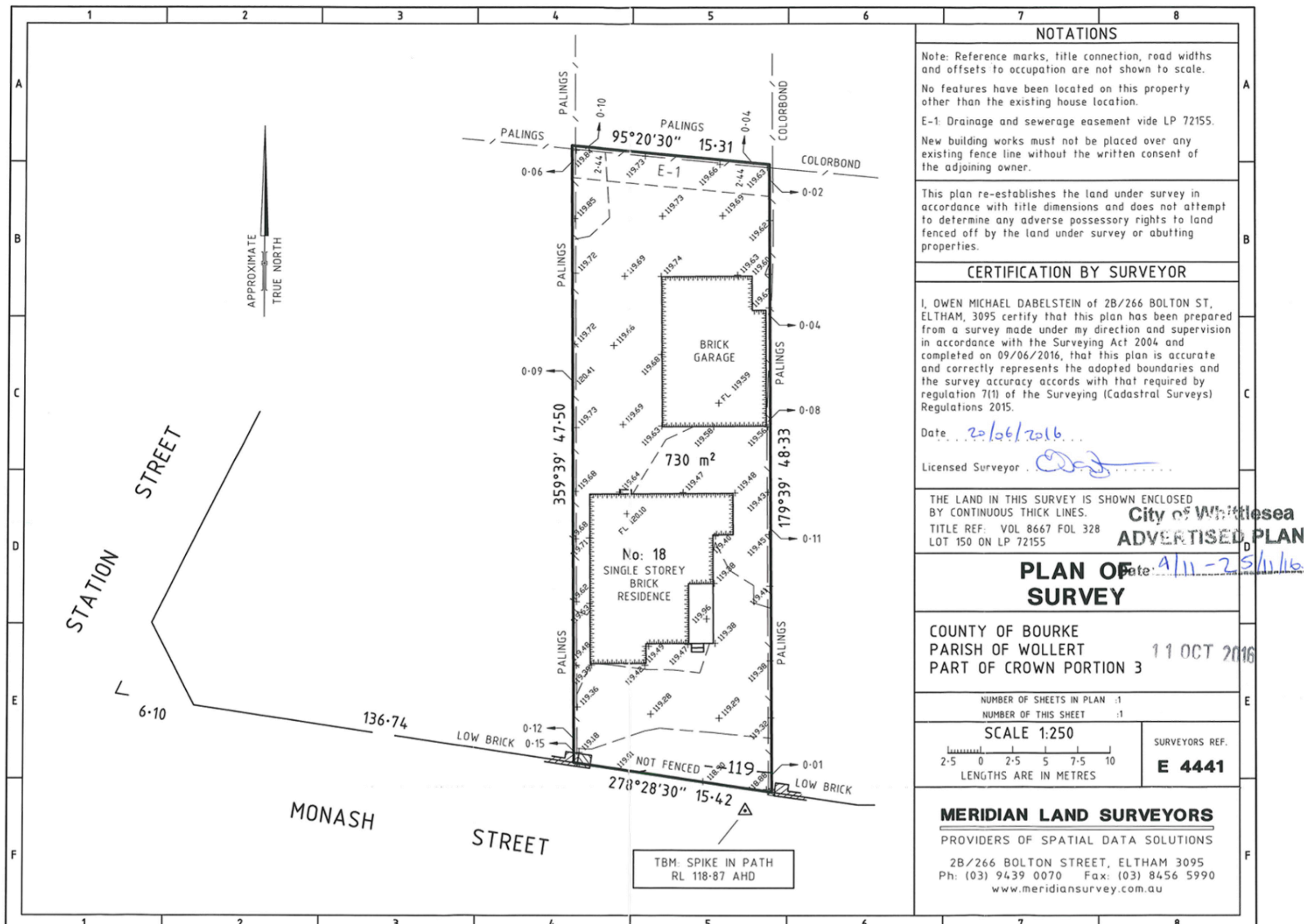


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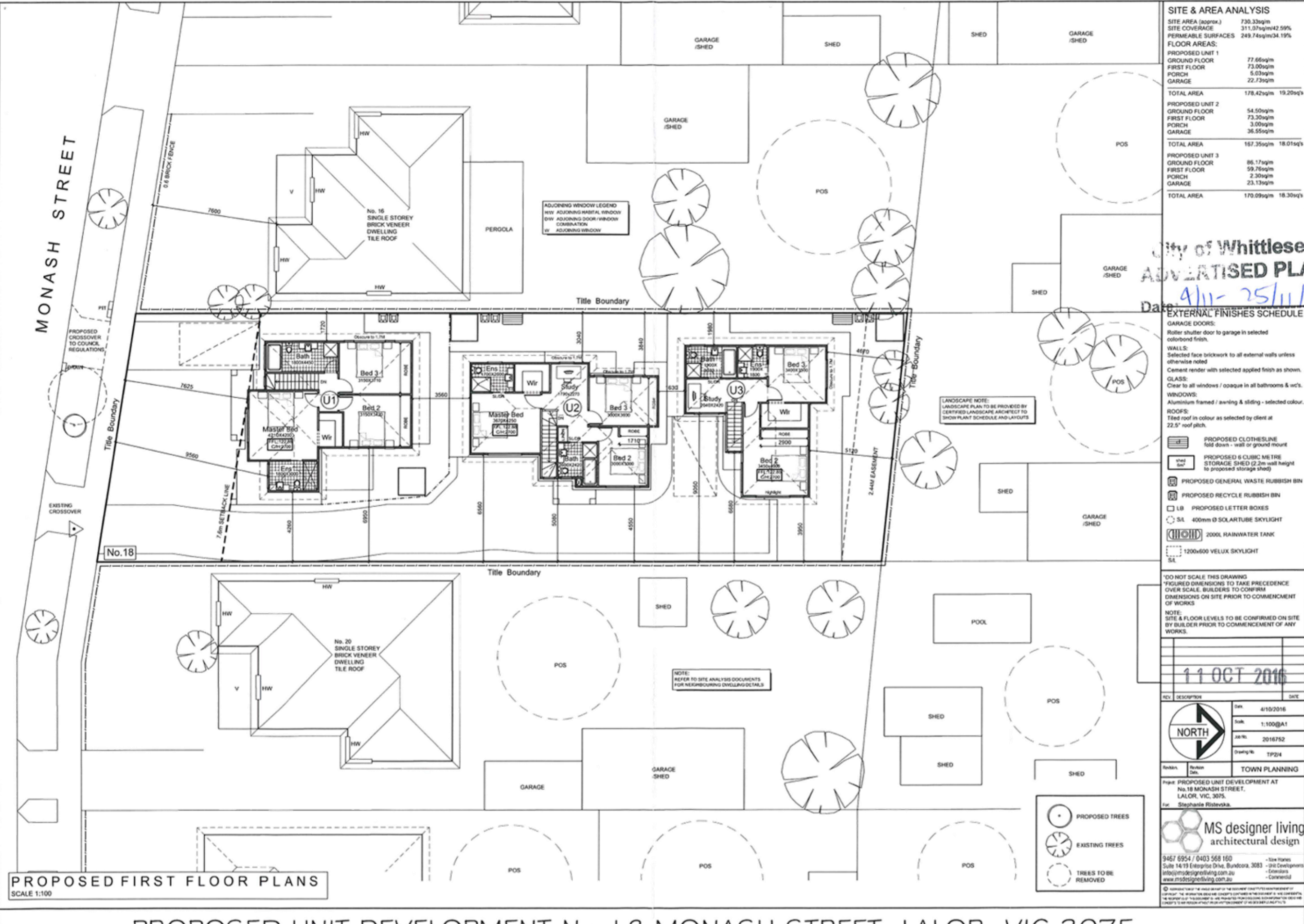


City of  
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## DEVELOPMENT ASSESSMENT REPORT





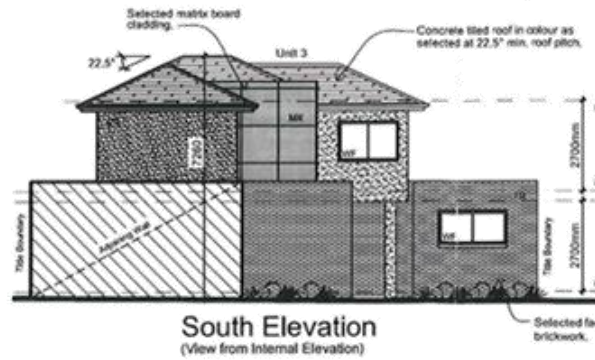
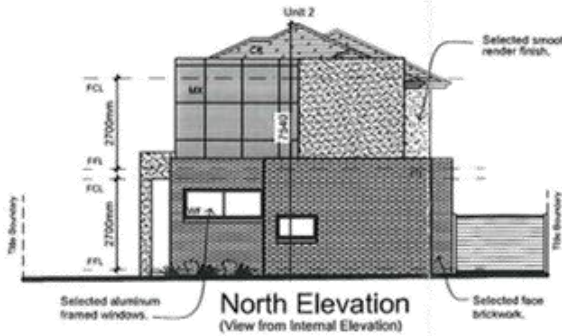
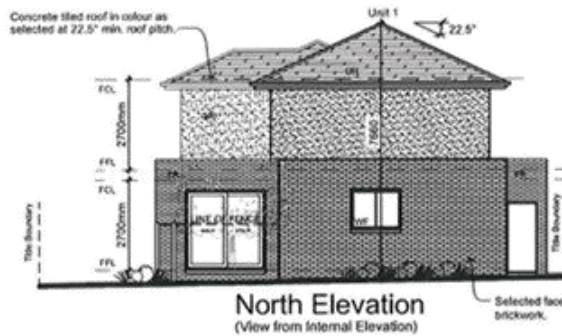
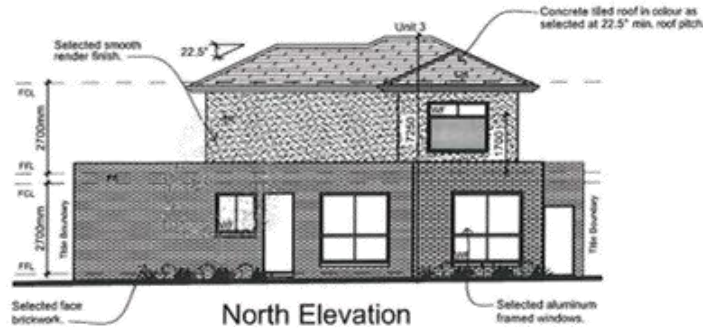
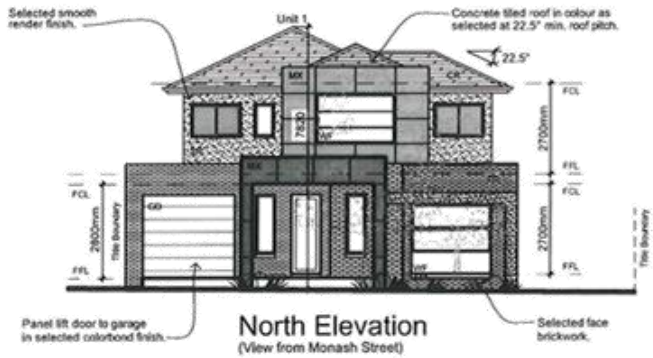








EXTERNAL COLOUR AND MATERIAL SCHEDULE	
SR	SMOOTH RENDER FINISH
WF	ALL ALUMINIUM FRAMED WINDOWS
CG	ALL COLORBOND GUTTERS
CF	ALL COLORBOND FASCIA
CR	ROOF TILES AT 22.5° PITCH
GD	ALL GARAGE DOORS
FB	FACE BRICKWORK
CONC	CONCRETE PAVED DRIVEWAY
MX	MATRIX CLADDING

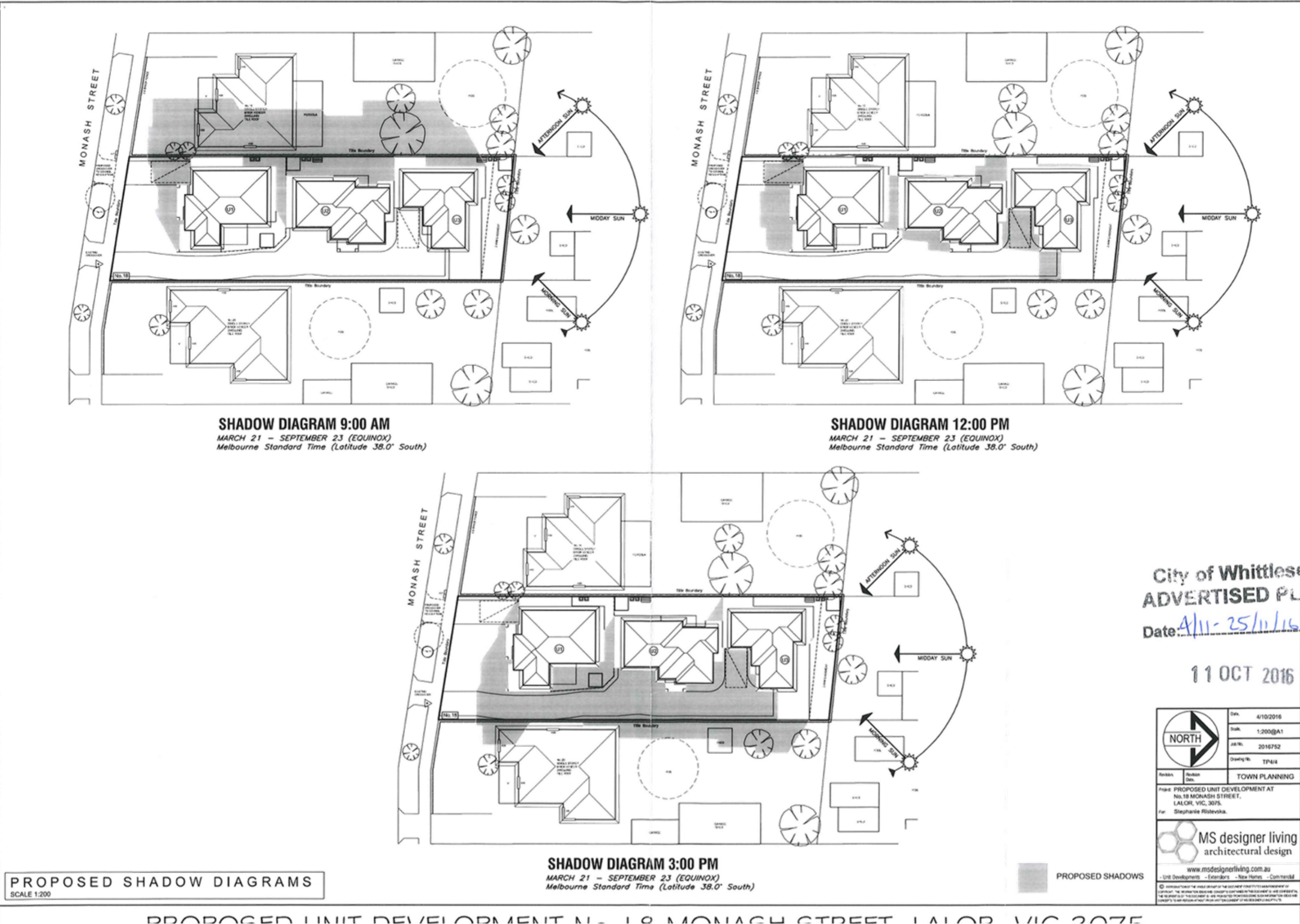


PROPOSED ELEVATIONS  
SCALE 1:100

City of Whittlesea  
ADVERTISED PLAN  
Date: 9/11/16 - 25/11/16

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NOTE: SITE & FLOOR LEVELS TO BE CONFIRMED ON SITE BY BUILDER PRIOR TO COMMENCEMENT OF ANY WORKS.	
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# PLANNING APPLICATION NO. 716003

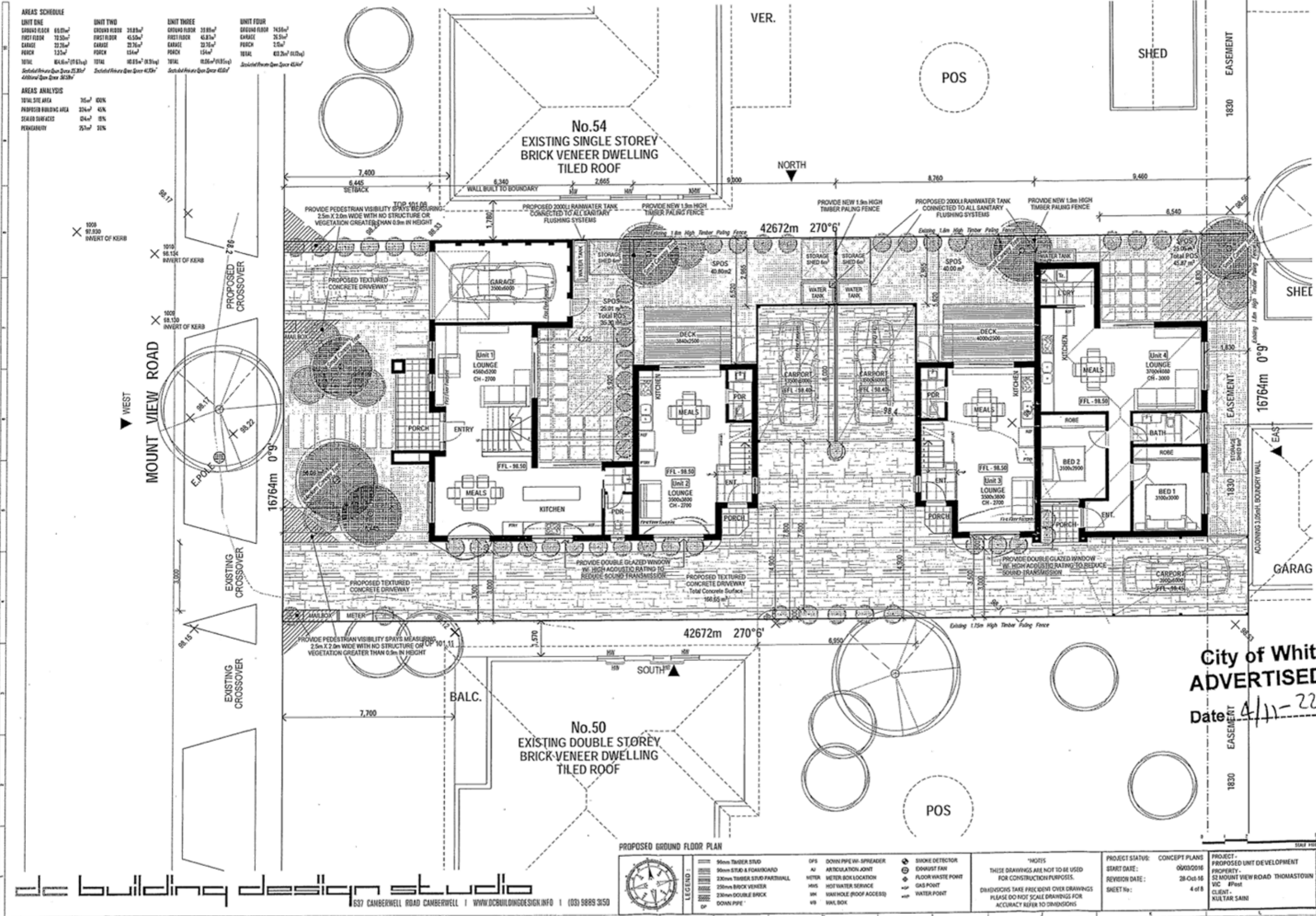


-  Subject Land
-  Medium Density Housing
-  Objector

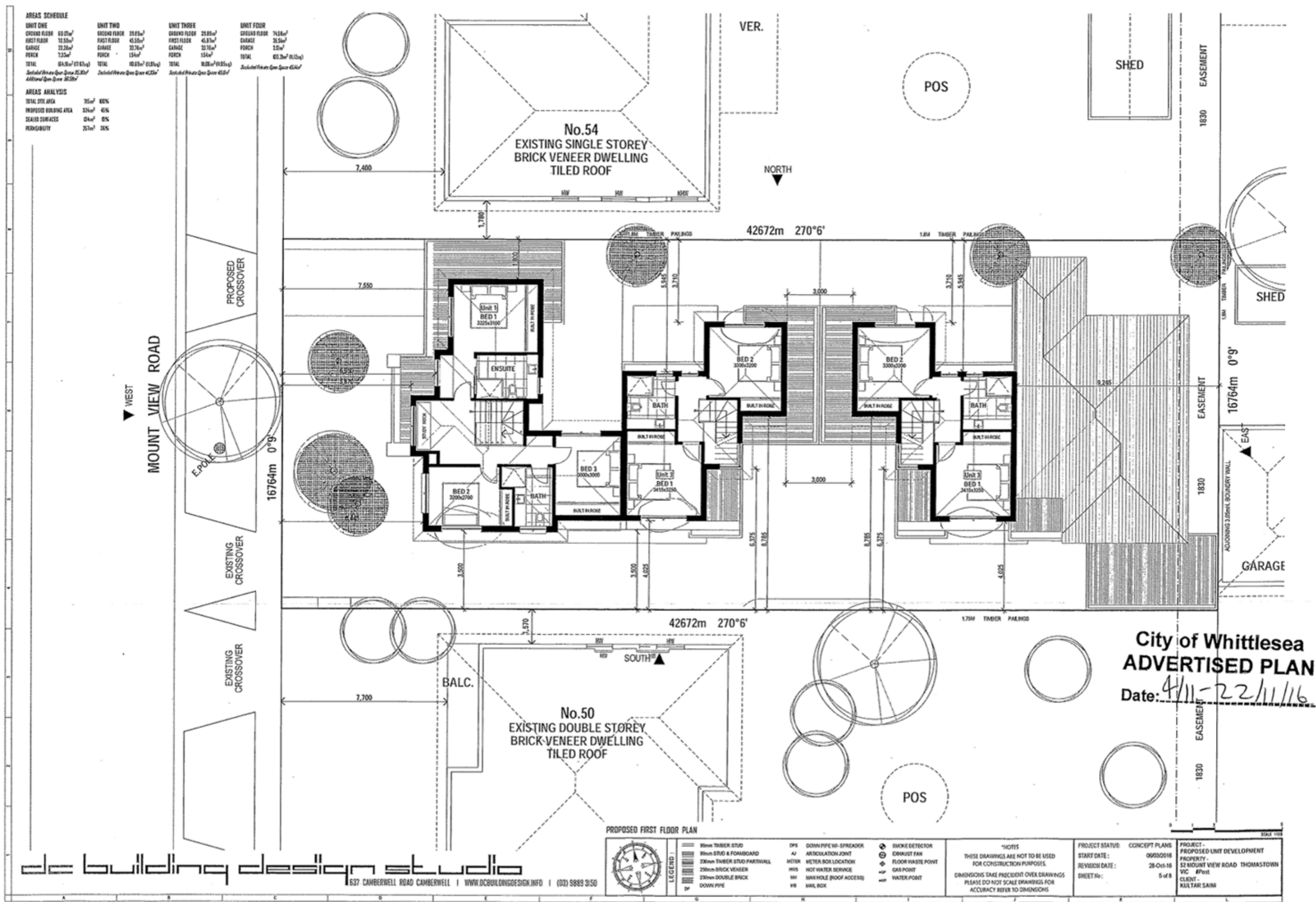


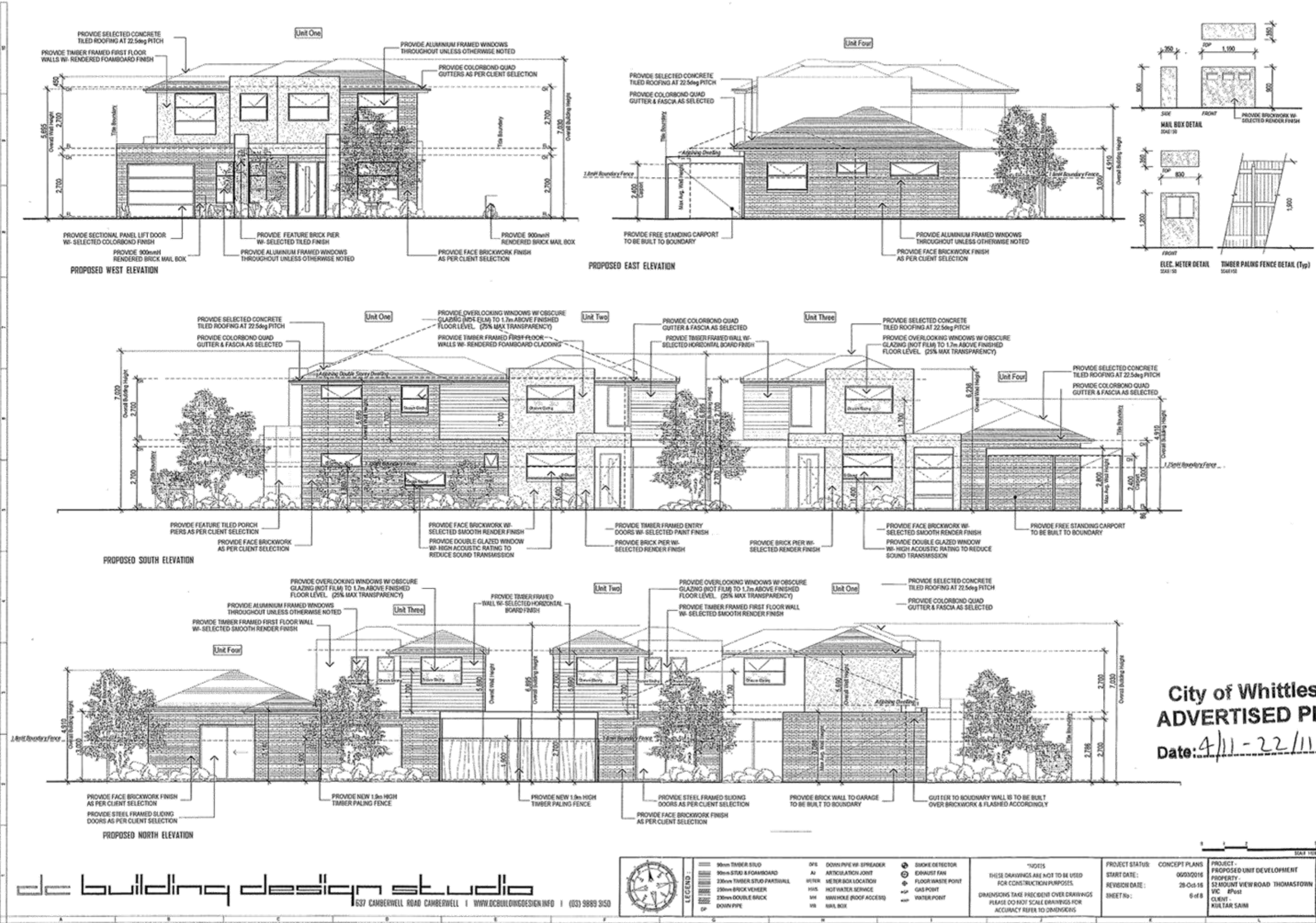
City of  
Whittlesea

## DEVELOPMENT ASSESSMENT REPORT















# PLANNING APPLICATION NO. 715551



-  Subject Land
-  Medium Density Housing
-  Objector

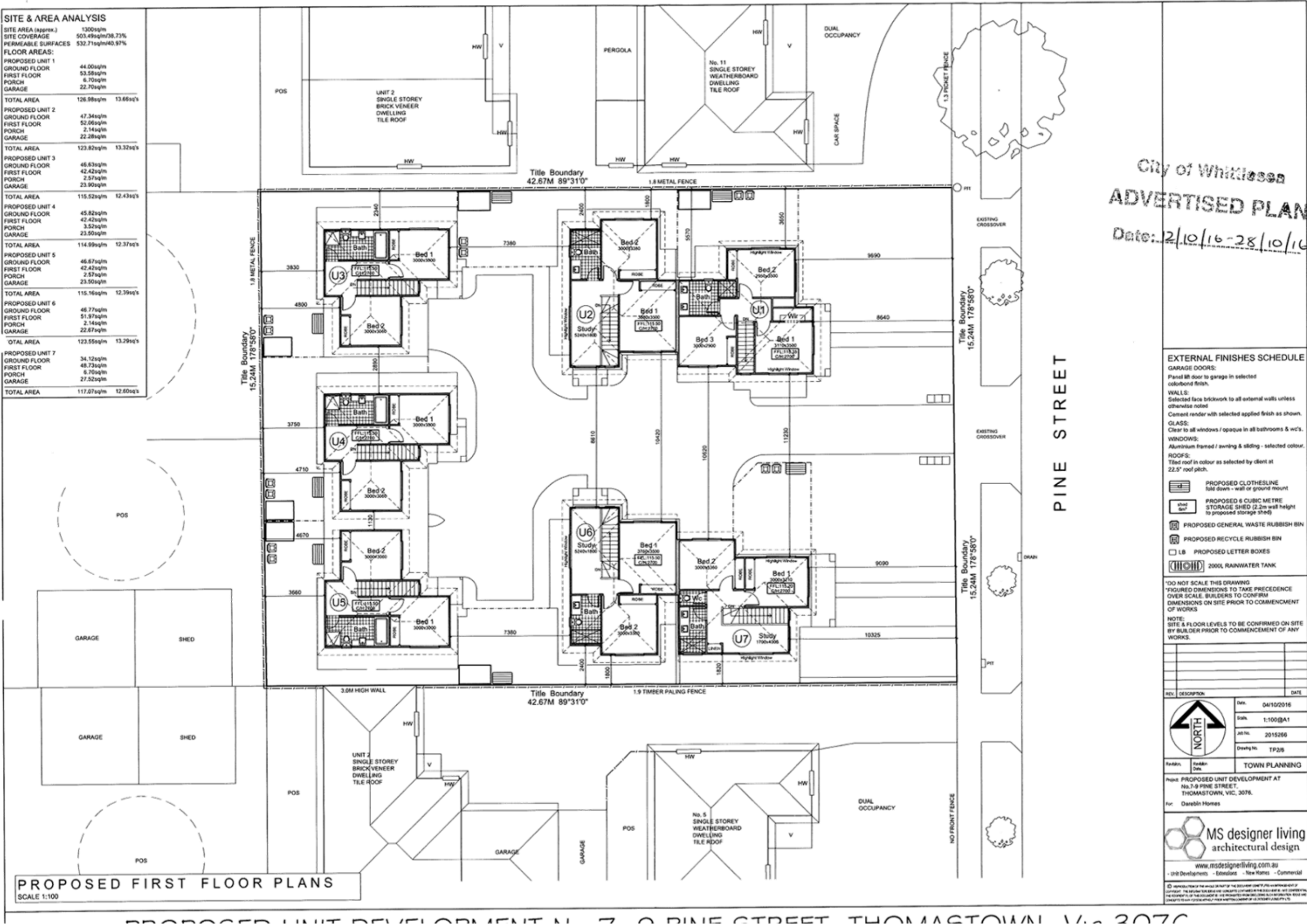


City of  
Whittlesea

## DEVELOPMENT ASSESSMENT REPORT







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CS	CEMENT SHEET BOARDS:
WF	ALL ALUMINIUM FRAMED WINDOWS:
CG	ALL COLORBOND GUTTERS:
CF	ALL COLORBOND FASCIAS:
CR	ROOF TILES AT 22.5° PITCH:
GD	ALL GARAGE DOORS:
FB	FACE BRICKWORK:
CONC	CONCRETE PAVED DRIVEWAY:
AL	ALUCOBOND CLADDING

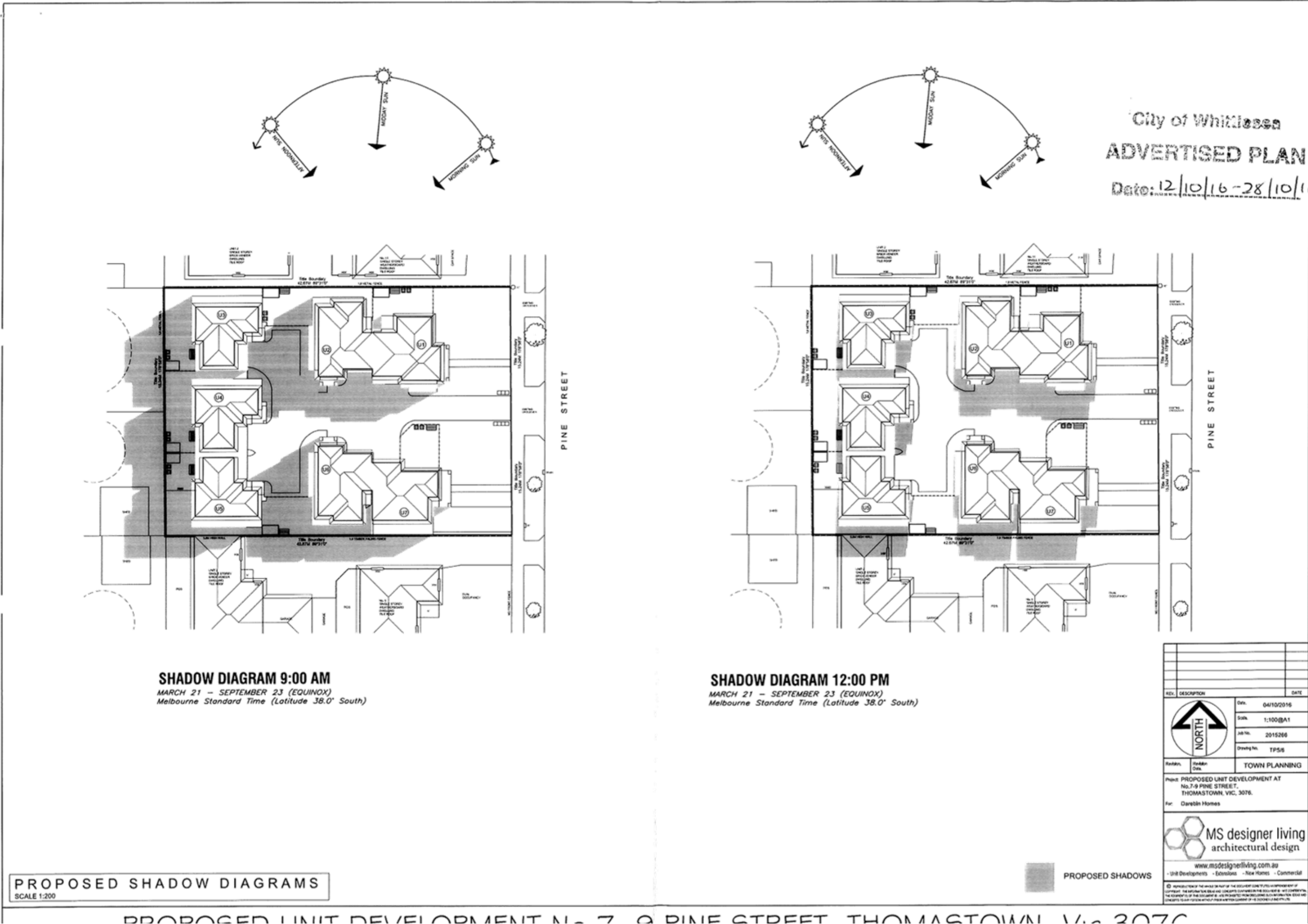


City of Whitesea  
ADVERTISED PLAN  
Date: 12/10/16 - 28/10/16

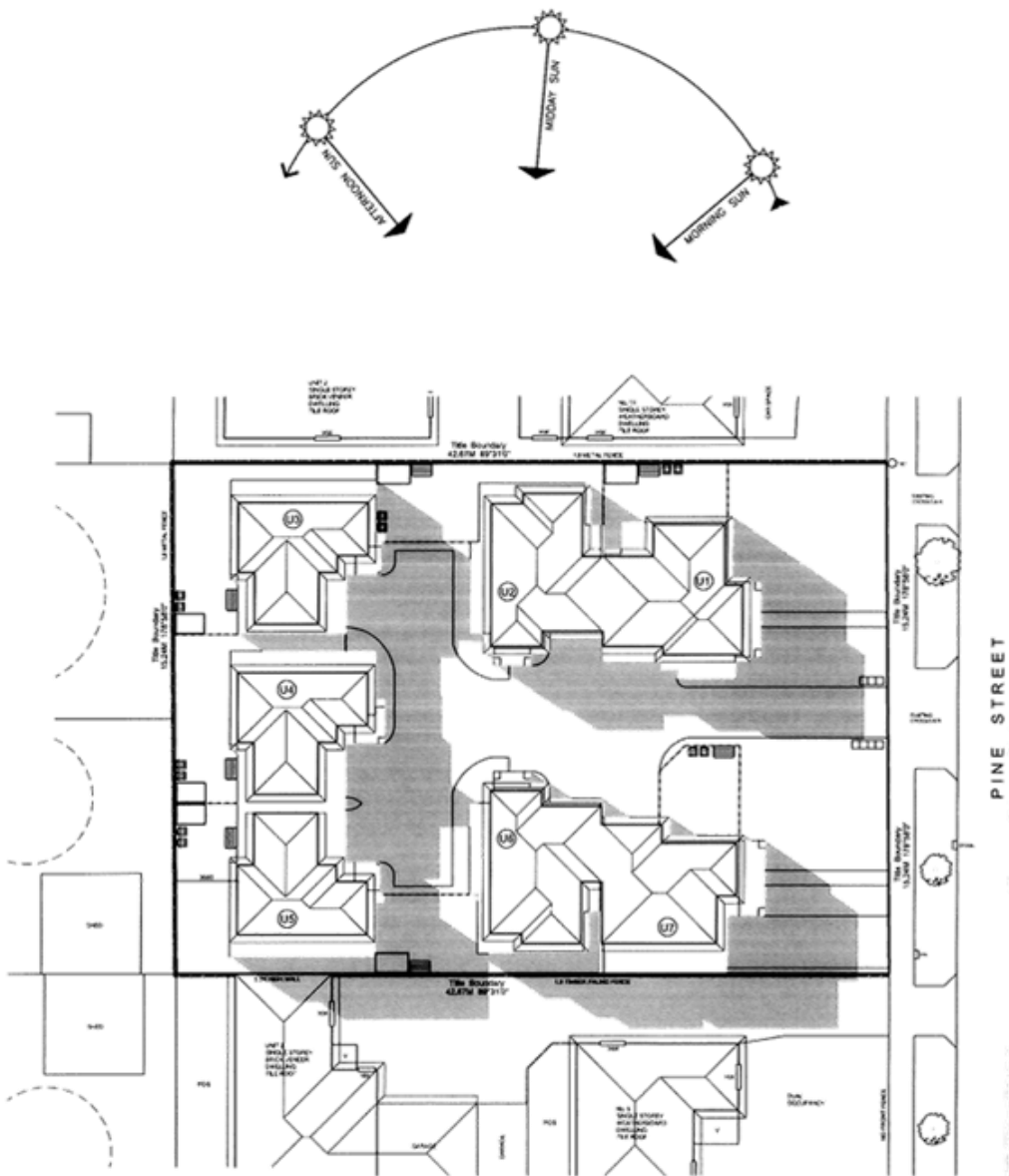
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City of Whittlesea  
ADVERTISED PLAN  
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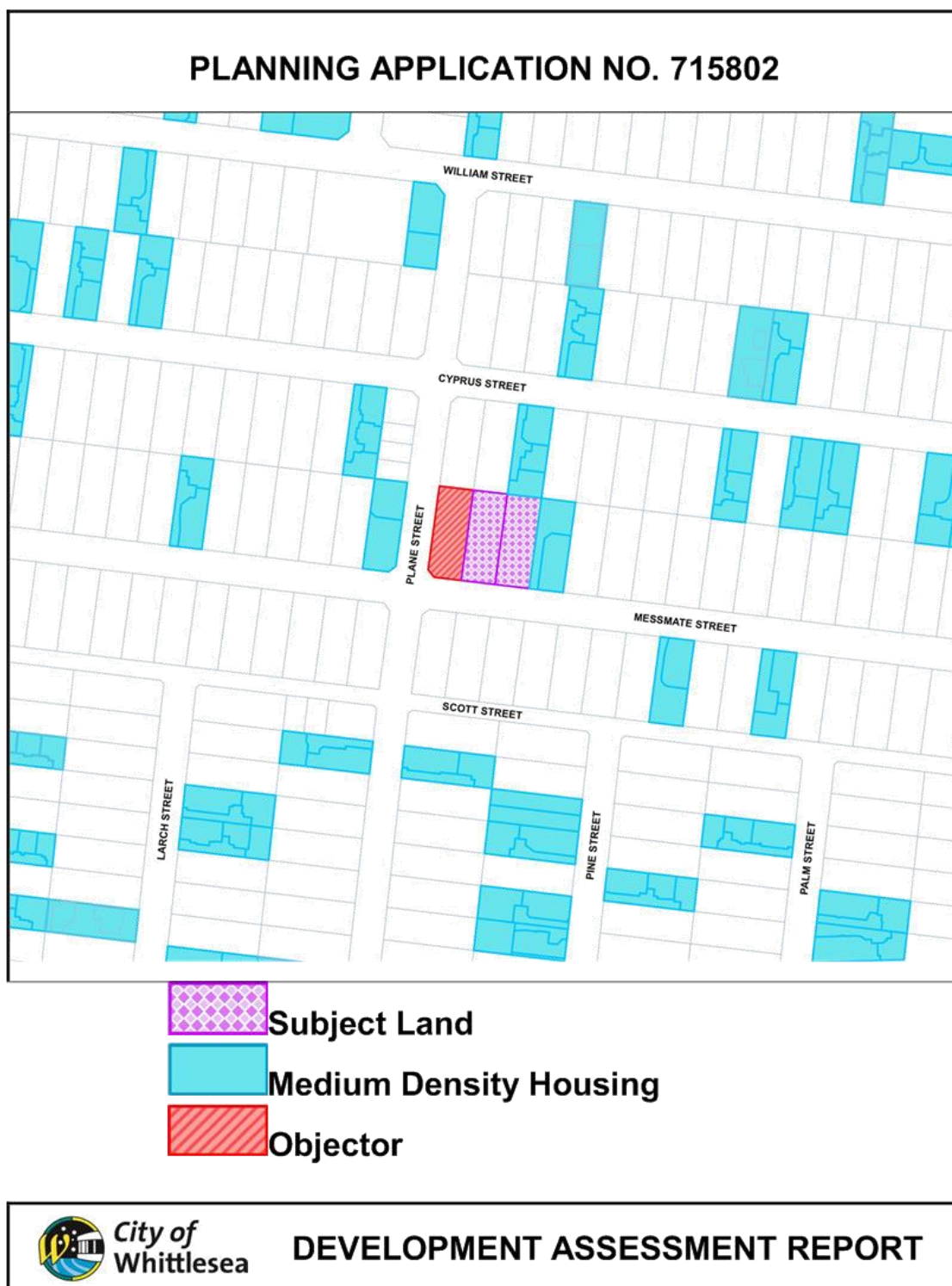
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Melbourne Standard Time (Latitude 38.0° South)

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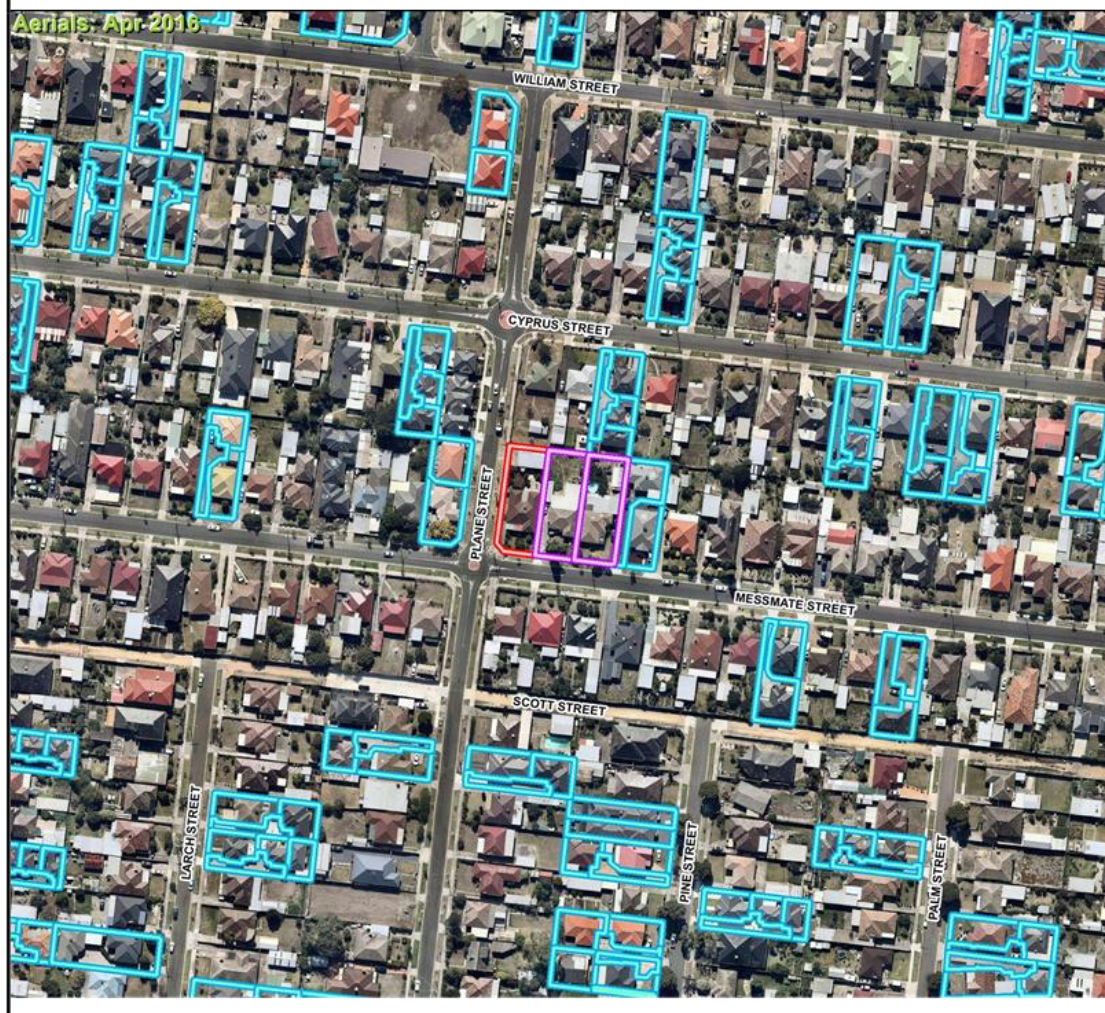


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**aPLANNING APPLICATION NO. 715802**



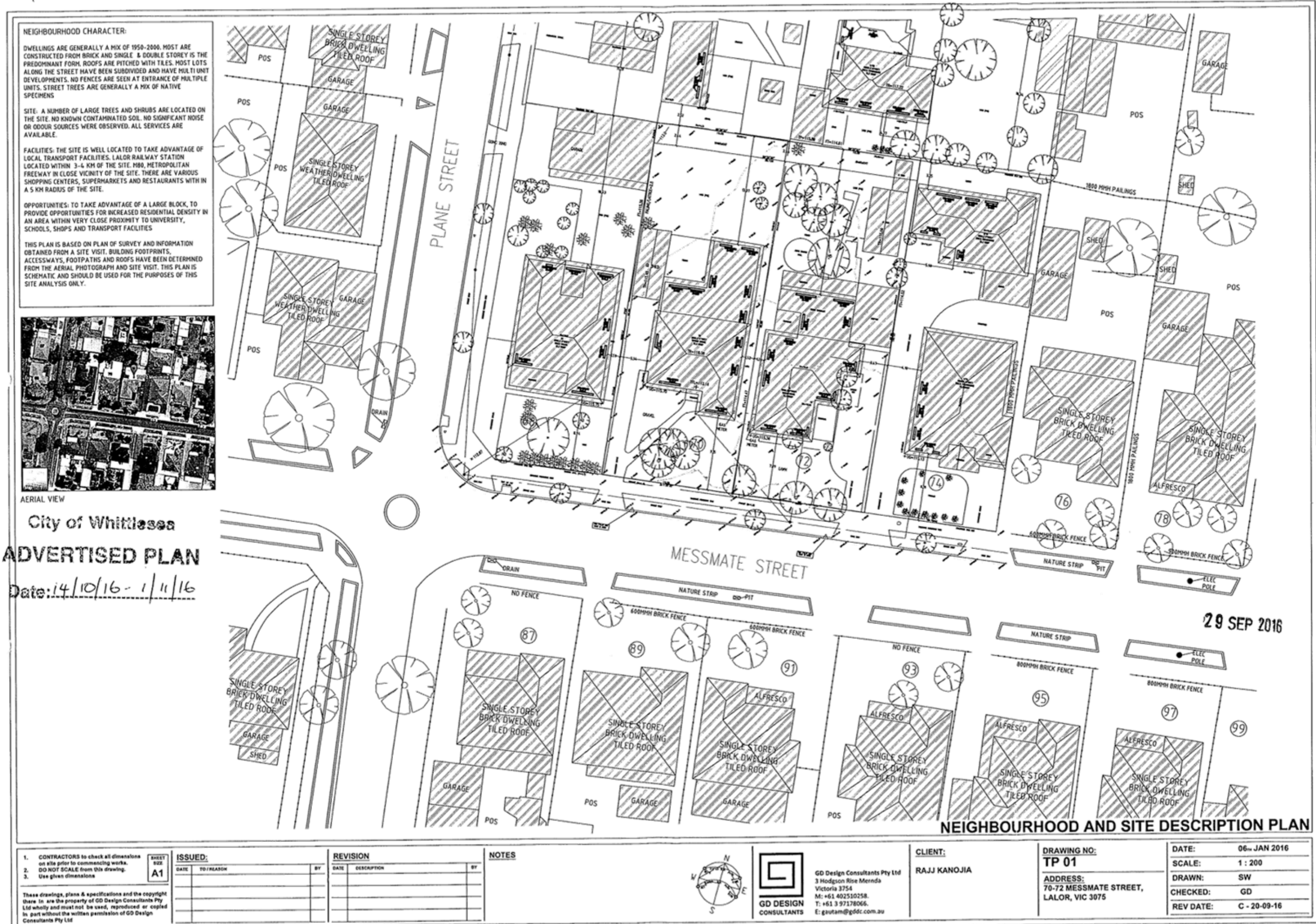
-  Subject Land
-  Medium Density Housing
-  Objector



City of  
Whittlesea

**DEVELOPMENT ASSESSMENT REPORT**





1. CONTRACTORS to check all dimensions on site prior to commencing work.

2. DO NOT SCALE from this drawing.

3. Use given dimensions

ISSUED:

DATE	TO / REASON	BY

REVISION

DATE	DESCRIPTION	BY

NOTES

GD DESIGN CONSULTANTS

GD Design Consultants Pty Ltd  
3 Hodgson Rise Mernda  
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M: +61 402510258  
T: +61 3 97178066  
E: gautam@gddc.com.au

CLIENT:

RAJJ KANOJIA

DRAWING NO:

TP 01

ADDRESS:

70-72 MESSMATE STREET,  
LALOR, VIC 3075

DATE:

06 JAN 2016

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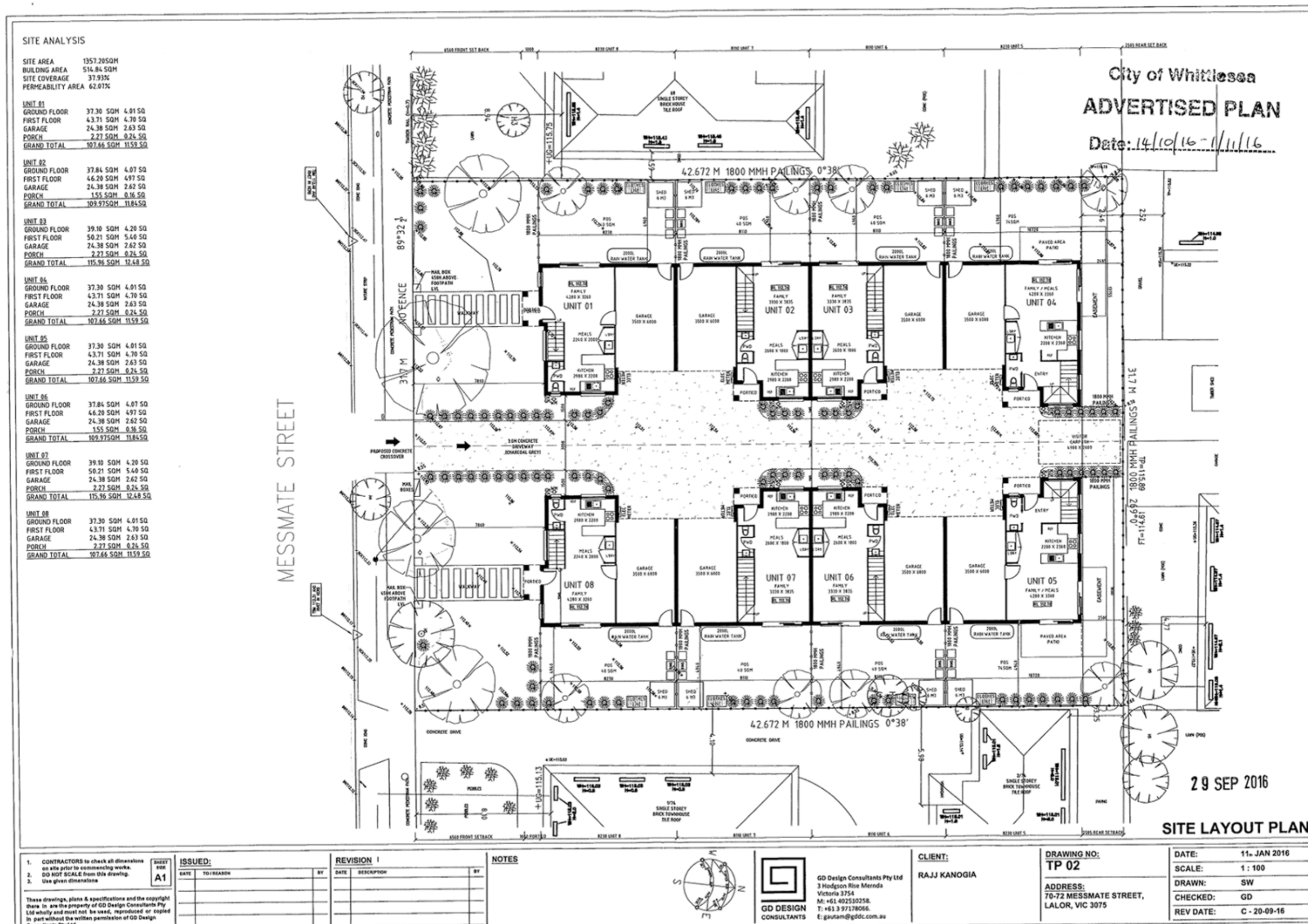
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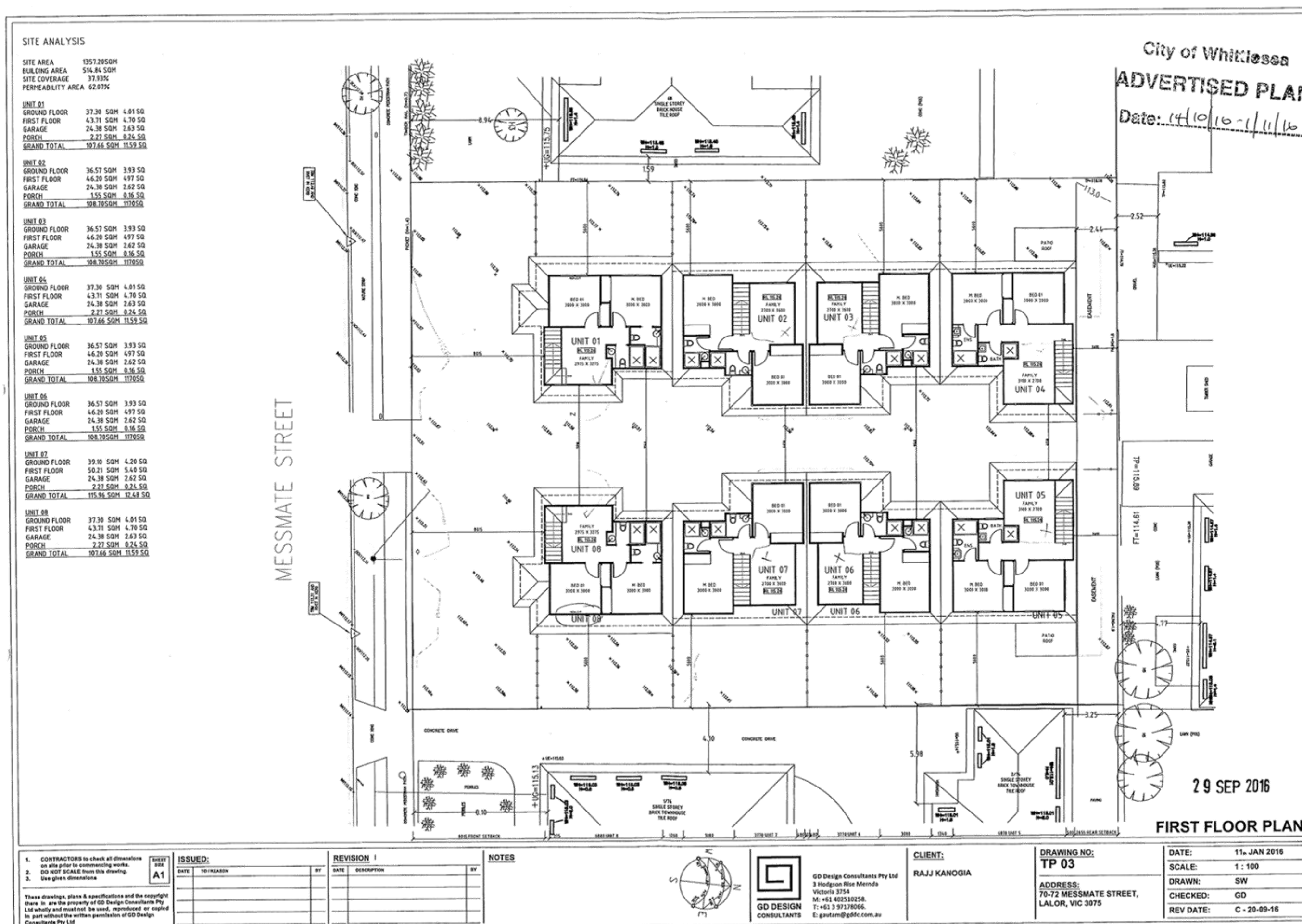
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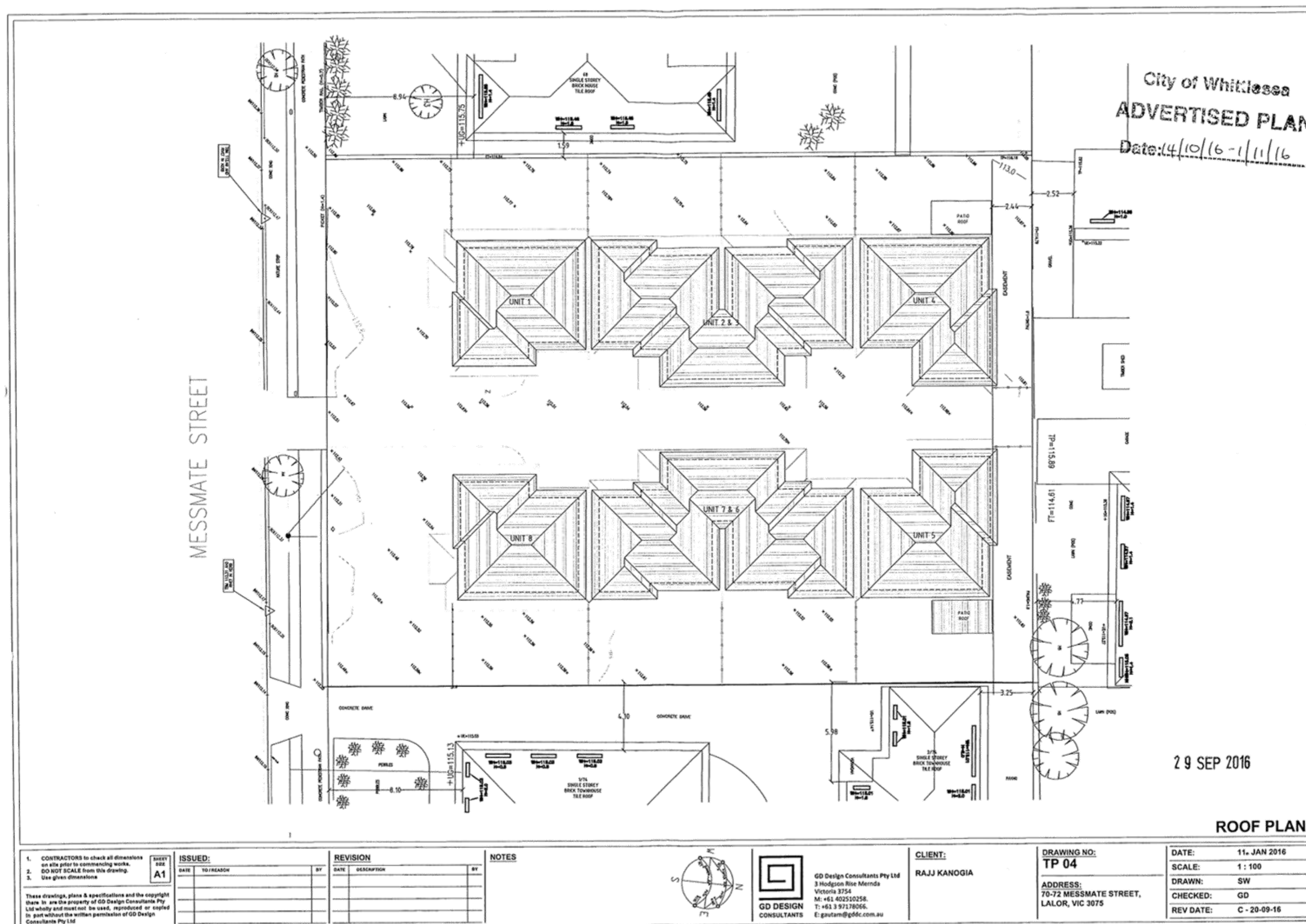
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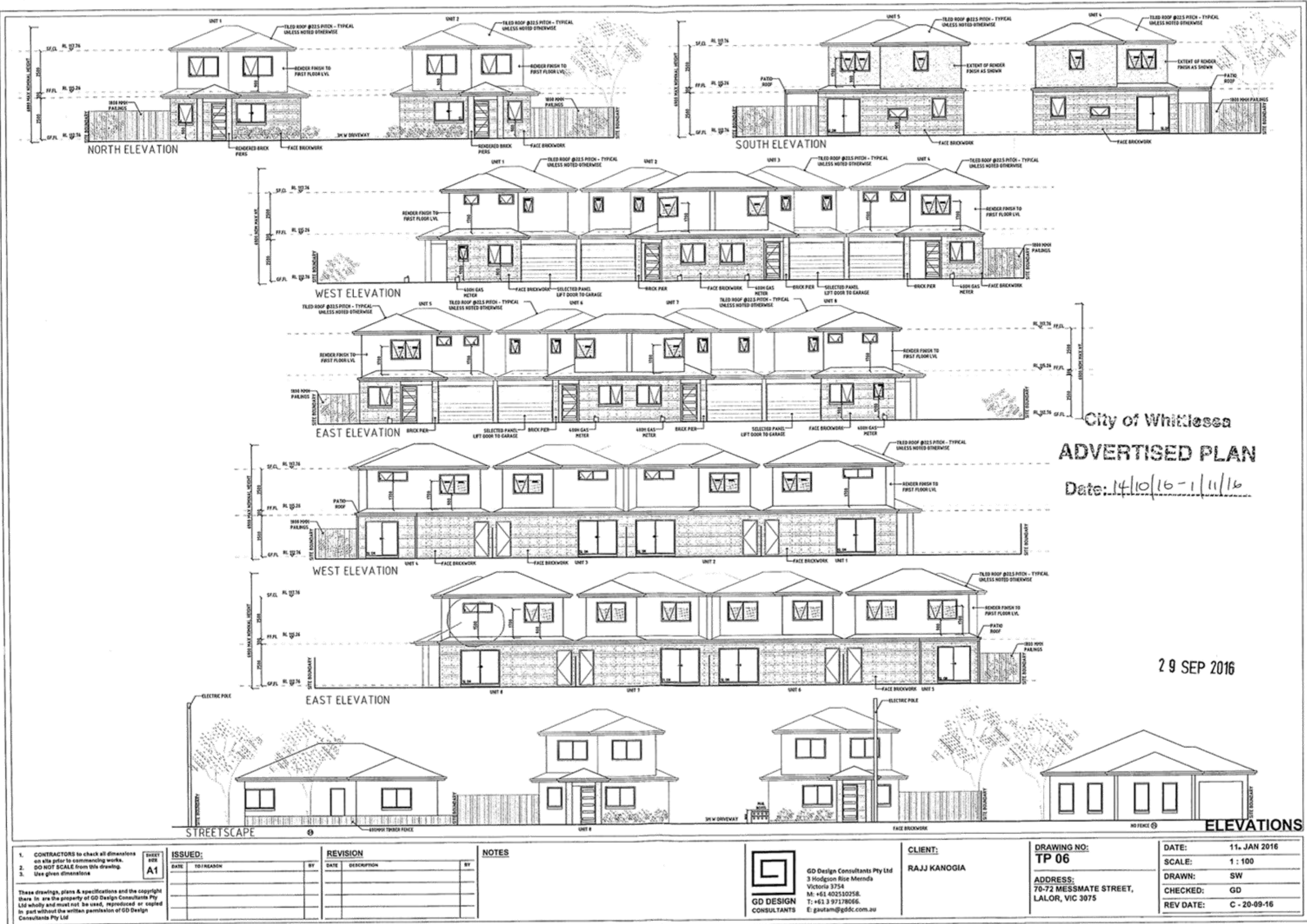
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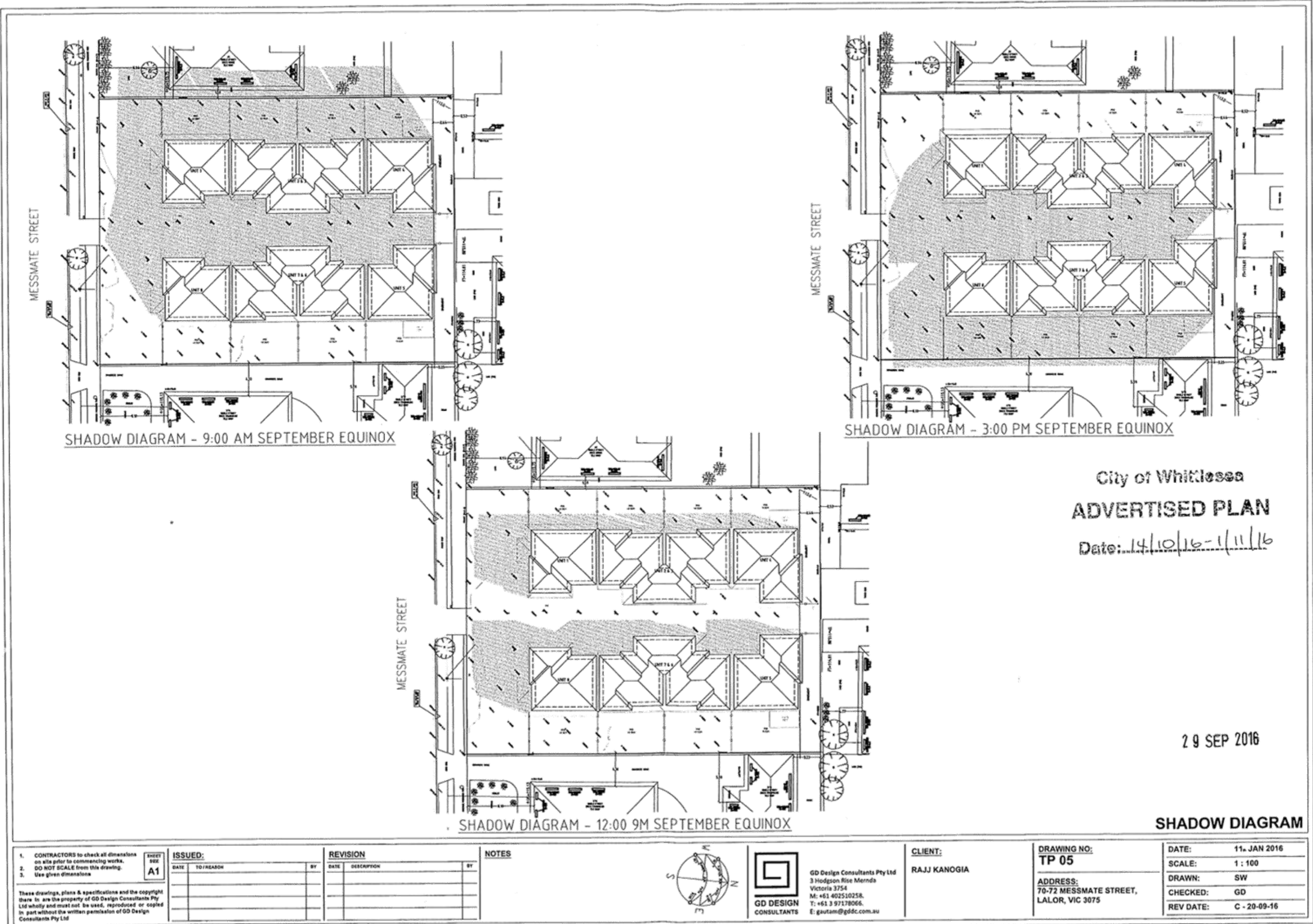




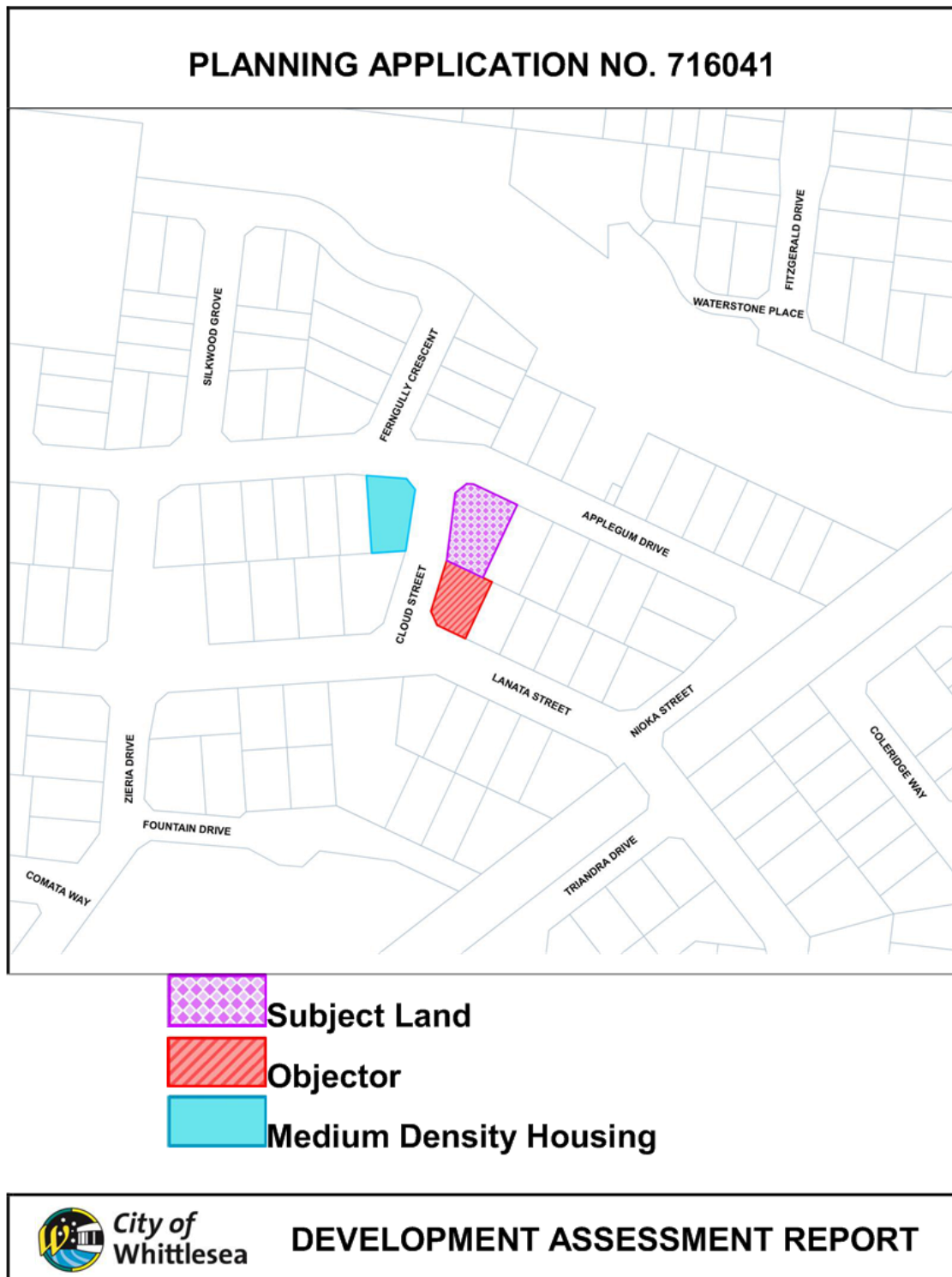












## PLANNING APPLICATION NO. 716041

Aerials: Apr 2016



Subject Land



Objector



Medium Density Housing



City of  
Whittlesea

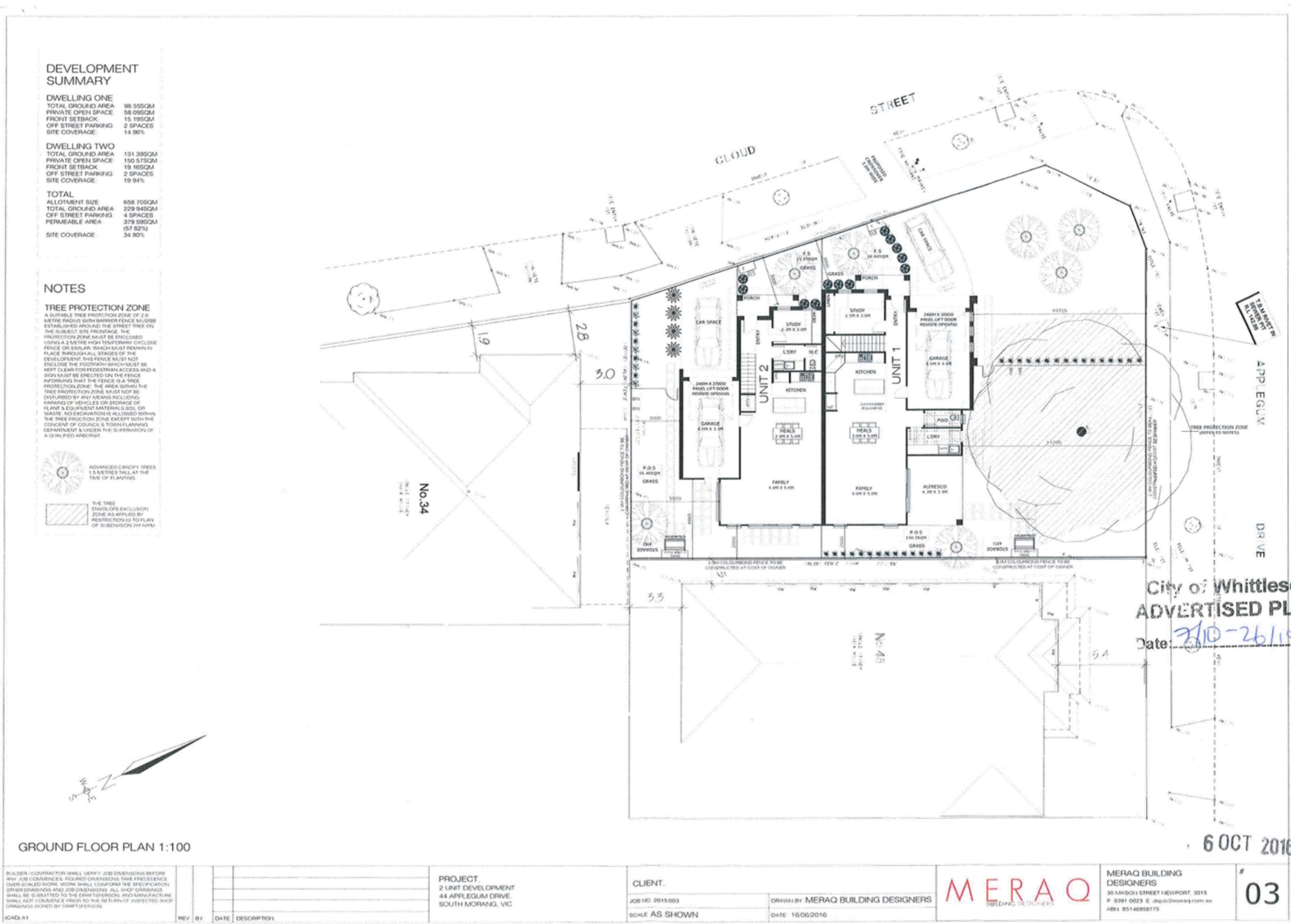
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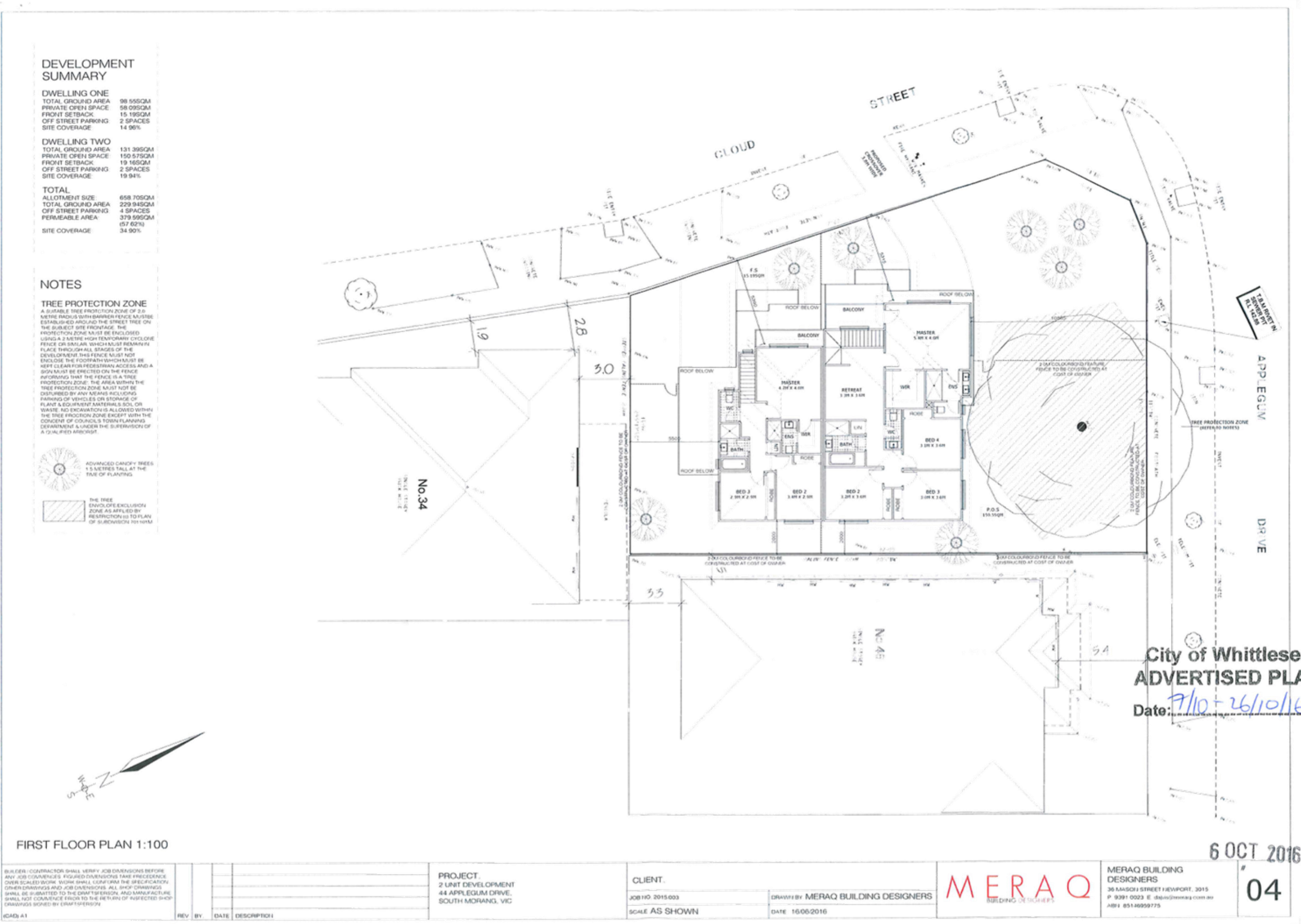










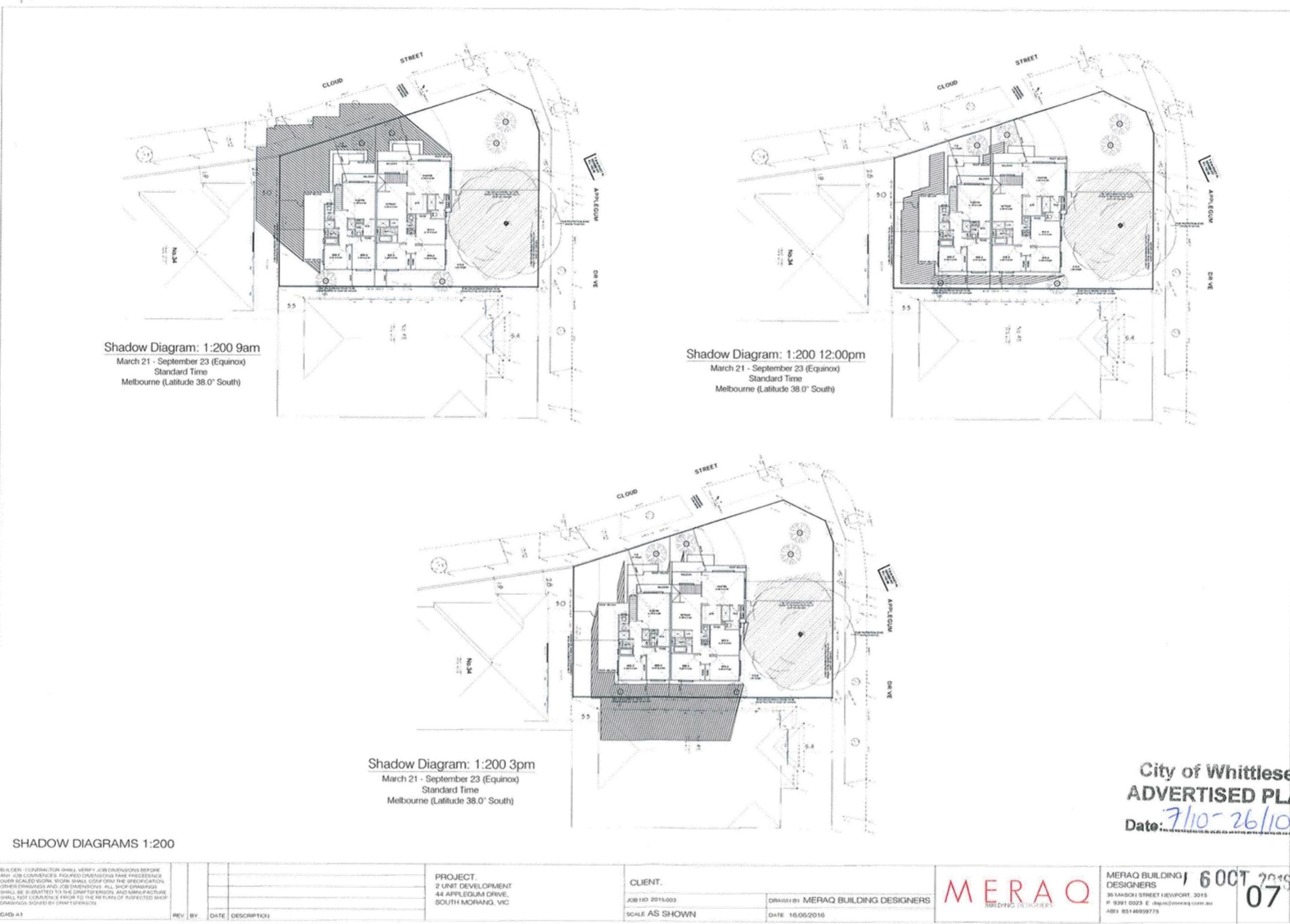


















**Subject Land**



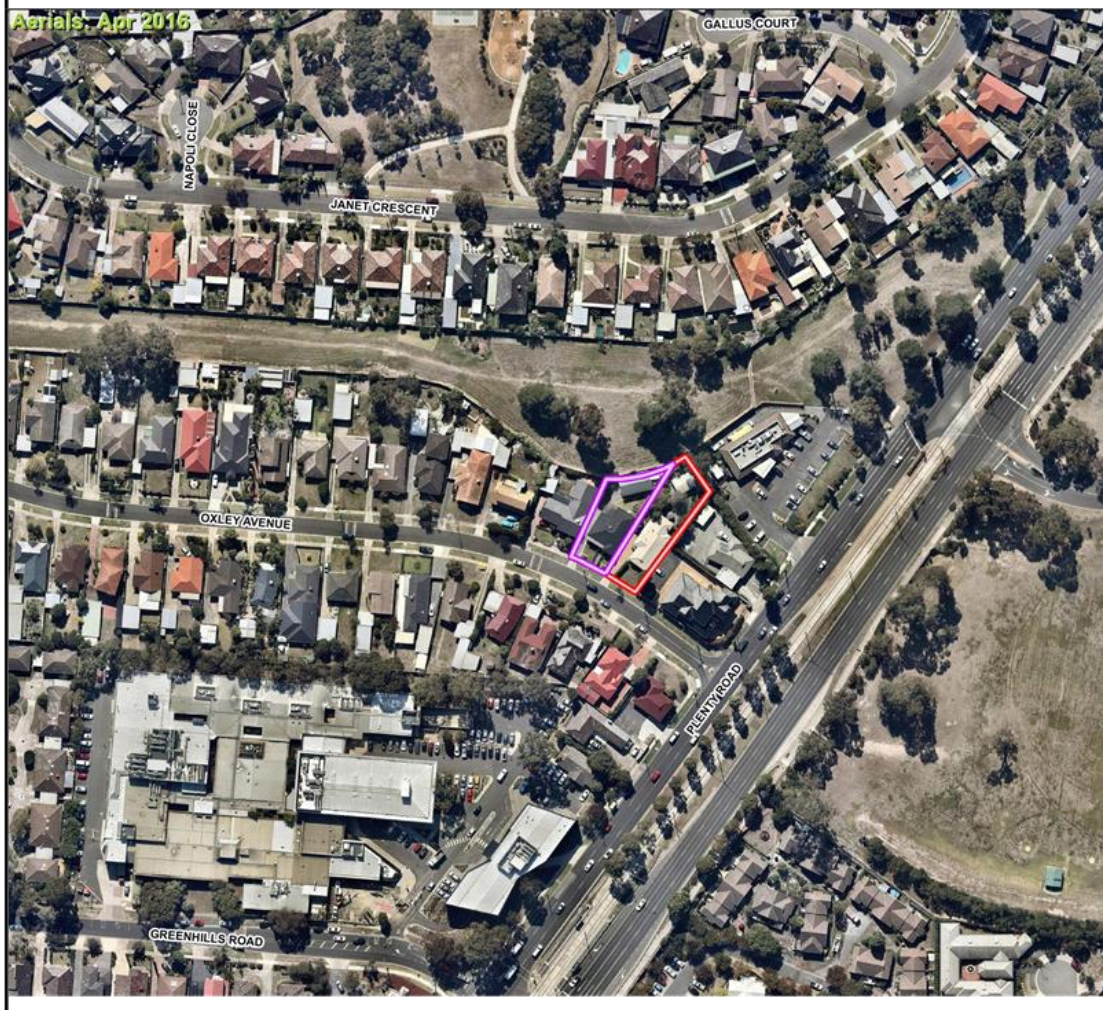
**Objector**





**City of  
Whittlesea**

**ESTABLISHED AREAS PLANNING REPORT**

## PLANNING APPLICATION NO. 715885



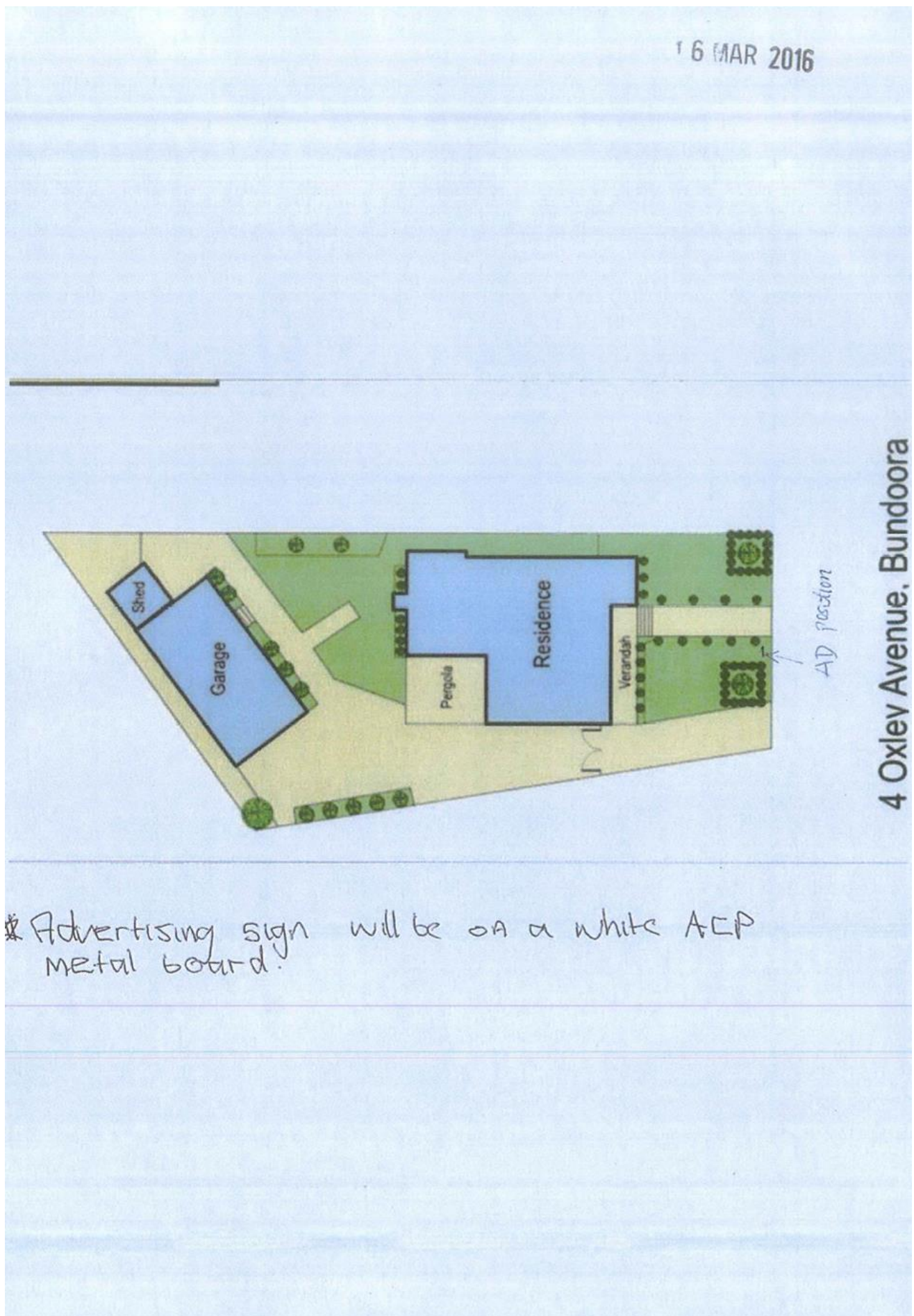
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-  Objector



City of  
Whittlesea

ESTABLISHED AREAS PLANNING REPORT







16 MAR 2016

专家坐诊 | 疑难杂症 | 中草成药 | 针灸推拿 | 刮痧拔罐

Expert Diagnosis | Incurable Diseases  
Herbal Medicines | Acupuncture | Cupping

Grace

CHINESE MEDICINE

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9467 2132

0417 479 734

1.6m

1.2m

# PLANNING APPLICATION NO. 716103



Subject Land



Objector





City of  
Whittlesea

ESTABLISHED AREAS PLANNING REPORT



# PLANNING APPLICATION NO. 716103

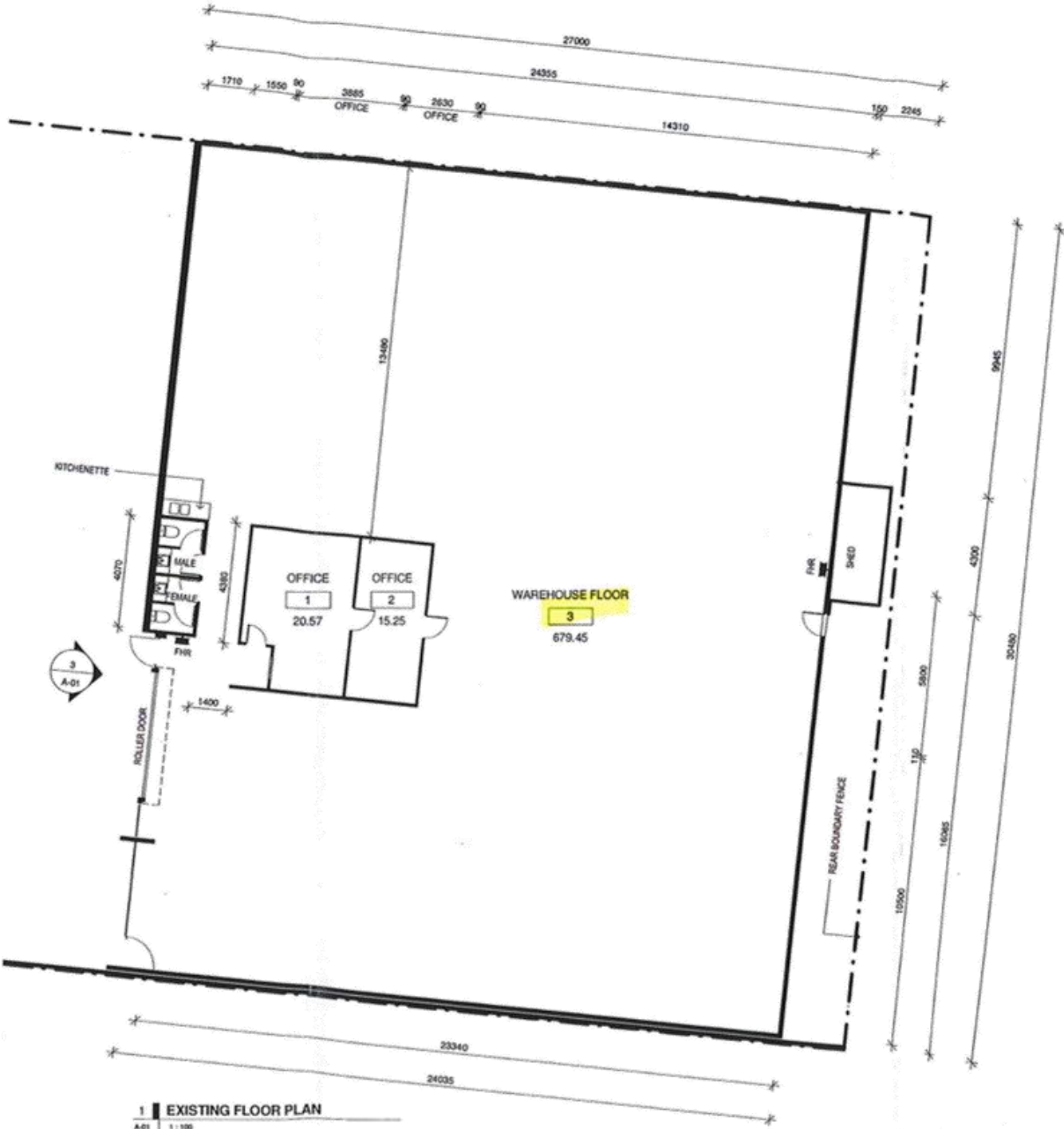


-  Subject Land
-  Objector

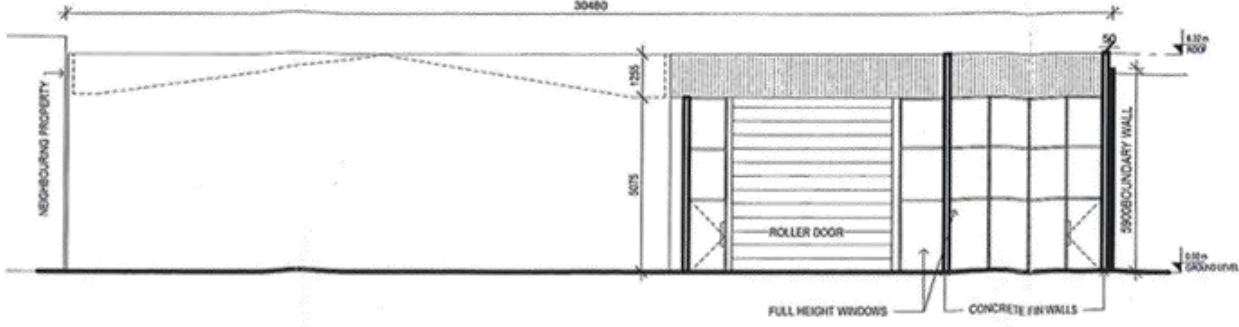


City of  
Whittlesea

ESTABLISHED AREAS PLANNING REPORT



1 EXISTING FLOOR PLAN  
A01 1:100



3 LOT 3 FRONT ELEVATION  
A01 1:100

Notes:

1. All dimensions are in millimetres.
2. All dimensions are to be checked on site.
3. Any discrepancy between the architectural drawing and other drawings relevant for construction should be notified before commencement.
4. Information provided in this plan is to assist in designing proposed construction on this site only.

No title boundary survey has been made. Boundary dimensions as shown have been taken from the title diagram or deed unless noted otherwise.

Services have been located by field survey where obvious, however, prior to any demolishing, excavation or construction on the site, services providers should be contacted to ascertain all possible locations of underground services. Features or improvements shown hereon may have been exaggerated for clarity - DO NOT SCALE.

City of Whittlesea  
ADVERTISED PLAN  
Date: 8/9-23/9/16

1 AUG 2016



RELEASED FOR TOWNPLANNING

**Jerdar** House of Jerdar Designs  
11 Jasper grove  
Greenvale, Vic 3009  
Mob: 0434513788  
e: medojessae@hotmail.com  
www.houseofjerdar.xyz

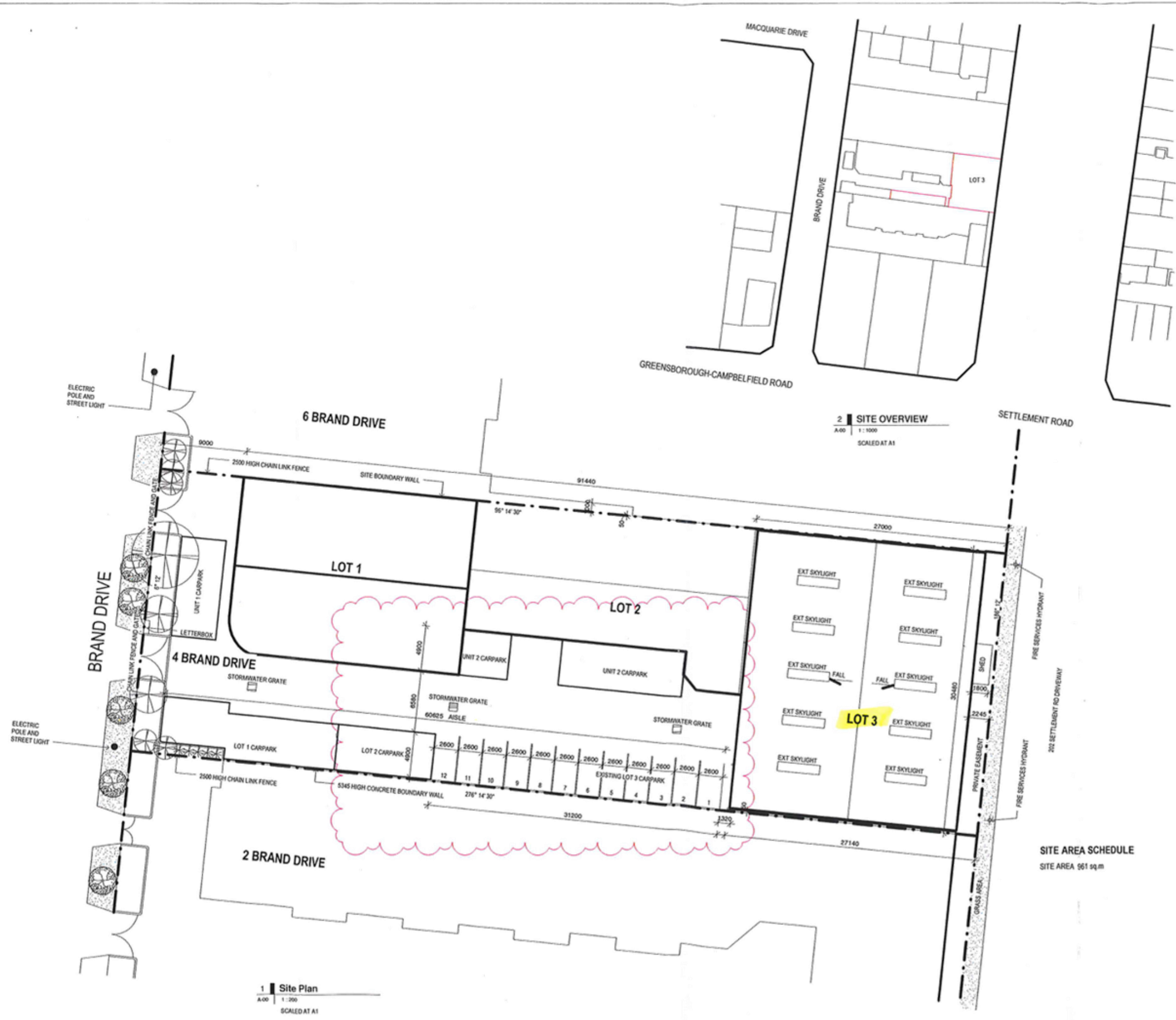
FAITH AND VICTORY CHURCH INC

3/4 BRAND DRIVE THOMASTOWN

Existing Floor Plan & Elevation  
SCALE: 1:100  
DATE: 22/7/16  
DRAWN: JS  
CHECKED: JS

Room Schedule		
Number	Name	Area
1	OFFICE	20.57
2	OFFICE	15.25
3	WAREHOUSE FLOOR	679.45
		715.28





Notes:

1. All dimensions are in millimetres.
2. All dimensions are to be checked on site.
3. Any discrepancy between the architectural drawing and other drawings relevant for construction should be notified before commencement.
4. Information provided in this plan is to assist in designing proposed construction on this site only.

No title boundary survey has been made. Boundary dimensions as shown have been taken from the title diagram or deed unless noted otherwise.

Services have been located by field survey where obvious, however, prior to any demolishing, excavation or construction on the site, services providers should be contacted to ascertain all possible locations of underground services. Features or improvements shown hereon may have been exaggerated for clarity - DO NOT SCALE.

City of Whittlesea  
ADVERTISED PLAN  
Date: 8/9-23/9/16

1	REQUEST FOR ADVERTISED INFORMATION	SEPT 2016
REV	DESCRIPTION	DATE

RELEASED FOR TOWNPLANNING

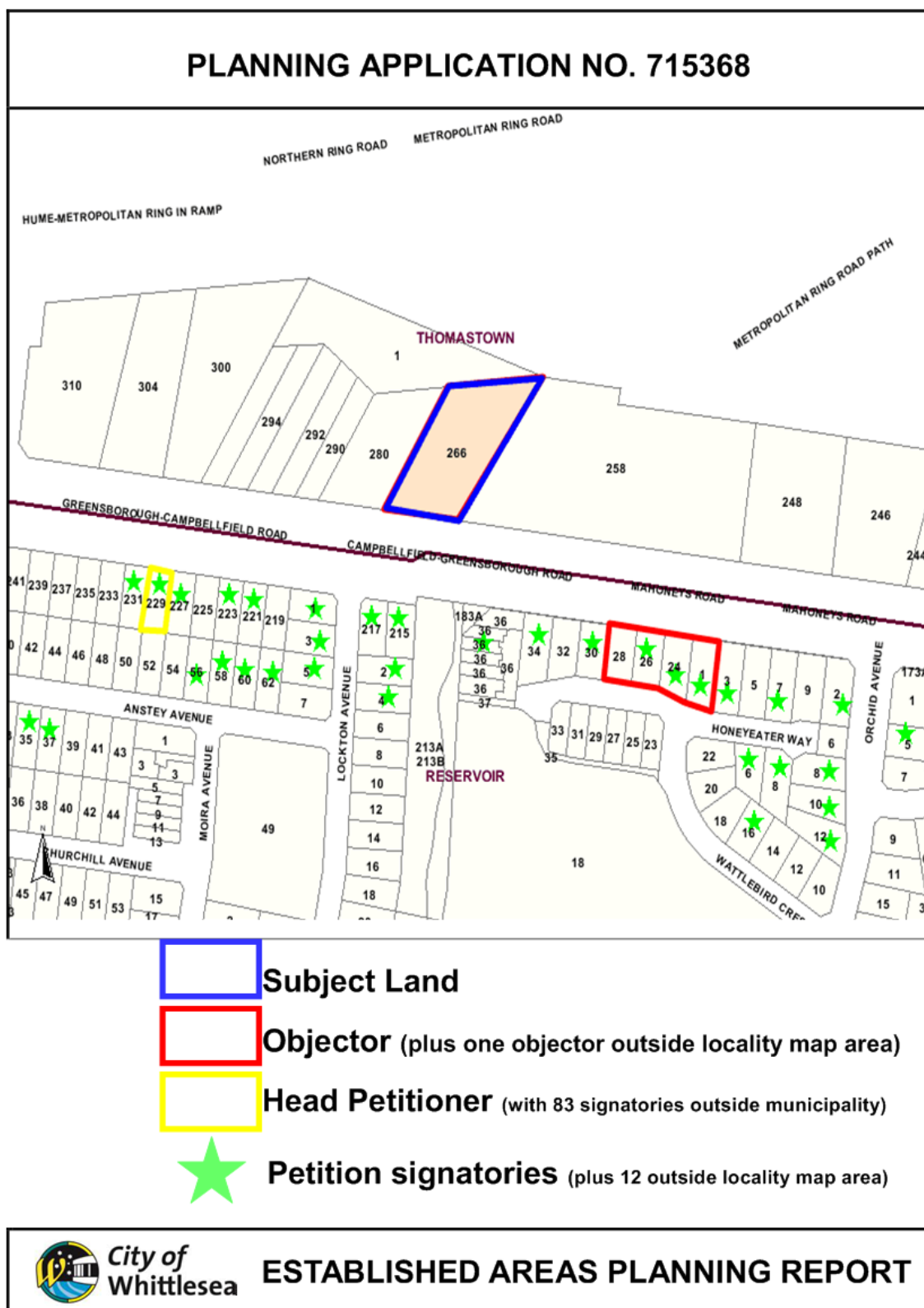
**Jerdar** House of Jendar Designs  
11 Jasper Grove  
Greenvale, Vic 3059  
Mob: 0434513788  
e: medojesse@hotmail.com  
www.houseofjendar.xyz

CLIENT  
FAITH AND VICTORY CHURCH INC

SITE  
3/4 BRAND DRIVE THOMASTOWN

SITE PLAN			
SCALE AT A1	DATE	DRAWN	CHECKED
As indicated	09/2016	JS	JS





# PLANNING APPLICATION NO. 715368



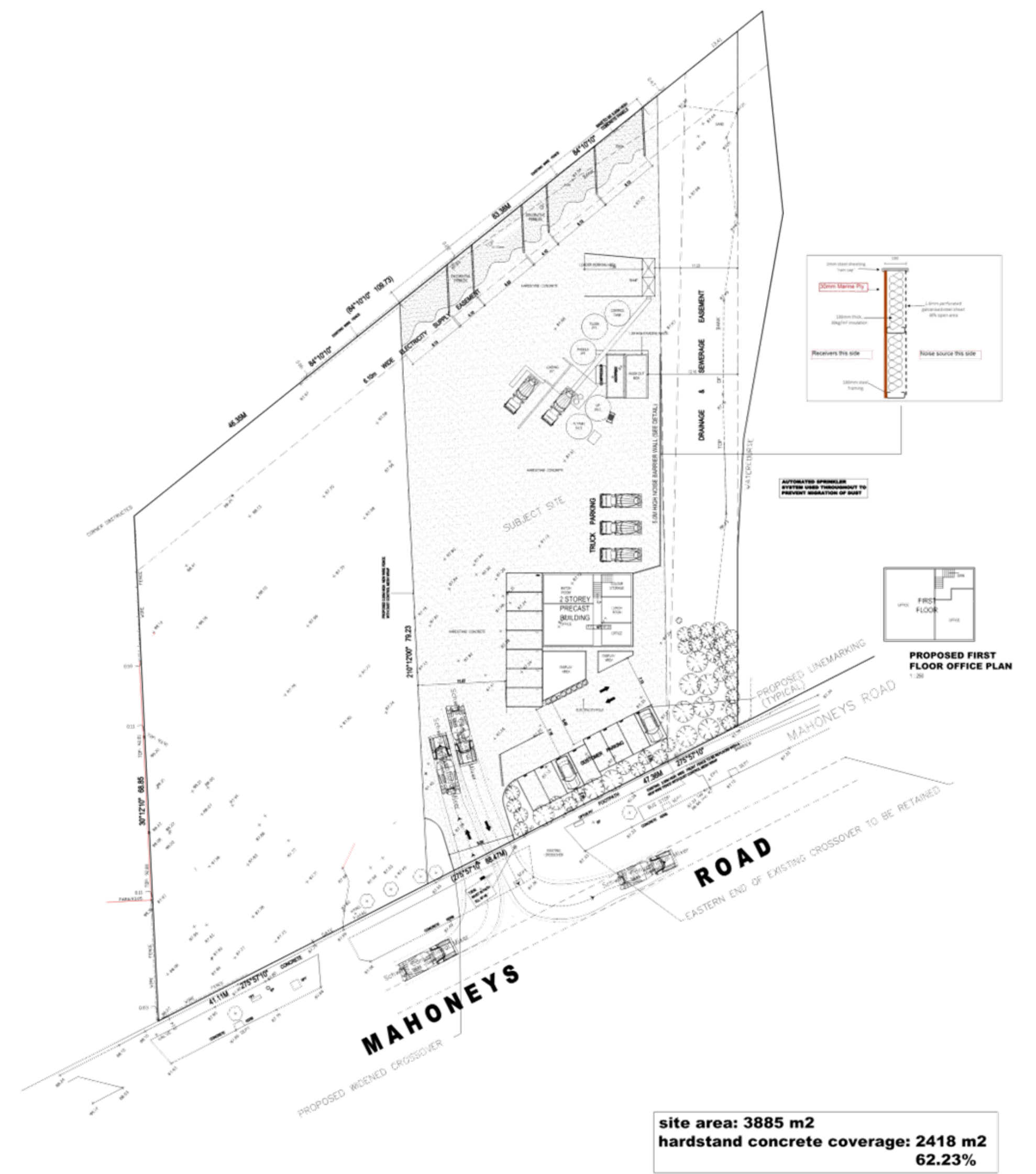
 Subject Land



City of  
Whittlesea

ESTABLISHED AREAS PLANNING REPORT






TOWN PLANNING SUBMISSION


MIDWAY MAHONEYS ROAD PROPOSAL  
265 MAHONEYS ROAD  
THOMASTOWN

REVISIONS			
By	Date	Dep No	Comments
DN	20/01/2016	1	CROSSOVER WIDENED - VEHICLE SLEEP PATHS ADDED
DN	10/09/2016	2	FIRST FLOOR OFFICE ADDED
DN	15/11/2016	REV 1	



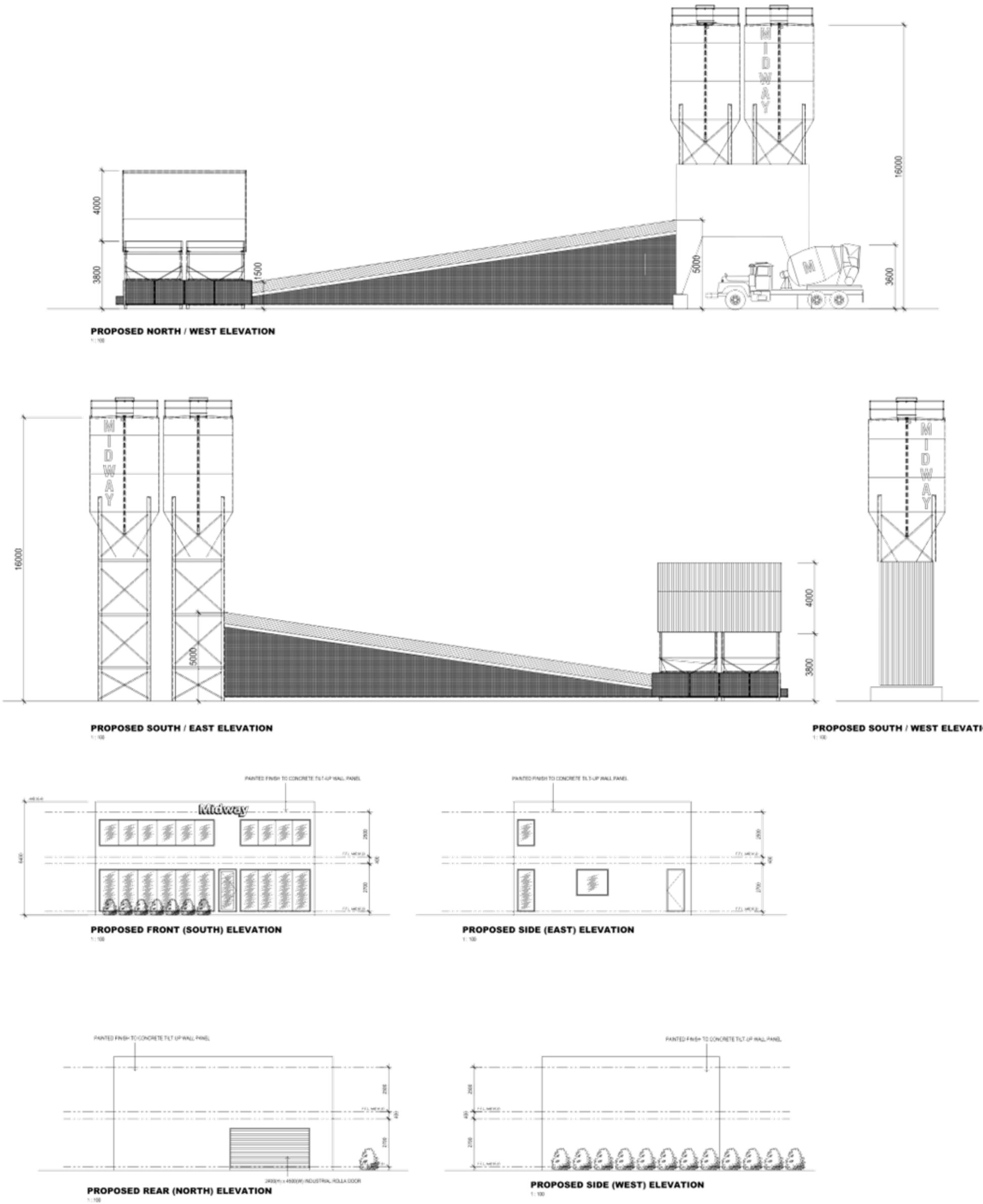
**JONTIAN GROUP**  
building design + town planning  
level 2 210-218 Buckle street, Exton, Victoria 3040  
t 9420 4600 f 9420 4602

Date: OCTOBER 2015  
Scale: 1:250 @ A1  
Drawing No: 01  
Sheet: 01 of 02  
Copyright ©



NORTH






TOWN PLANNING SUBMISSION

**MIDWAY MAHONEYS ROAD PROPOSAL**  
**265 MAHONEYS ROAD**  
**THOMASTOWN**

REVISIONS			
Rev.	Date	Drawn	Comments
01	20/01/2016	D.V.	CROSSOVER WIDENED - VEHICLE SLEEPING PATHS ADDED
02	11/08/2016	D.V.	FIRST FLOOR OFFICE ADDED
03	15/11/2016	D.V.	REVISION

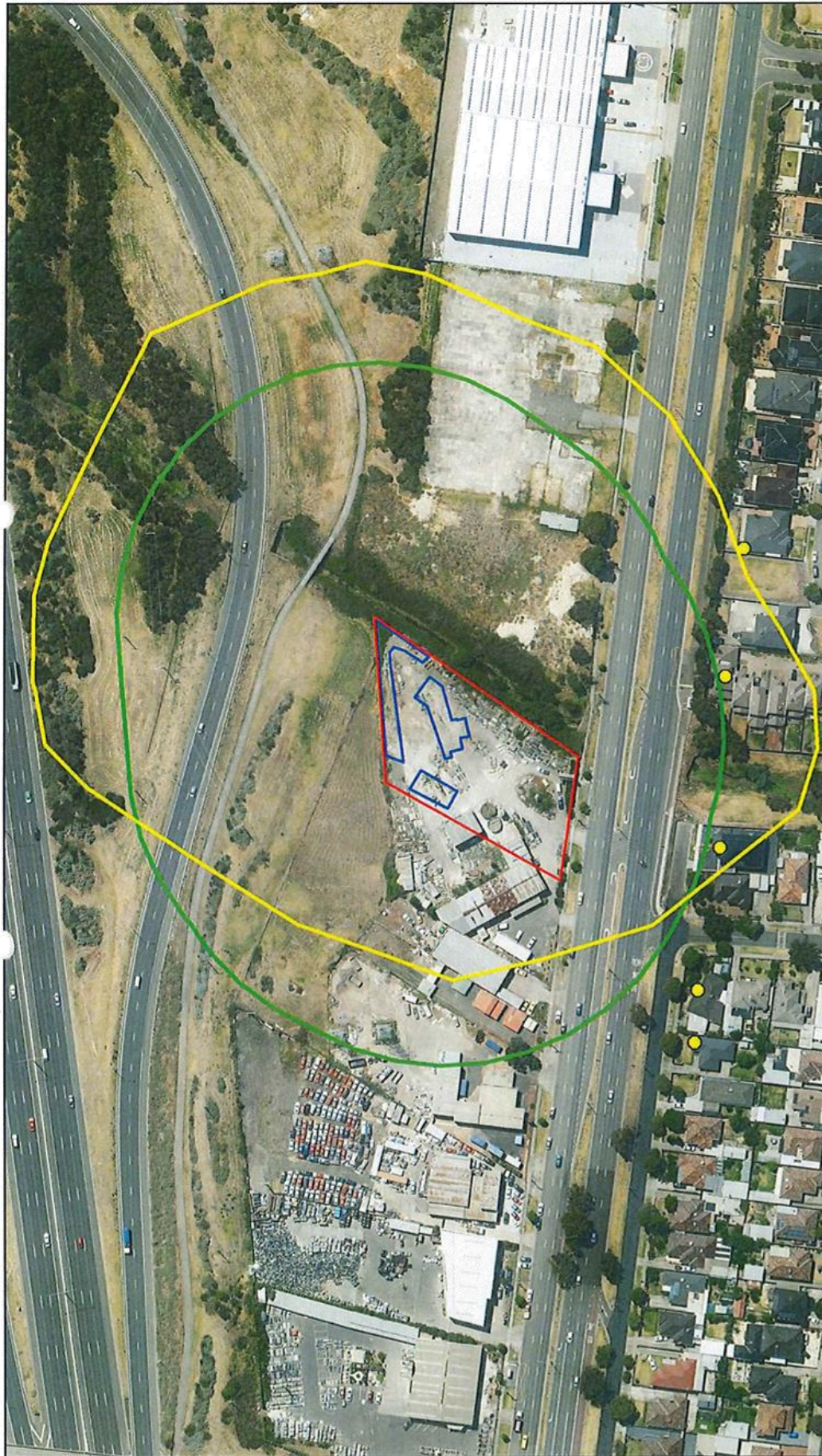


**JONTIAN GROUP**  
building design + town planning  
Level 2, 210-215 Burke Street, Essendon, Victoria 3040  
t: 9457 8877 f: 9457 8882

Date: OCTOBER 2015  
Scale: 1:100 @ A1  
Drawing No: 01  
Sheet: 02 of 02  
Copyright ©

Drawn: D.V.





Job Number | 31-33521  
Revision | A  
Date | 19/02/2016

Midway Concrete & Garden Supplies  
Buffer constraint assessment



CLIENTS PEOPLE PERFORMANCE

100 m Directional buffer

Figure 9

100 m Default buffer  
100 m Directional buffer

LEGEND  
Site Boundary  
Nearest residences  
Envelope of Potential Sources



Map Projection: Universal Transverse Mercator  
Horizontal Datum: Geocentric Datum of Australia 1994  
GCS: Map Grid Of Australia, Zone 55

8780 Lonsdale St Melbourne VIC 3000 Australia P 61 3 8837 8800 F 61 3 8837 8111 E midmail@ghd.com.au W www.ghd.com.au  
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(Where in contract, text or otherwise) for any expenses, losses, damages and/or costs (including indirect or consequential damages) which are or may be incurred as a result of the product being inaccurate, incomplete or unsuitable in any way and for any reason.  
Source: Google Earth Pro, Created by: B. McKay





Job Number | 31-33521  
Revision | A  
Date | 19/02/2016

Midway Concrete & Garden Supplies  
Buffer constraint assessment



CLIENTS PEOPLE PERFORMANCE

36 m Default buffer  
36 m Directional buffer

LEGEND  
Site Boundary  
Nearest residences  
Envelope of Potential Sources



1:2,000  
0 20 40 60  
Metres (at A4)

Map Projection: Universal Transverse Mercator  
Horizontal Datum: Geocentric Datum of Australia 1994  
GHD: Map GHD Of Australia, Zone 35

36 m Directional de-rated buffer Figure 10

G:\3133521\GIS\Map\Working\BaseMap\WOR  
© 2015. While GHD has taken care to ensure the accuracy of this product, GHD and Midway Concrete & Garden Supplies, make no representations or warranties about its accuracy, completeness or suitability for any particular purpose. GHD and Midway Concrete & Garden Supplies cannot accept liability of any kind (whether in contract, tort or otherwise) for any expenses, losses, damages and/or costs (including indirect or consequential damage) which are or may be incurred as a result of the product being inaccurate, incomplete or unusable in any way and for any reason.  
Source: Google Earth Pro; Created by: B. McKay





# PLANNING APPLICATION NO. 714104



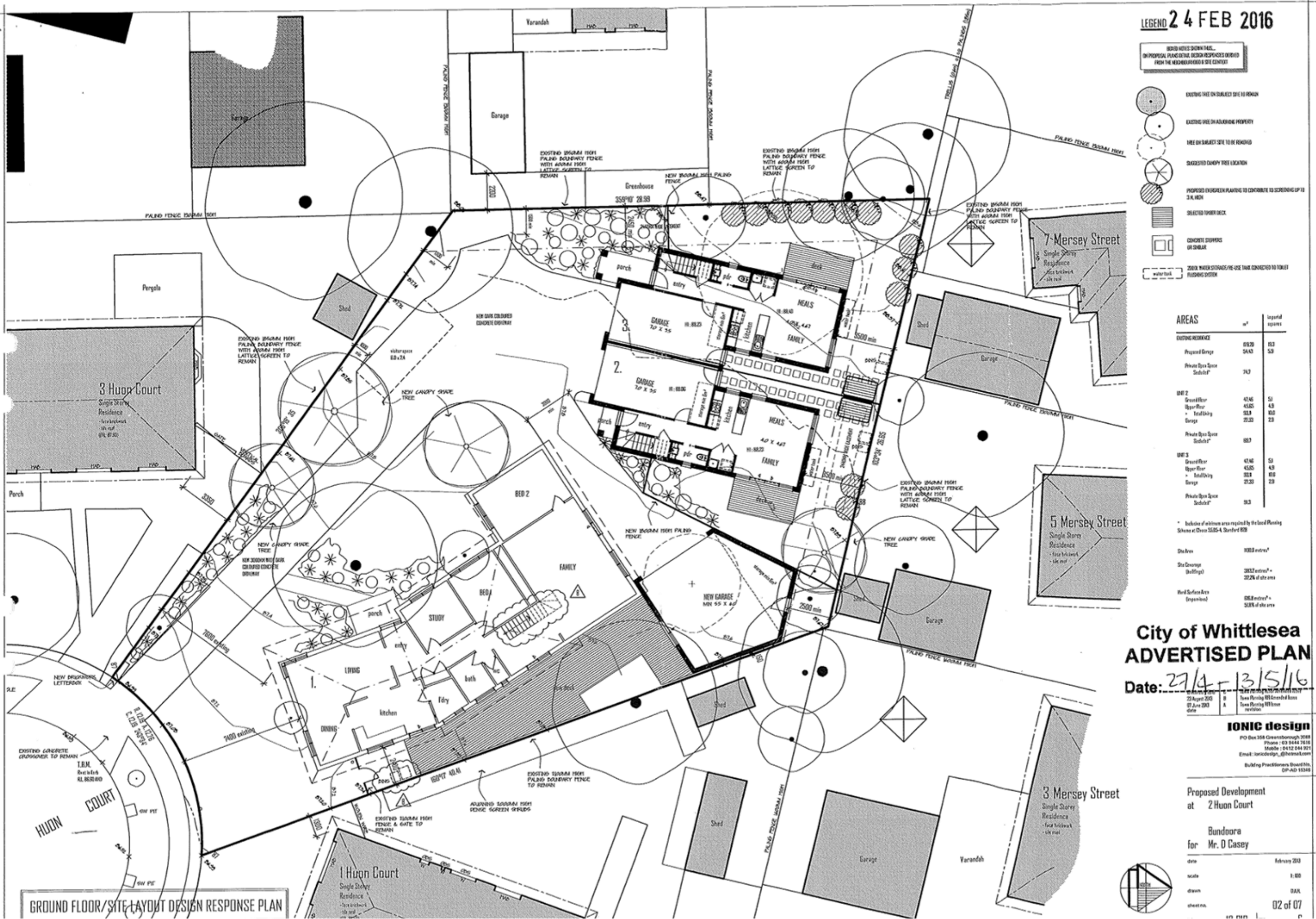
- Subject Land
- Objector
- Medium Density Housing



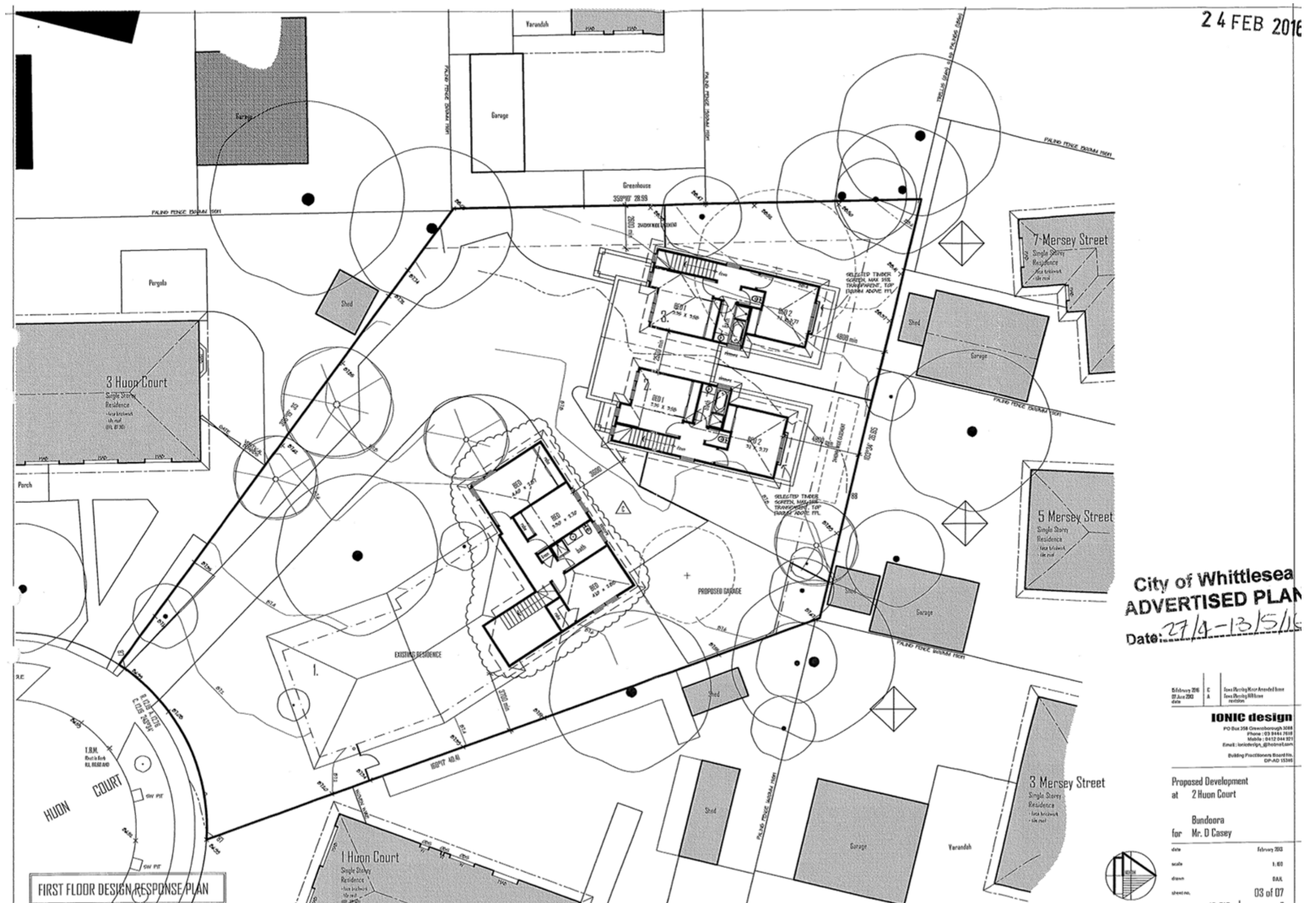
City of  
Whittlesea

ESTABLISHED AREAS PLANNING REPORT





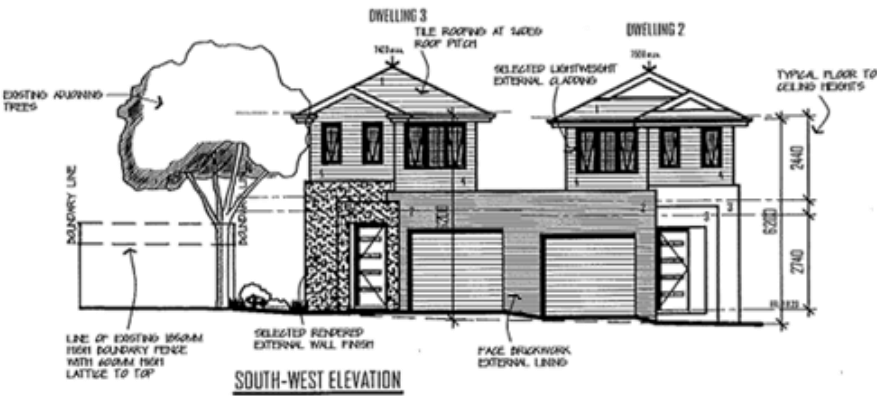




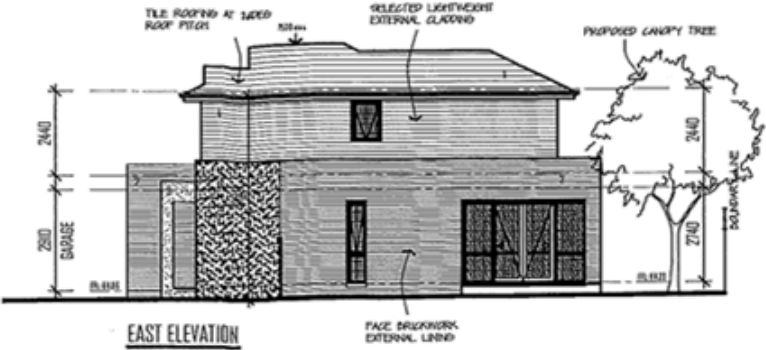
24 FEB 2016

SCHEDULE OF FINISHES

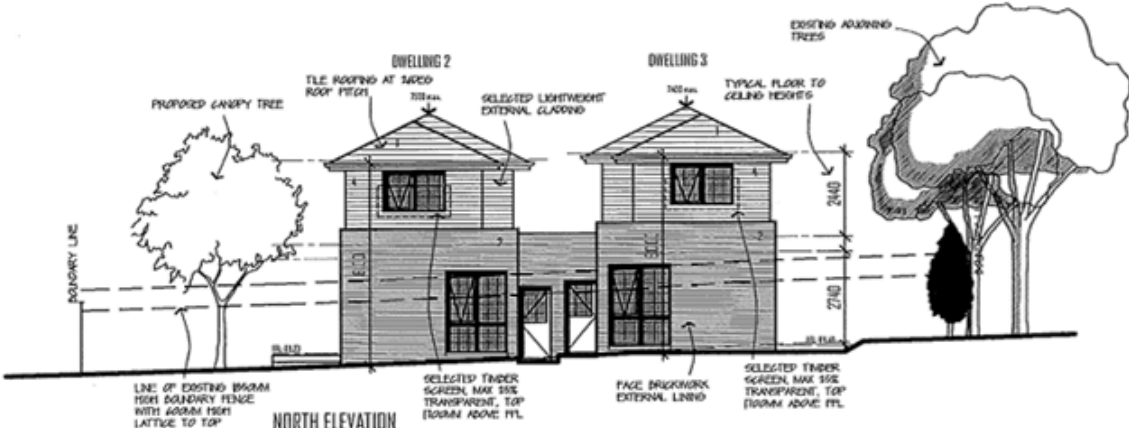
- TILE ROOF COVERING  
Duroflex or similar
- FACE BRICKWORK  
Acacia Brick 750mm x 150mm or similar
- SELECTED RENDER  
Duroflex Brick or similar
- SELECTED RENDER CLADDING  
Duroflex Brick or similar



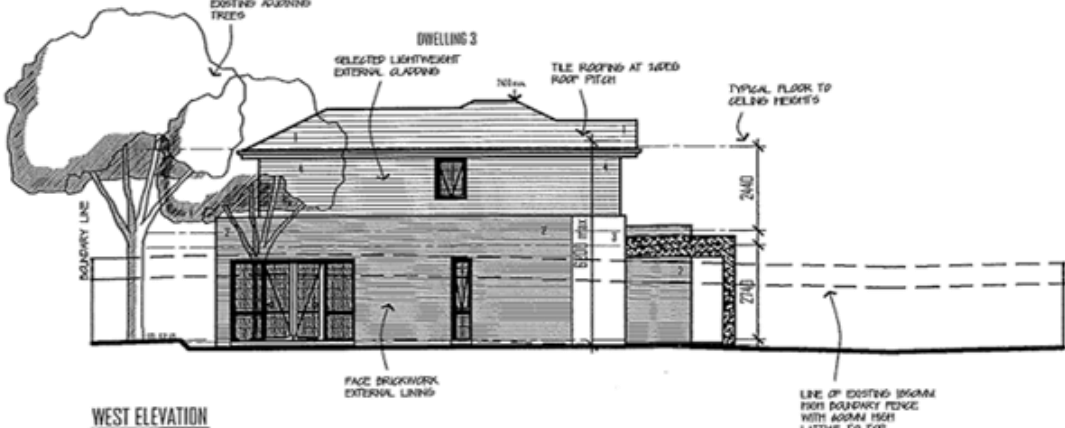
SOUTH-WEST ELEVATION



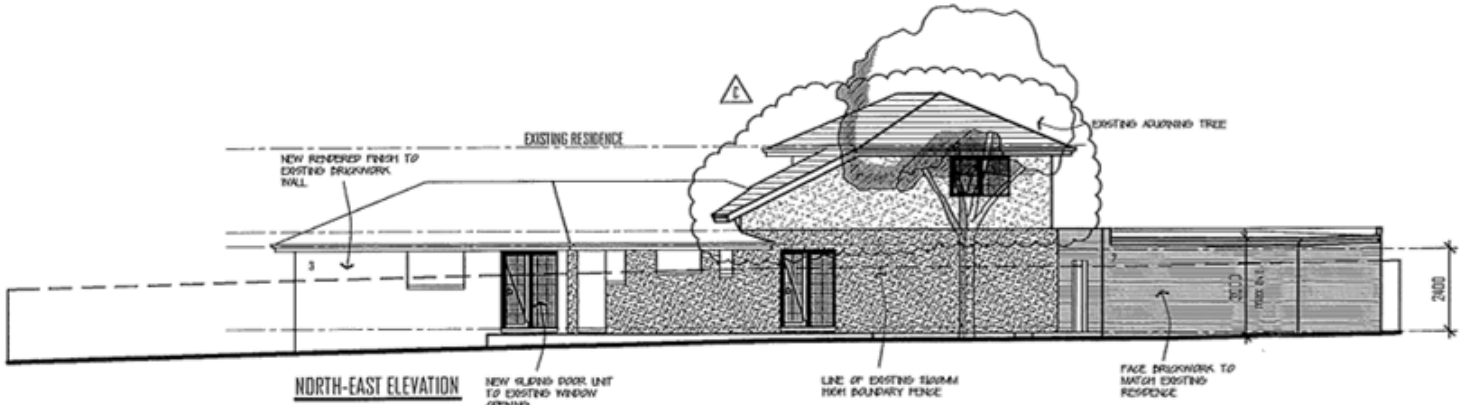
EAST ELEVATION



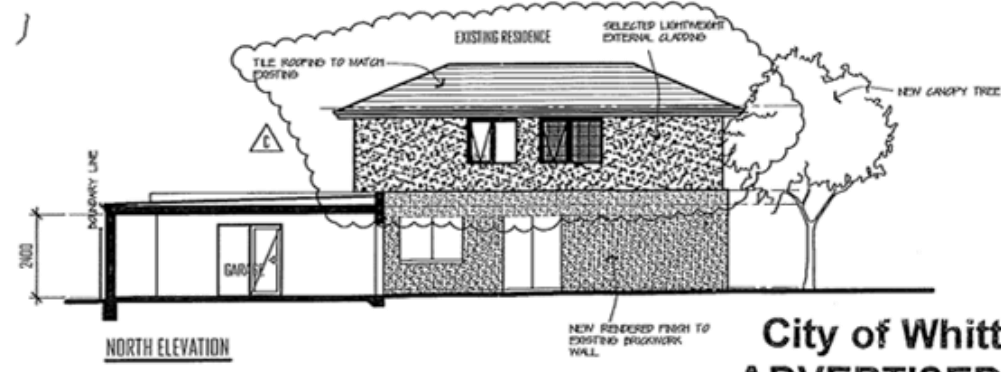
NORTH ELEVATION



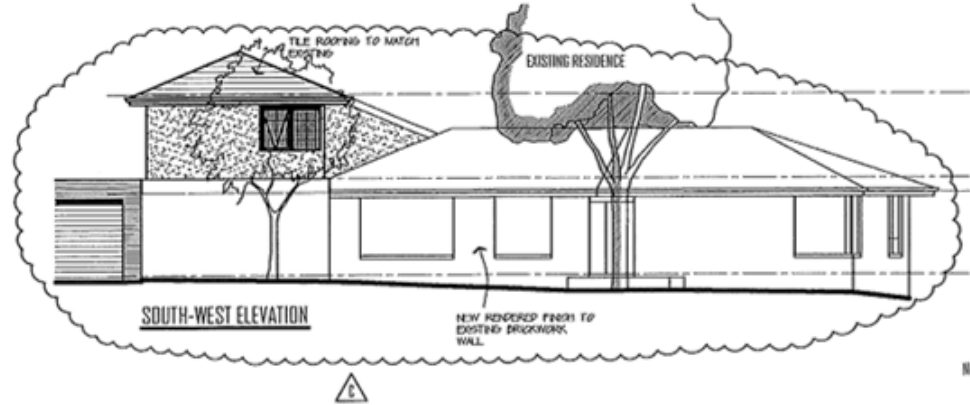
WEST ELEVATION



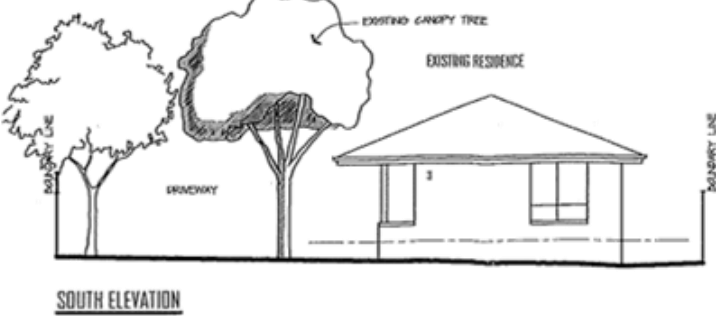
NORTH-EAST ELEVATION



NORTH ELEVATION



SOUTH-WEST ELEVATION



SOUTH ELEVATION



STREETSCAPE ELEVATION

City of Whittlesea  
ADVERTISED PLAN  
Date: 27/4 - 13/5/16

23 August 2015  
07 June 2015  
date  
B  
A  
Issue Review  
Issue Review  
revision  
**IONIC design**  
PO Box 358 Greenborough 3088  
Phone: 03 9444 7510  
Mobile: 0412 044 929  
Email: ionicdesign@telnet.com.au  
Building Practitioners Board No. DP-AD 15316

Proposed Development  
at 2 Huon Court  
Bundoora  
for Mr. D Casey  
date February 2015  
scale 1:100 61:200  
drawn BAJ  
sheet no. 04 of 07

ELEVATIONS - DESIGN RESPONSE PLAN







**City of  
Whittlesea**

**DEVELOPMENT ASSESSMENT REPORT**

# PLANNING APPLICATION NO. 712059



-  Subject Land
-  Medium Density Housing



City of  
Whittlesea

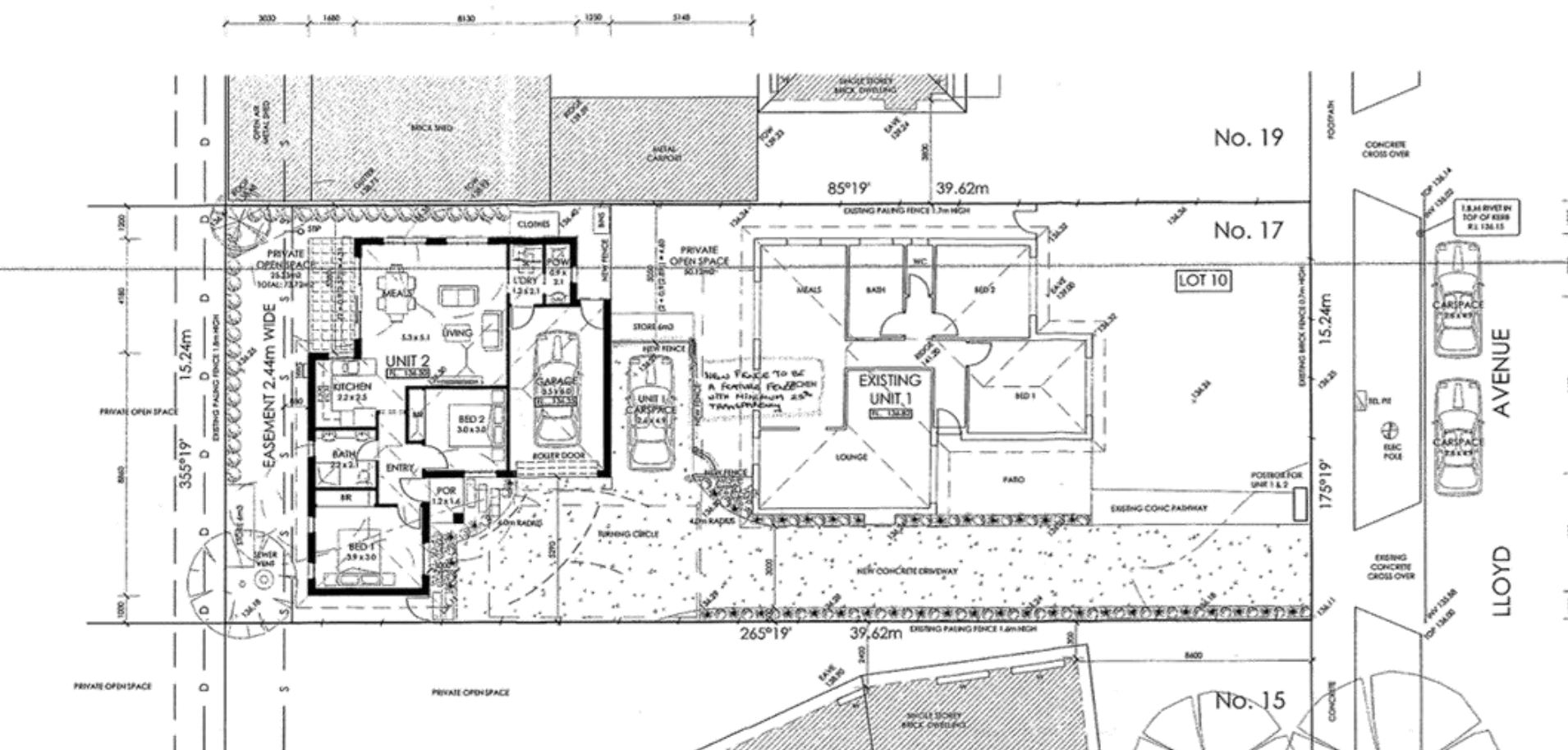
## DEVELOPMENT ASSESSMENT REPORT

City of Whittlesea  
ADVERTISED PLAN

Date: 2/1/16 - 3/5/16



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GROUND FLOOR PLAN  
SCALE 1:100

# DEVELOPMENT SUMMARY

SITE AREA: 403.80m<sup>2</sup>  
DENSITY: 1:301  
NO. OF DWELLINGS: 2  
CAR PARKING: 4  
UNIT 1: 12  
UNIT 2: 2  
BUILDING SITE COVERAGE: 255.83m<sup>2</sup> (63.40%)  
SITE PERMEABILITY: 245.17m<sup>2</sup> (60.69%)  
PRIVATE OPEN SPACE  
UNIT 1: 50.12m<sup>2</sup> + FRONT YARD  
UNIT 2: 73.72m<sup>2</sup>

# GROSS BUILDING AREA/UNIT:

	UNIT 1	UNIT 2
FLOOR	15.80m <sup>2</sup> (1.49SQ)	2.08m <sup>2</sup> (0.22SQ)
GROUND	102.24m <sup>2</sup> (7.31SQ)	82.11m <sup>2</sup> (8.83SQ)
GARAGE		25.52m <sup>2</sup> (2.72SQ)
	118.04m <sup>2</sup> (12.81SQ)	107.79m <sup>2</sup> (11.55SQ)

# CONSTRUCTION:

INTERNAL WALLS:  
240mm BRICK VENEER STUD CONSTRUCTION  
EXTERNAL WALLS:  
GROUND FLOOR - 240mm BRICK VENEER CONSTRUCTION

# STORMWATER

ALL STORMWATER, INCLUDING ROOF AND PAVEMENT  
STORMWATER TO BE COLLECTED VIA ON SITE SYSTEM AND  
CONNECTED TO LEGAL POINT OF DISCHARGE DETAILS TO BE  
SUBMITTED AS PART OF BUILDING PERMIT. NO STORMWATER TO  
BE DISCHARGED TO ADJACENT SITES OR STREETS.

# LANDSCAPING

FOR DETAILS OF LANDSCAPING, REFER TO SEPARATE  
LANDSCAPE PLAN - TO BE SUBMITTED LATER

# EXTERNAL FINISHES SCHEDULE

ROOFING	CONCRETE ROOF TILES
GUTTERS & DOWNPIPES	COLORBOND
FASCIAS	COLORBOND
WALLS	BRICKWORK
WINDOWS & DOORS	FEATURE CLADDING POWDERCOAT ALUMINIUM

# LEGEND

150mm Ø VC SEWER PIPE - DEPTH TO INVERT LEVEL 1.453m APPROX  
- DEPTH 2.15m APPROX EAST FROM WEST BOUNDARY. CONFIRM  
EXACT POSITION ON SITE PRIOR TO CONSTRUCTION

CONCRETE STORMWATER DRAIN - TYPE UNIFORMITY, DEPTH & OFFSET  
WORKING - CONFIRM EXACT POSITION ON SITE PRIOR TO  
CONSTRUCTION

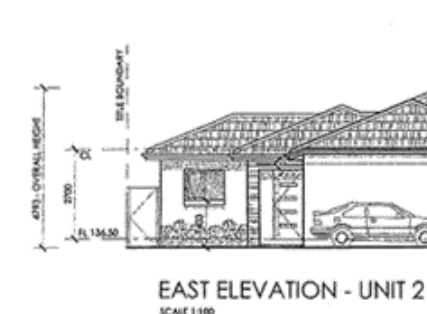
OUTBUILDINGS & VEGETATION TO BE DEMOLISHED/REMOVED  
SHOWN DASHED



W - WINDOW  
SWP - SEWER RE IN POINT  
HWS - HOT WATER SYSTEM  
SL - SETBACK

NOTE:  
ALL NEW VENTILATORS TO BE BRICK FINISHED AS A PARASOL  
HEIGHT 600 - 1000mm ABOVE GROUND  
ALL LEVELS TO A.H.D.

2/1/16



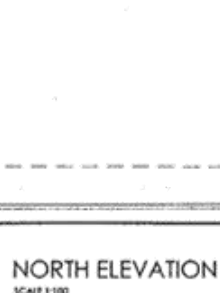
EAST ELEVATION - UNIT 2  
SCALE 1:100



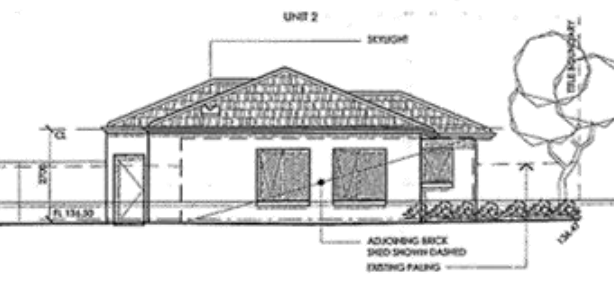
SOUTH ELEVATION  
SCALE 1:100

PLAN AND ENVIRONMENTAL  
CITY OF WHITTLESEA  
PLANNING  
Plan referred to in Permit to use  
and/or develop the site.  
Application No. 712,000  
Sheet 1 of 2  
Date Permit Issued: 16/11/2016  
Drawn: J. McKeown  
Date: 2/1/2016  
Title: PRELIMINARY

EXISTING UNIT 1



NORTH ELEVATION  
SCALE 1:100



WEST ELEVATION - UNIT 2  
SCALE 1:100

DUAL OCCUPANCY DEVELOPMENT  
FOR JIM BELEVSKI  
17 LLOYD AVENUE, EPPING

TP-03 B Job No. Scale. Drawn. Date.  
4476 1:100 JBI APR 16  
FLOOR PLANS, ELEVATIONS & AREA ANALYSIS



ikonomidisreid

building design consultants

603 Plenty Road,  
Ruswarp, VIC 3107  
Email: admin@ikonomidisreid.com.au  
Ph: 9478 2836  
Fax: 9478 2034

8 APR 10 FURTHER INFORMATION

A NOV 09 TOWNPLANNING APPLICATION  
P1 NOV 09 PRELIMINARY ISSUE







**City of  
Whittlesea**

## DEVELOPMENT ASSESSMENT REPORT



# PLANNING APPLICATION NO. 714892



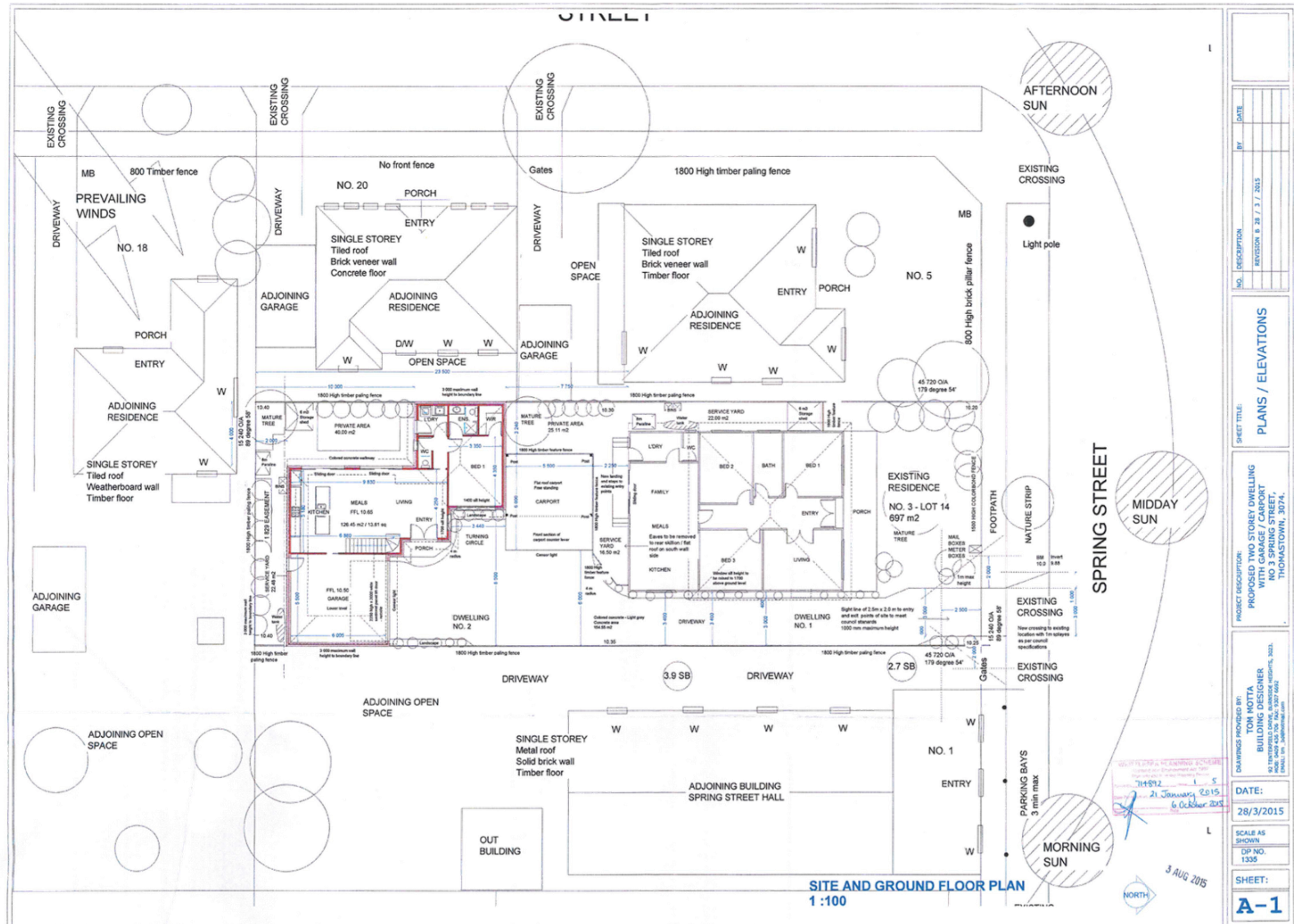
-  Subject Land
-  Medium Density Housing



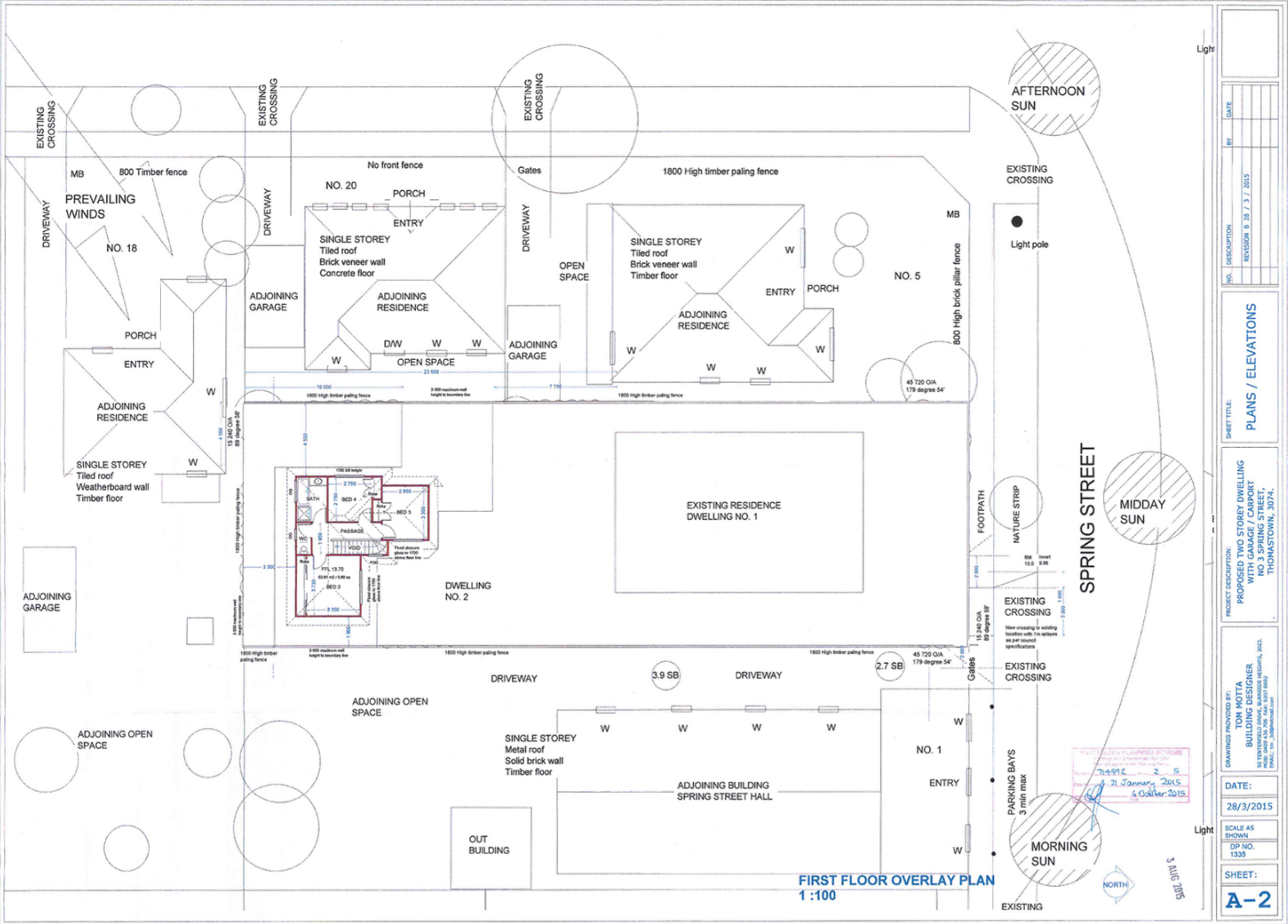
City of  
Whittlesea

## DEVELOPMENT ASSESSMENT REPORT

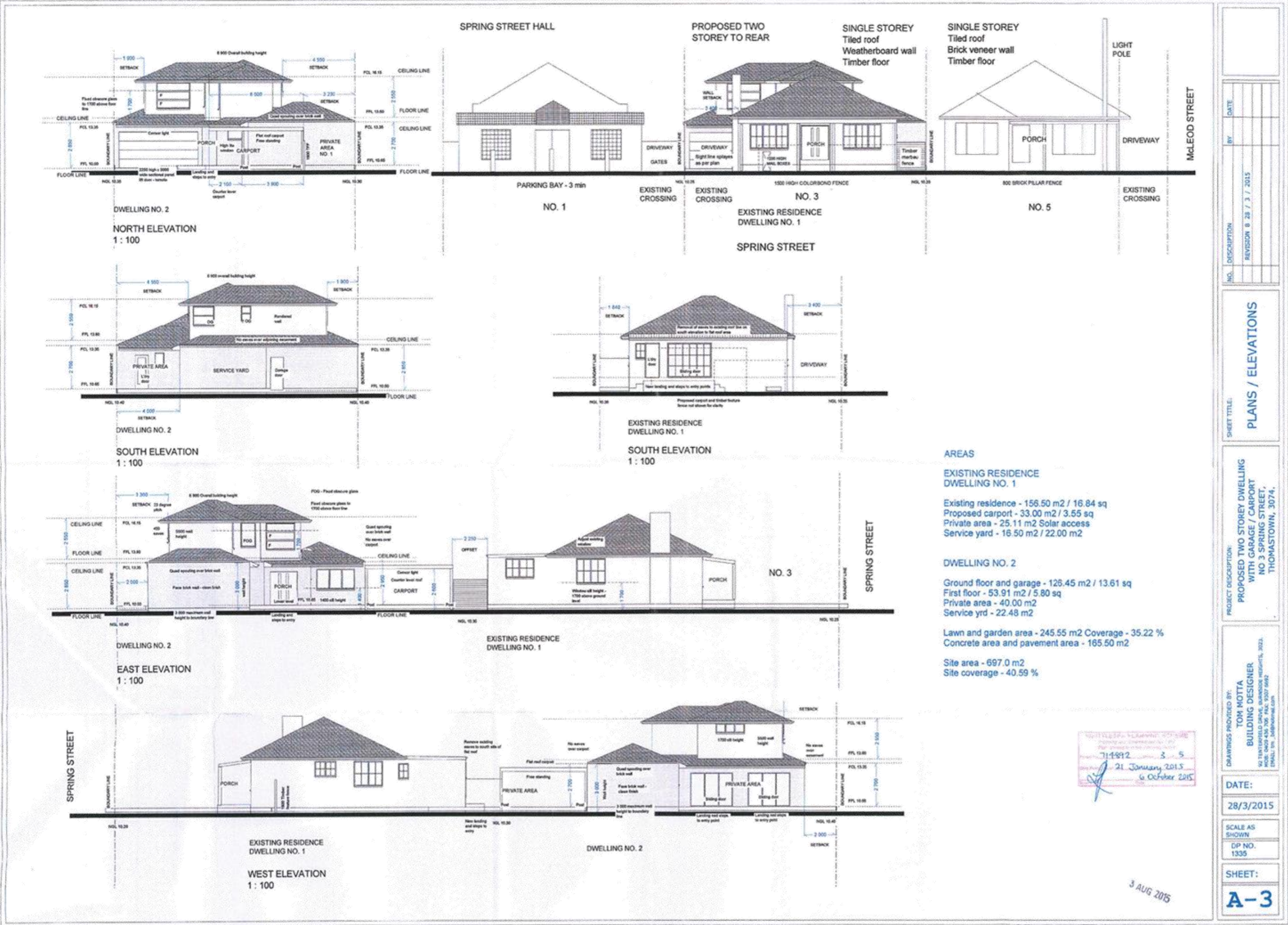




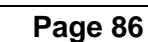














**City of  
Whittlesea**

**DEVELOPMENT ASSESSMENT REPORT**



# PLANNING APPLICATION NO. 714774



Subject Land



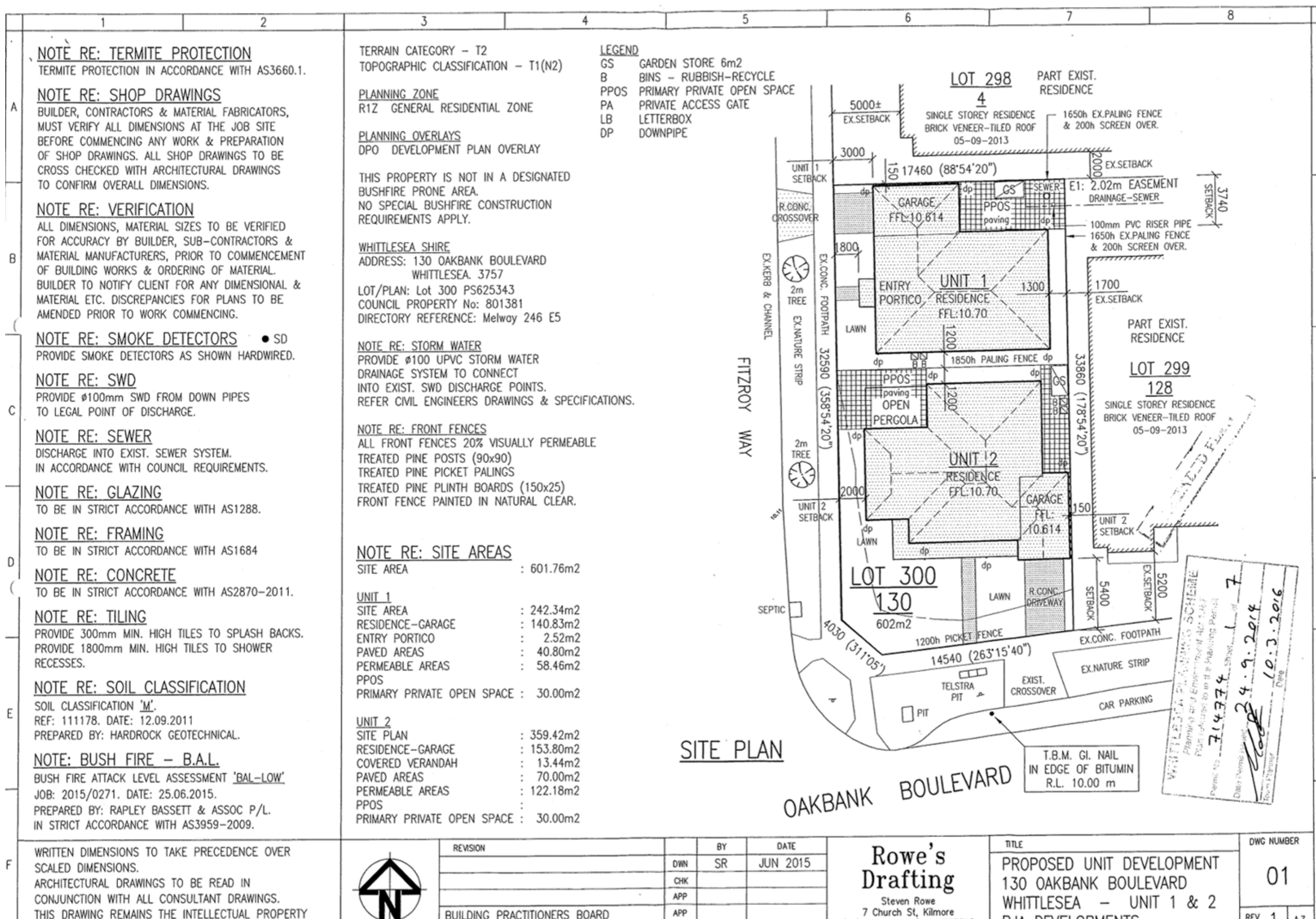
Medium Density Housing

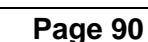


City of  
Whittlesea

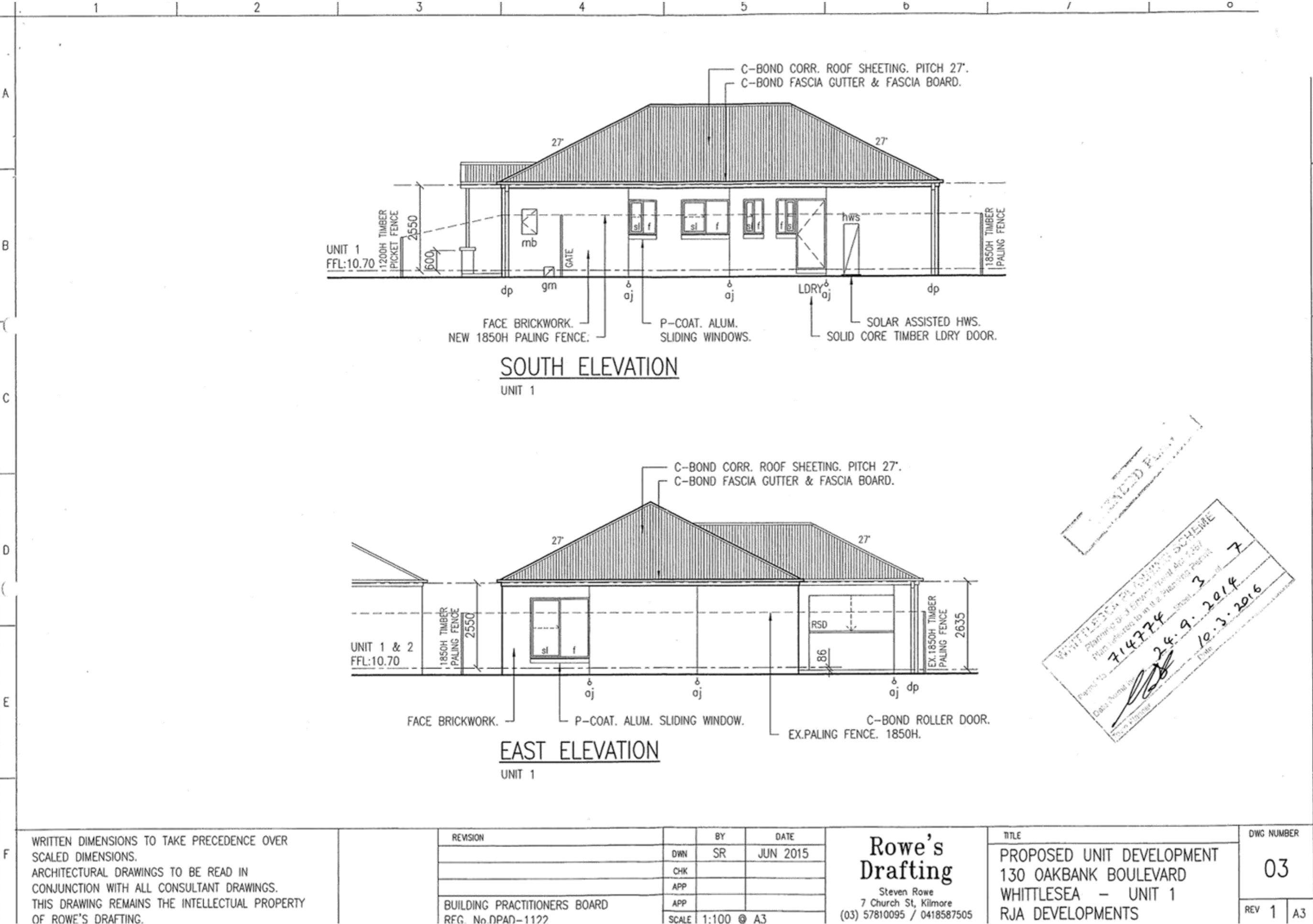
## DEVELOPMENT ASSESSMENT REPORT

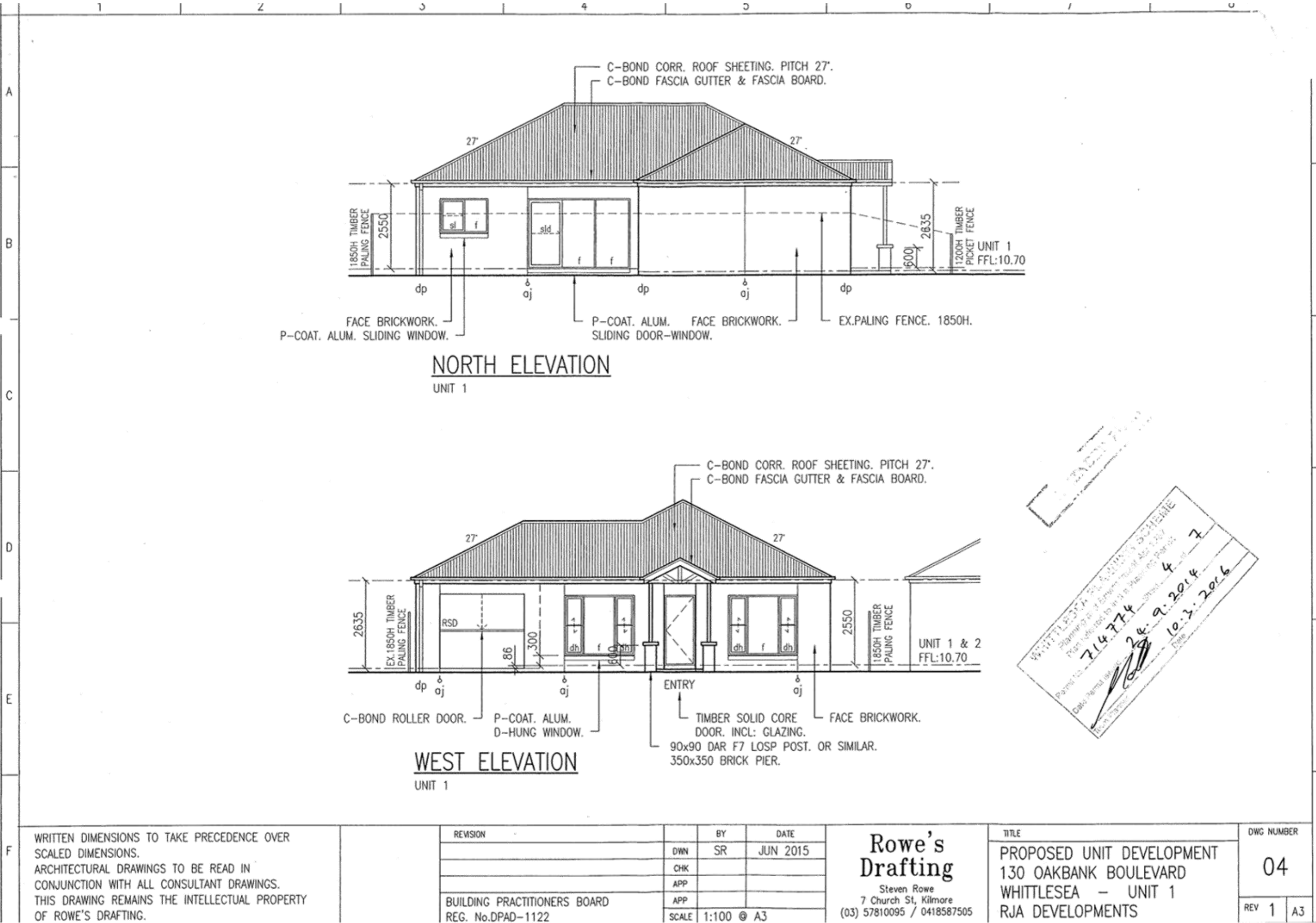


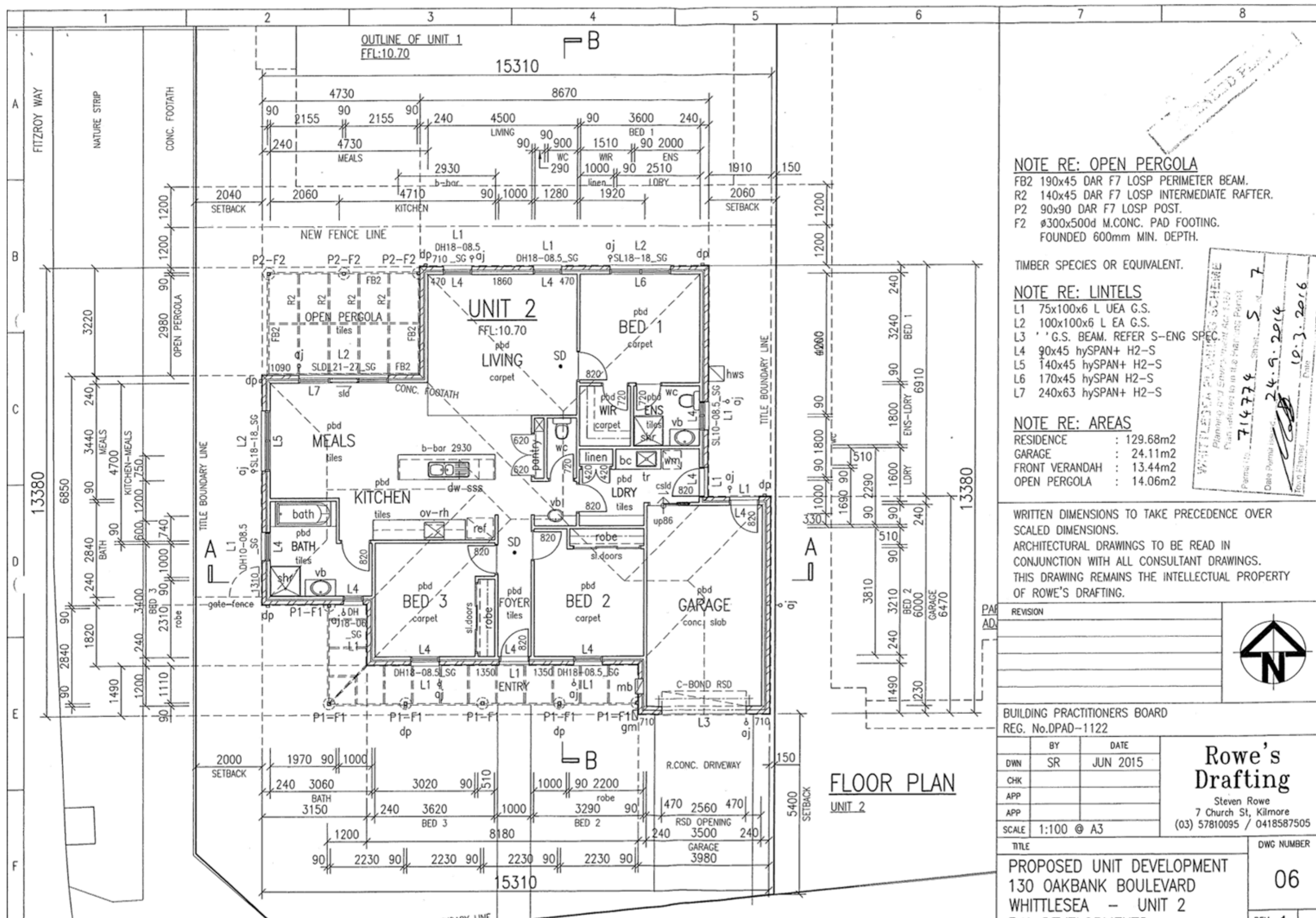






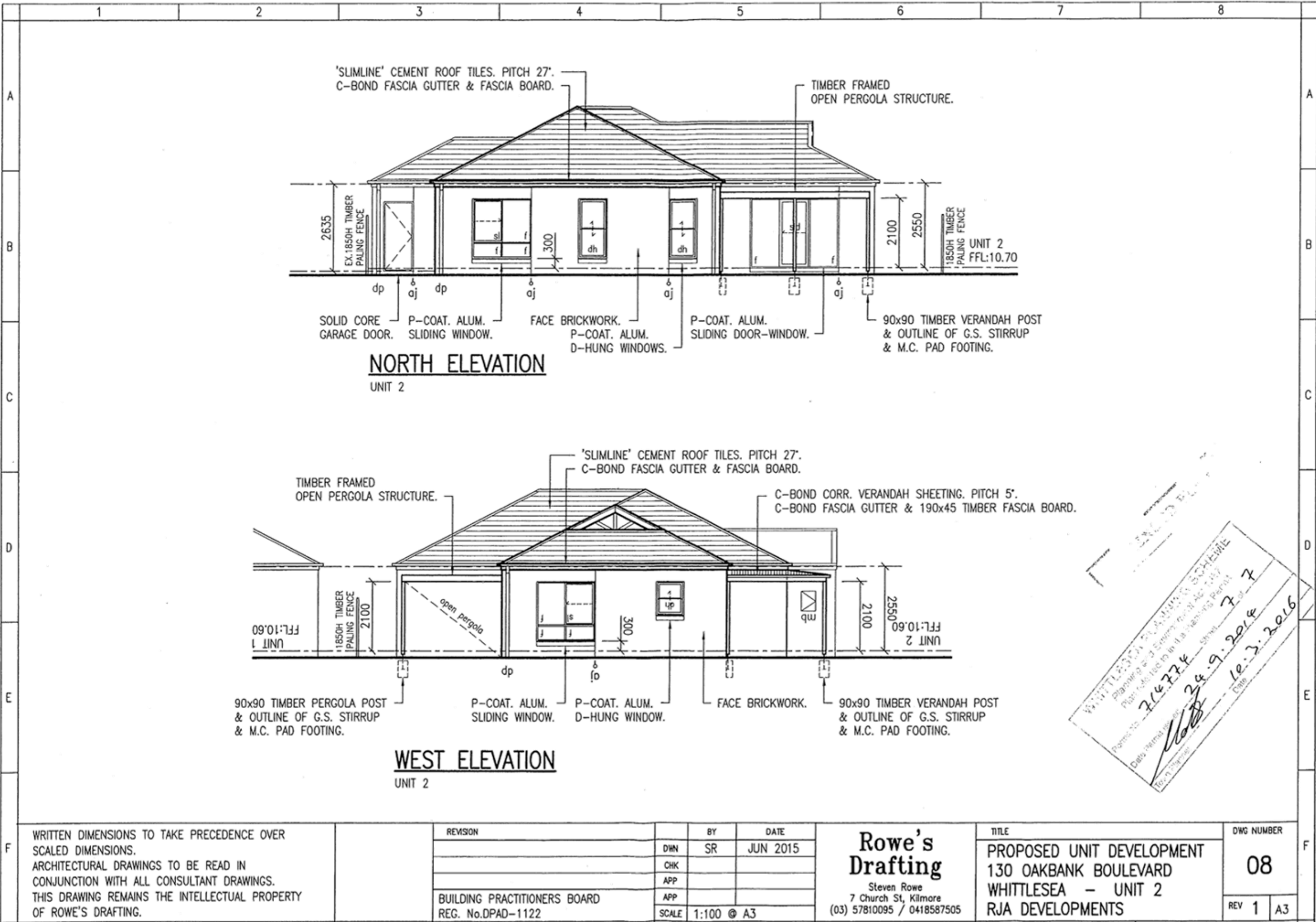














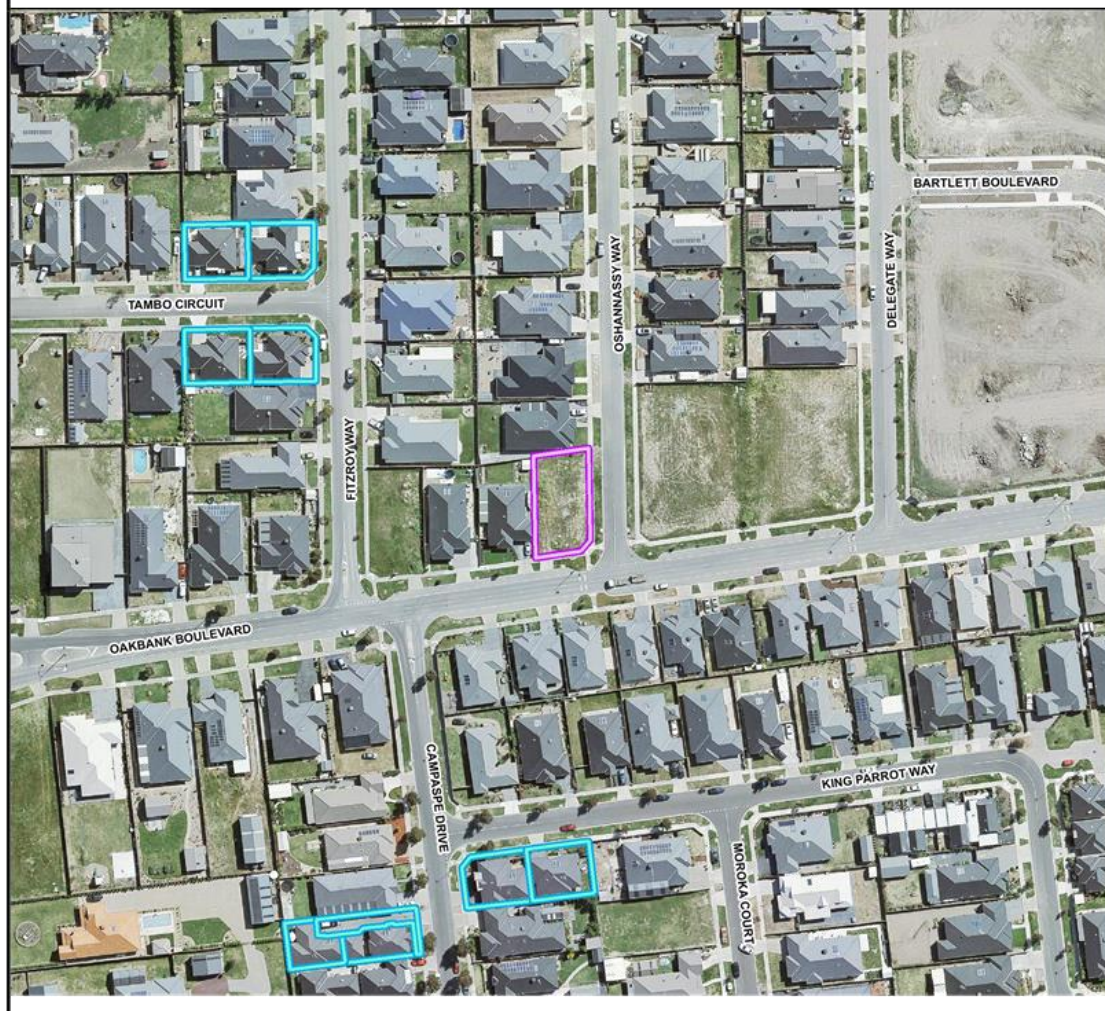




**City of  
Whittlesea**

## DEVELOPMENT ASSESSMENT REPORT

PLANNING APPLICATION NO. 714773



Subject Land

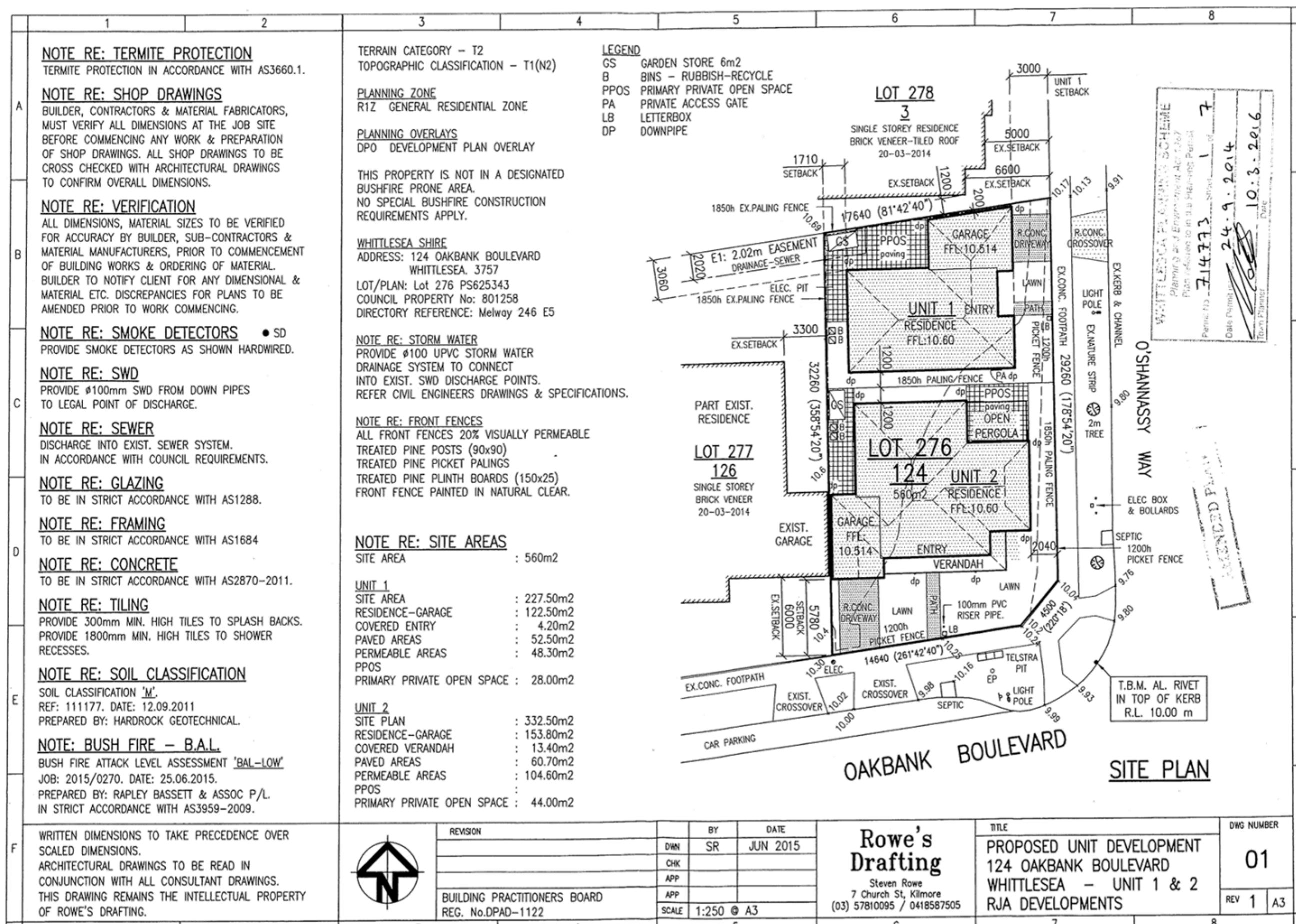


Medium Density Housing

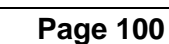


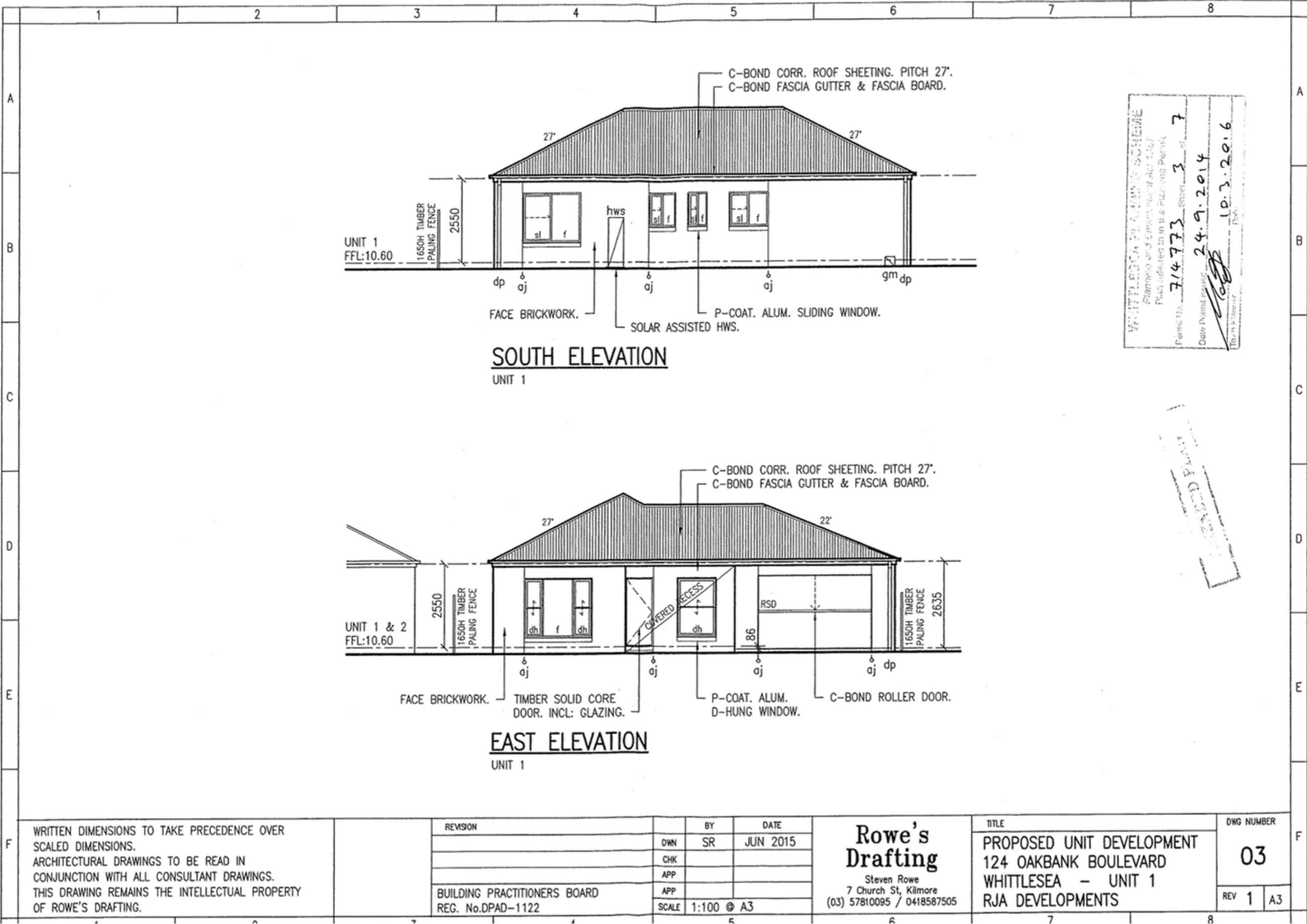
City of  
Whittlesea

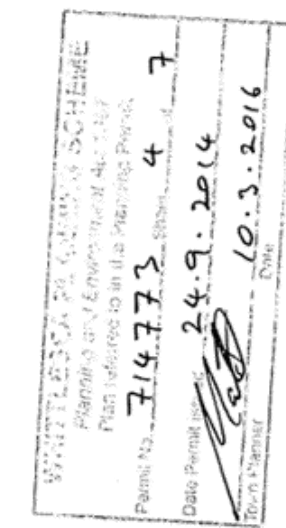
DEVELOPMENT ASSESSMENT REPORT



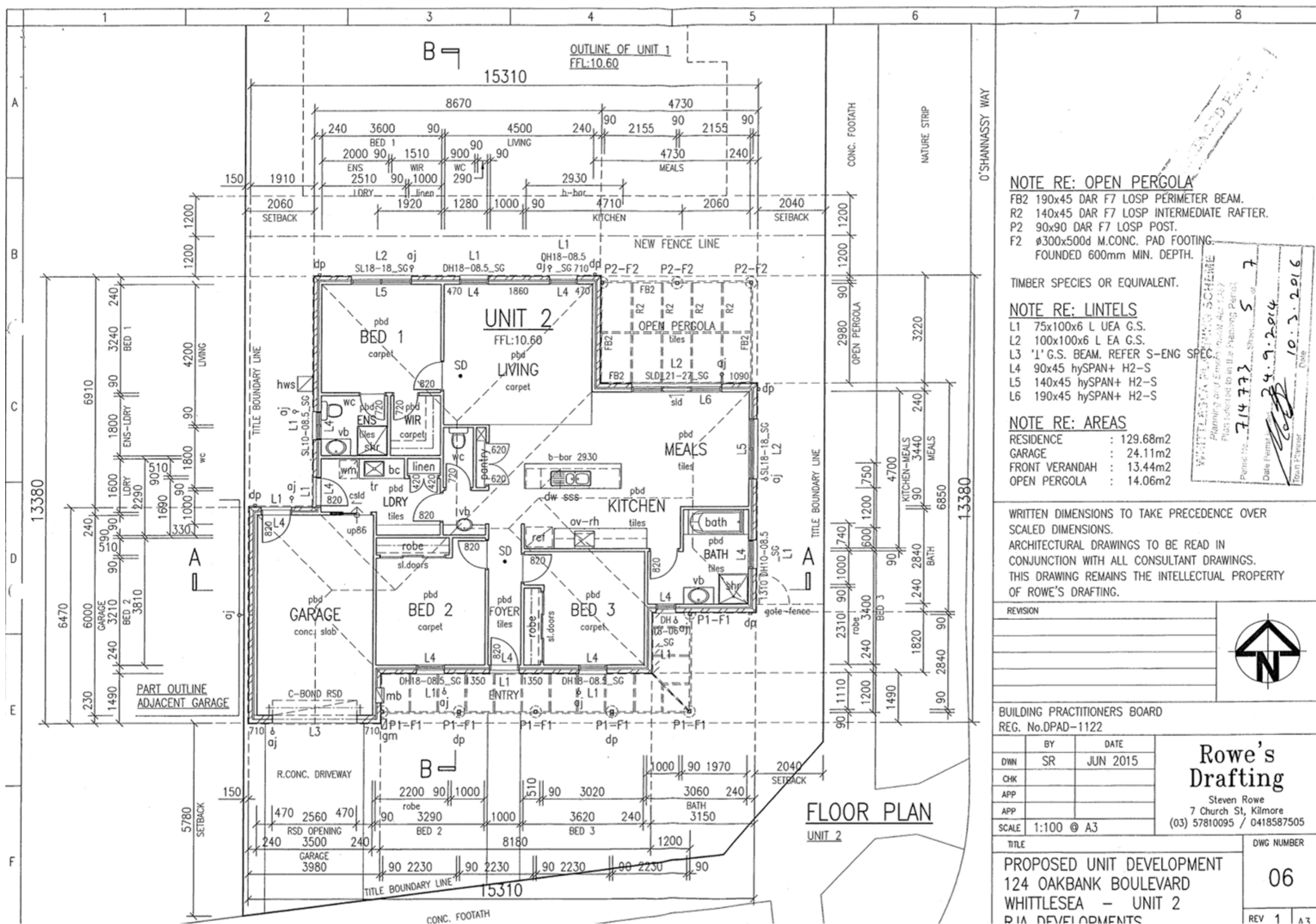


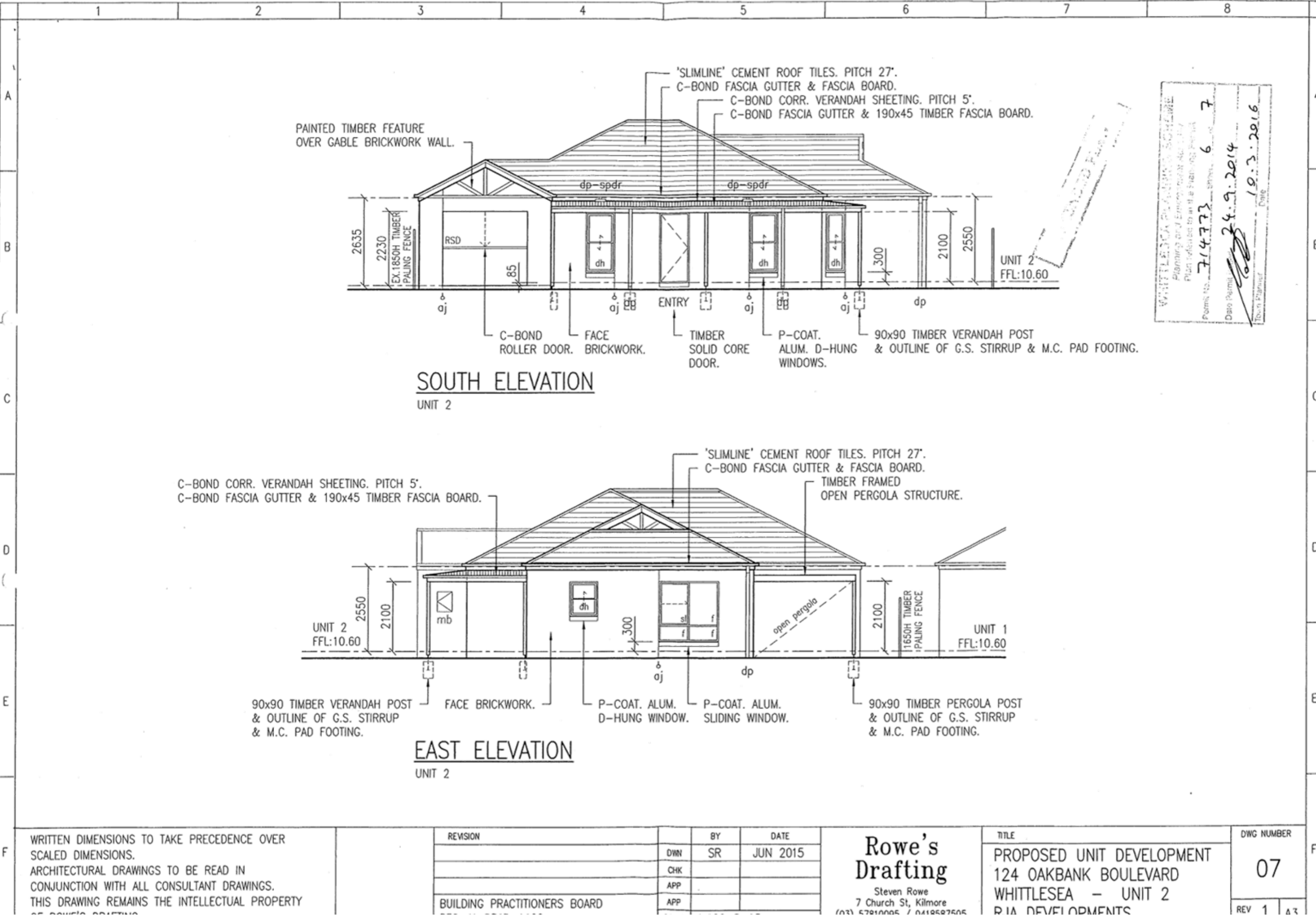
















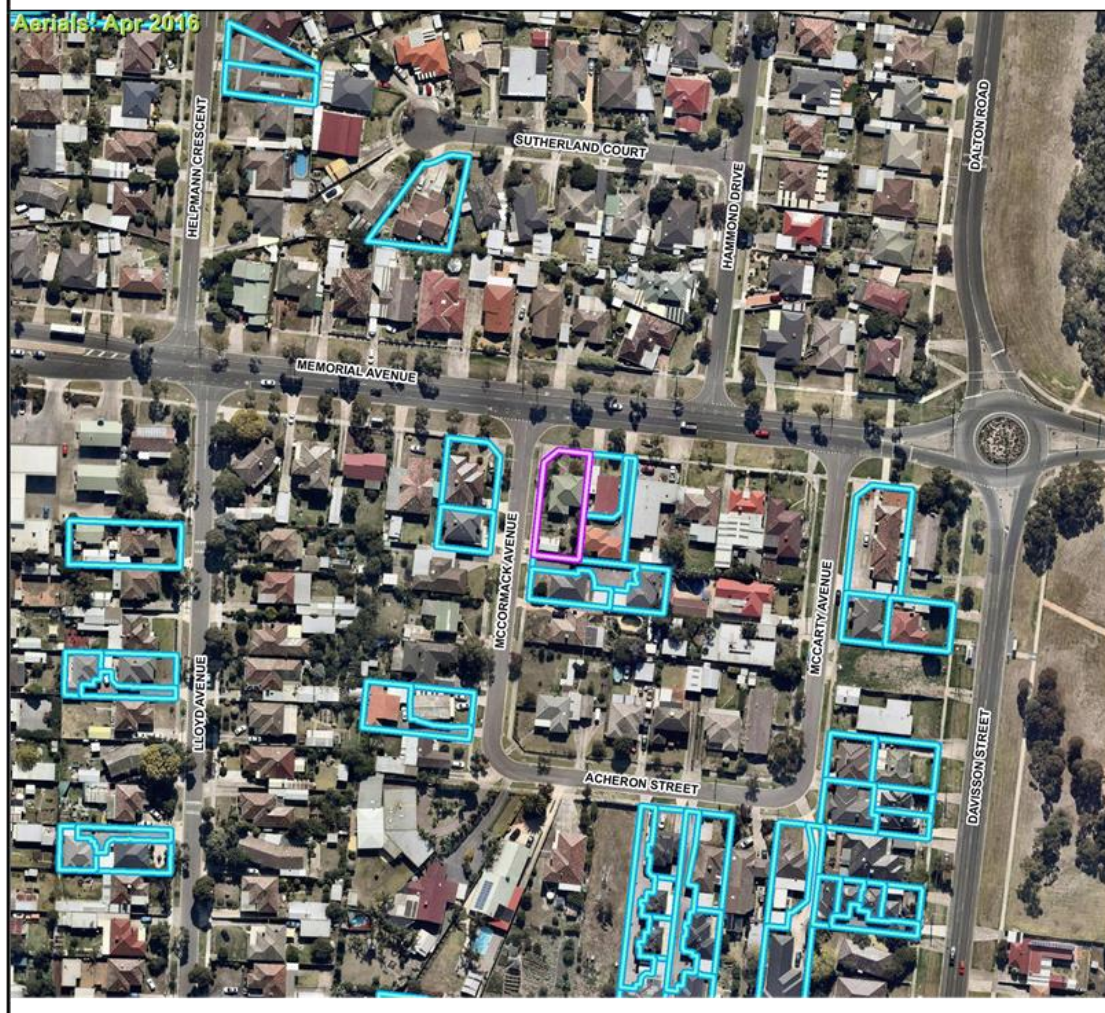




**City of  
Whittlesea**

## DEVELOPMENT ASSESSMENT REPORT

PLANNING APPLICATION NO. 714800



Subject Land



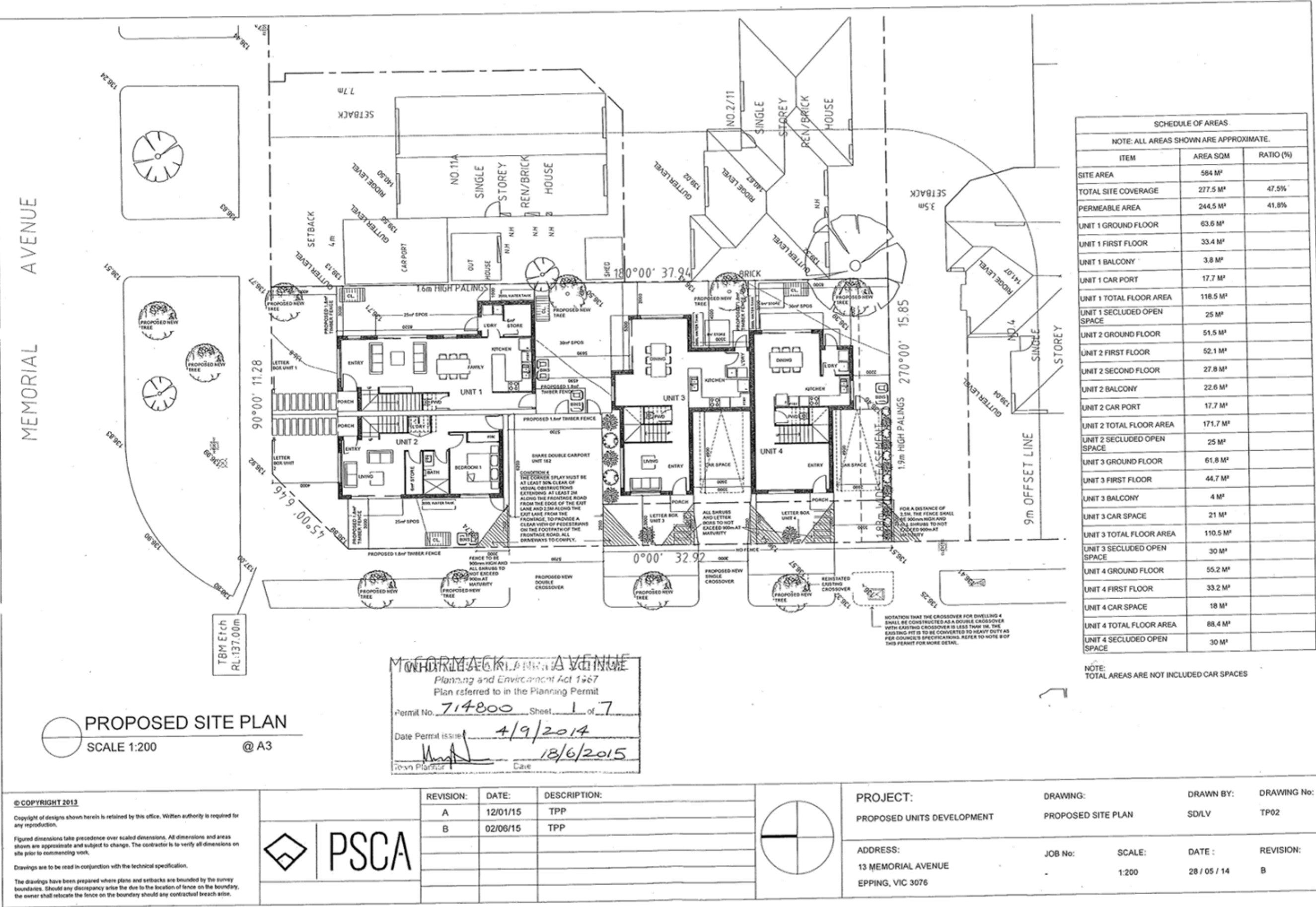
Medium Density Housing

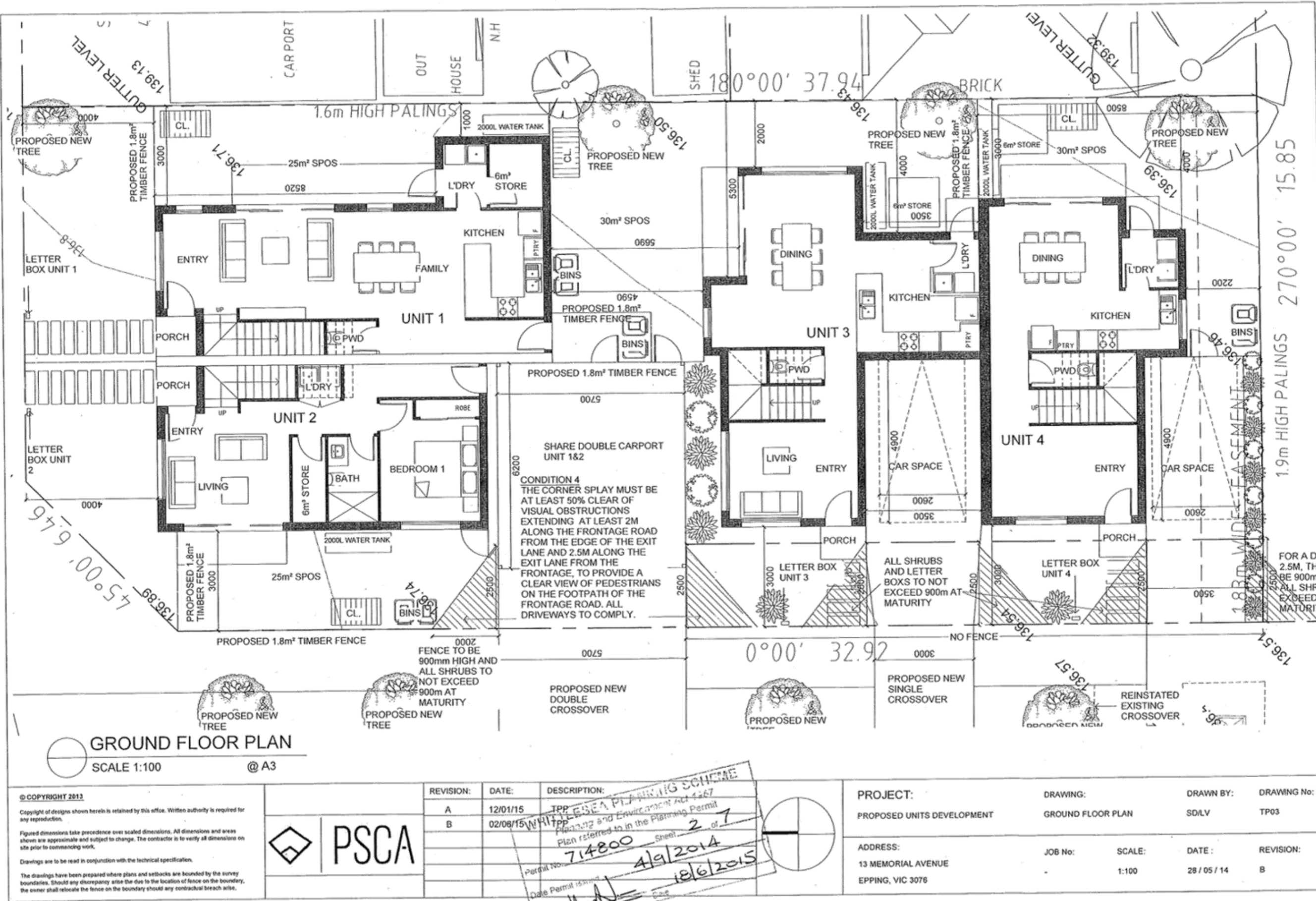


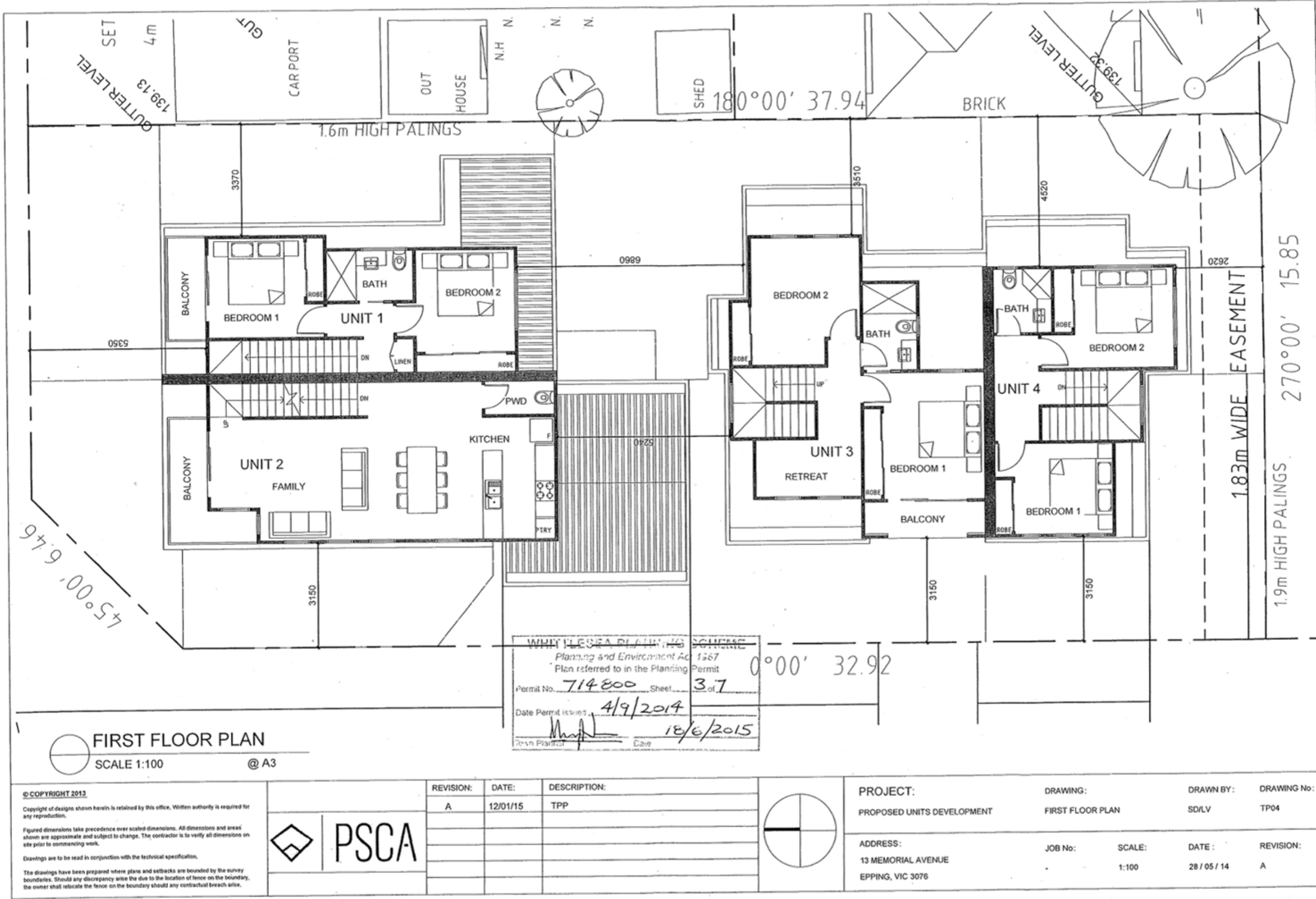
City of  
Whittlesea

DEVELOPMENT ASSESSMENT REPORT

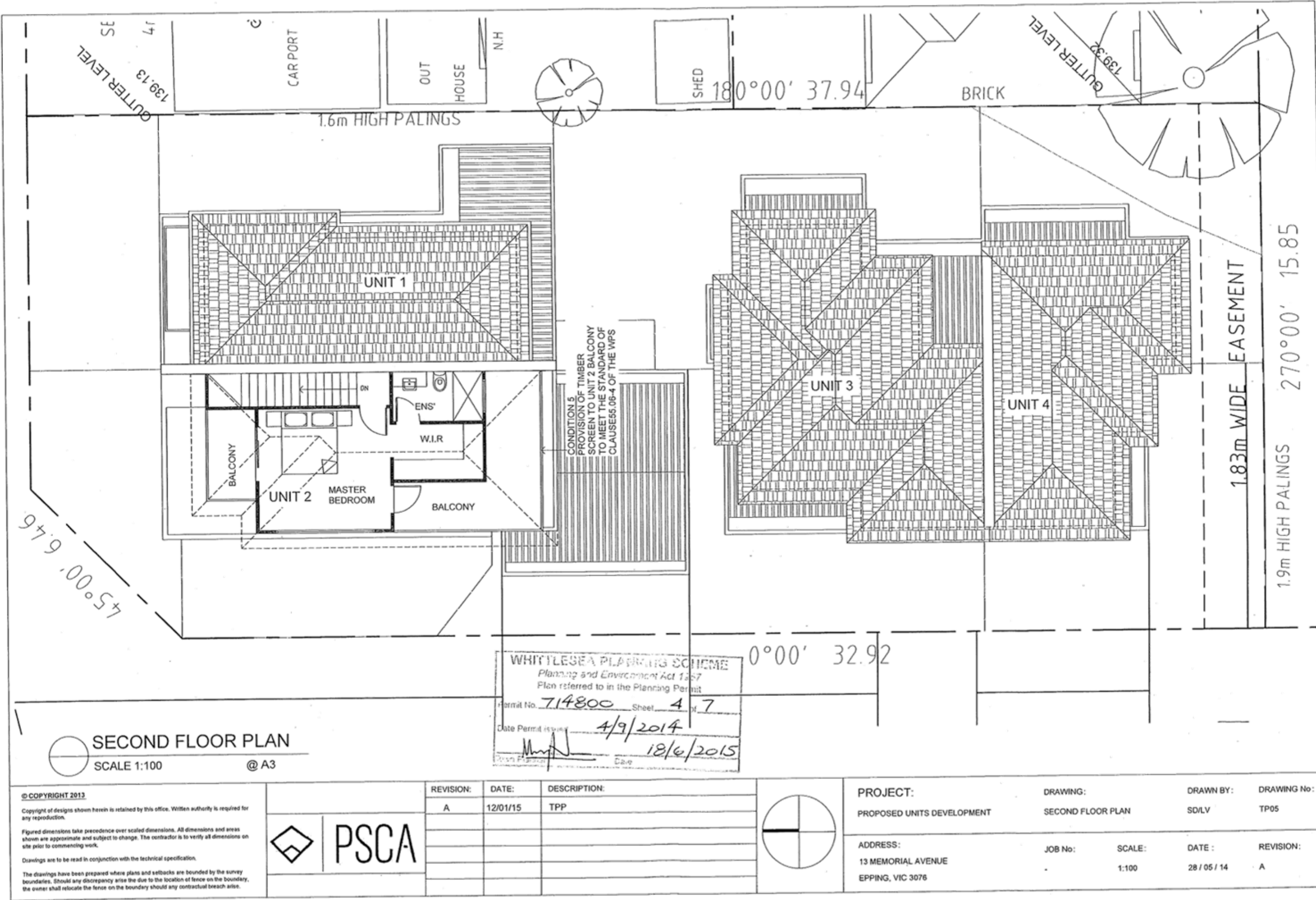


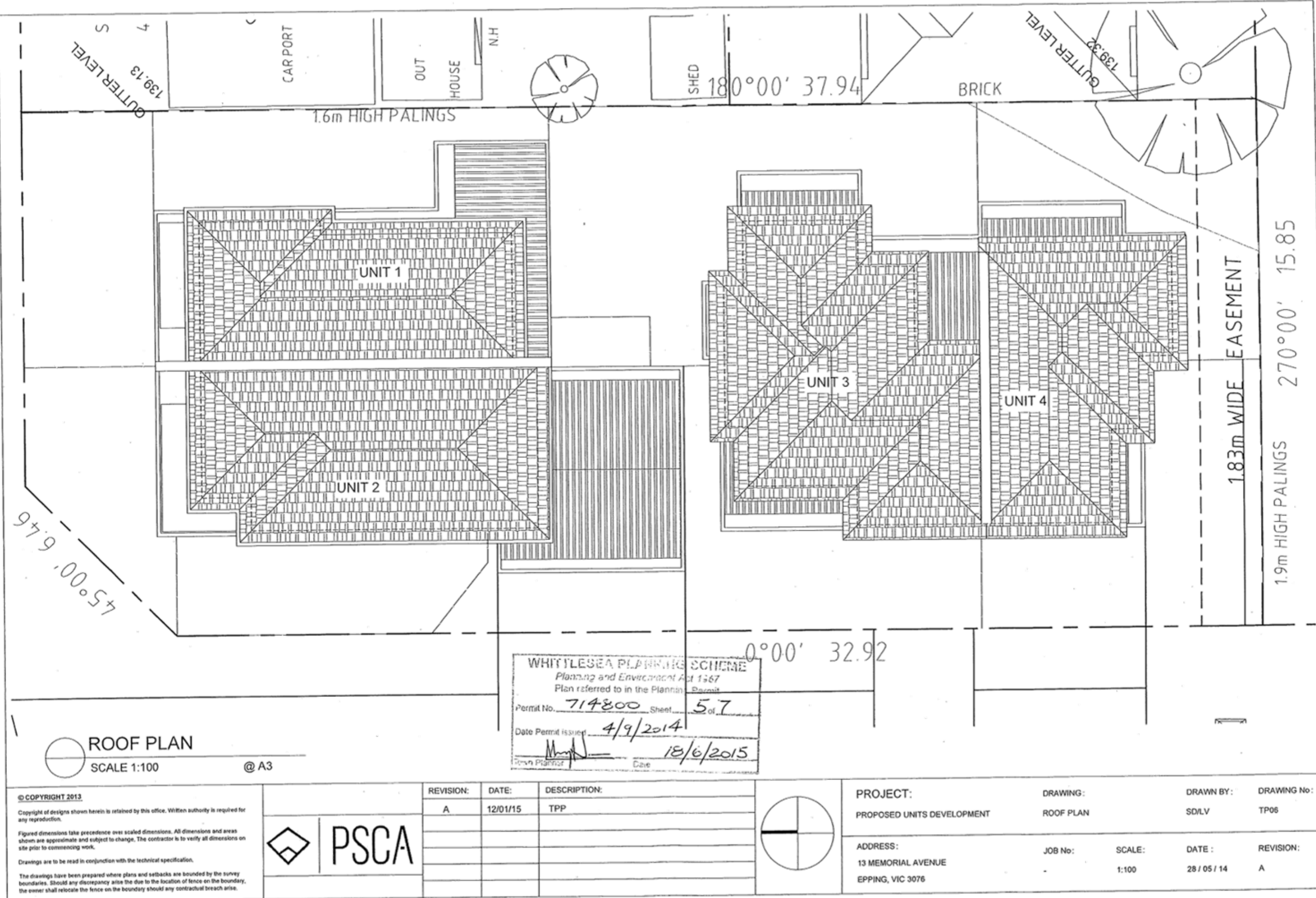


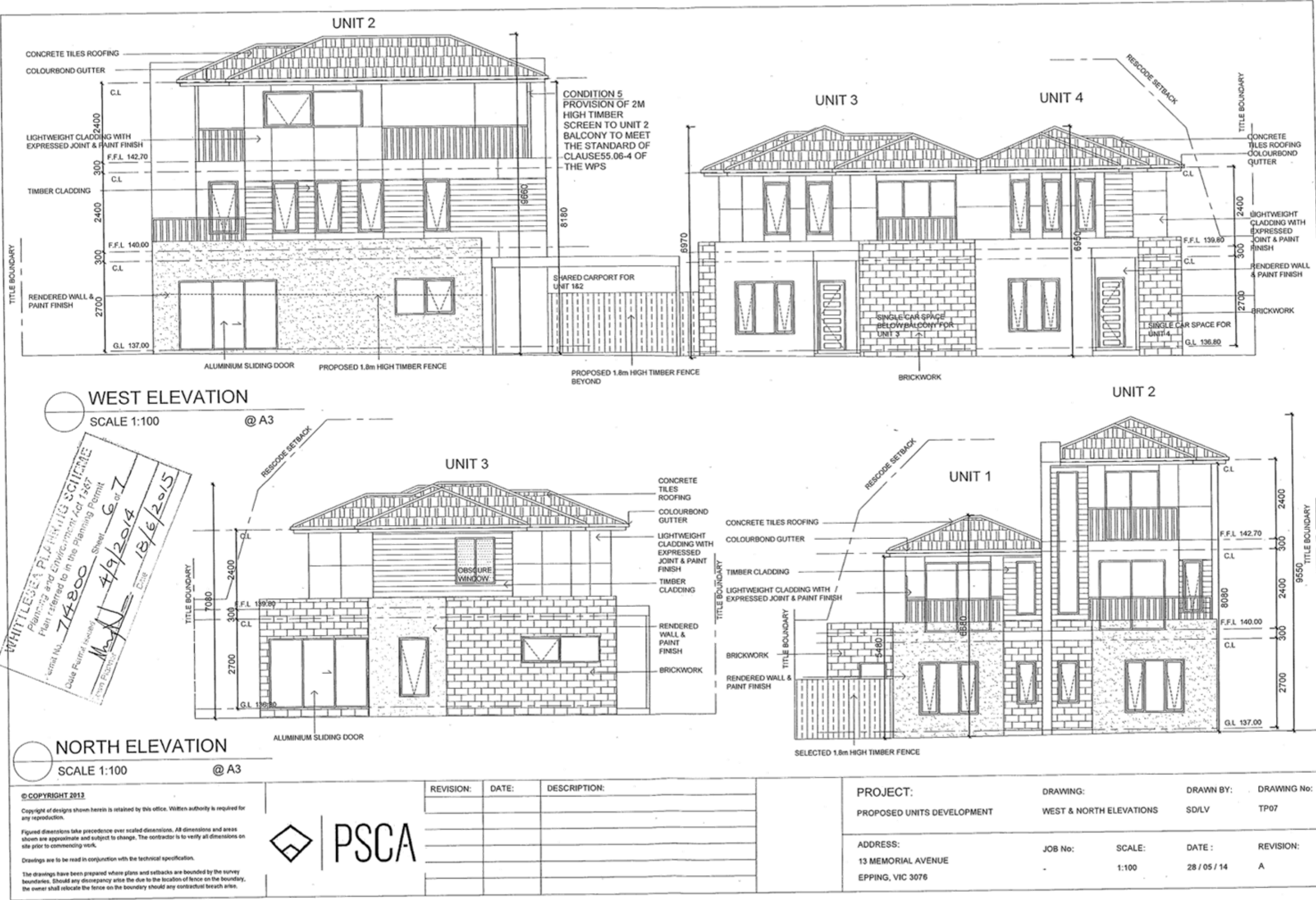




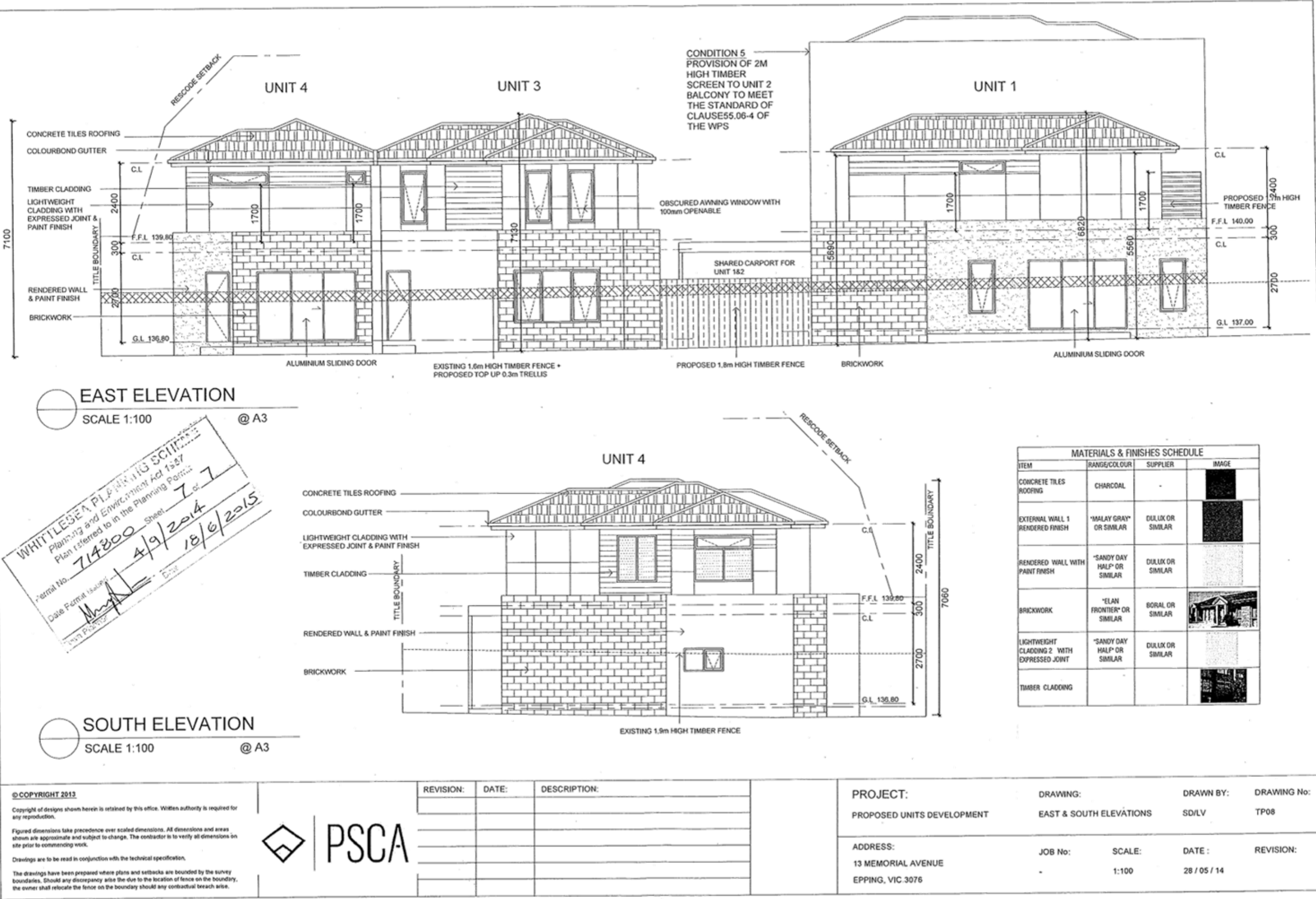




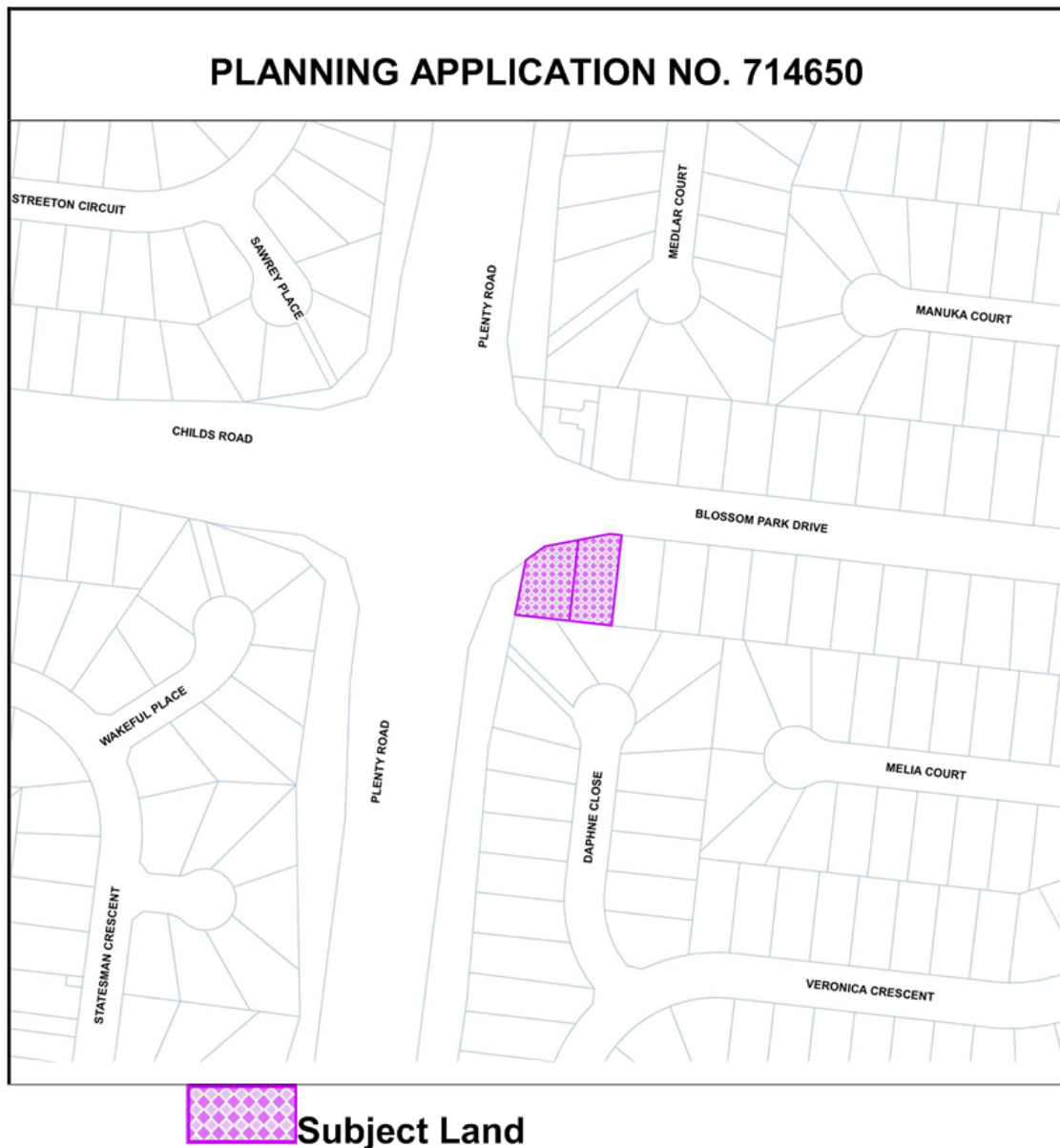










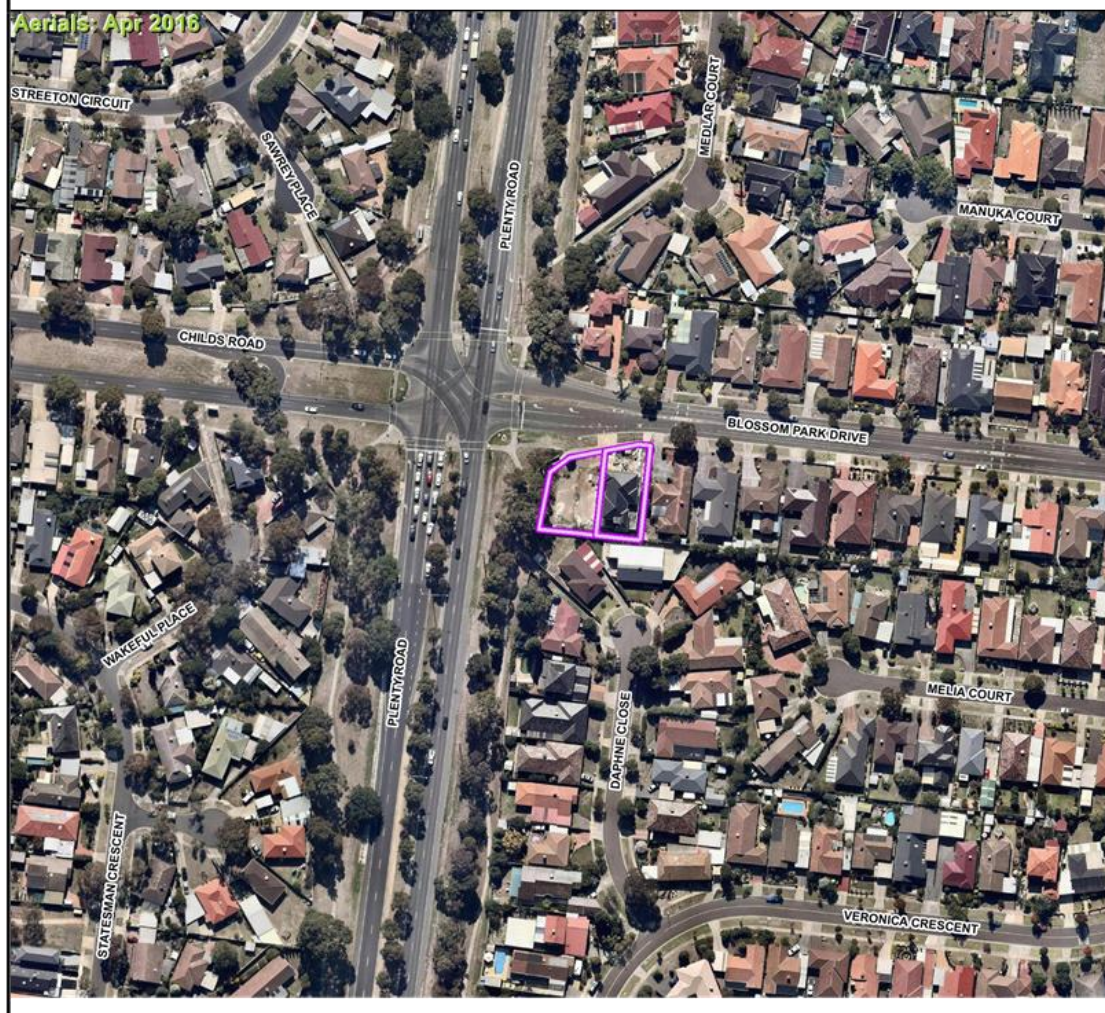


**City of  
Whittlesea**

## DEVELOPMENT ASSESSMENT REPORT



# PLANNING APPLICATION NO. 714650

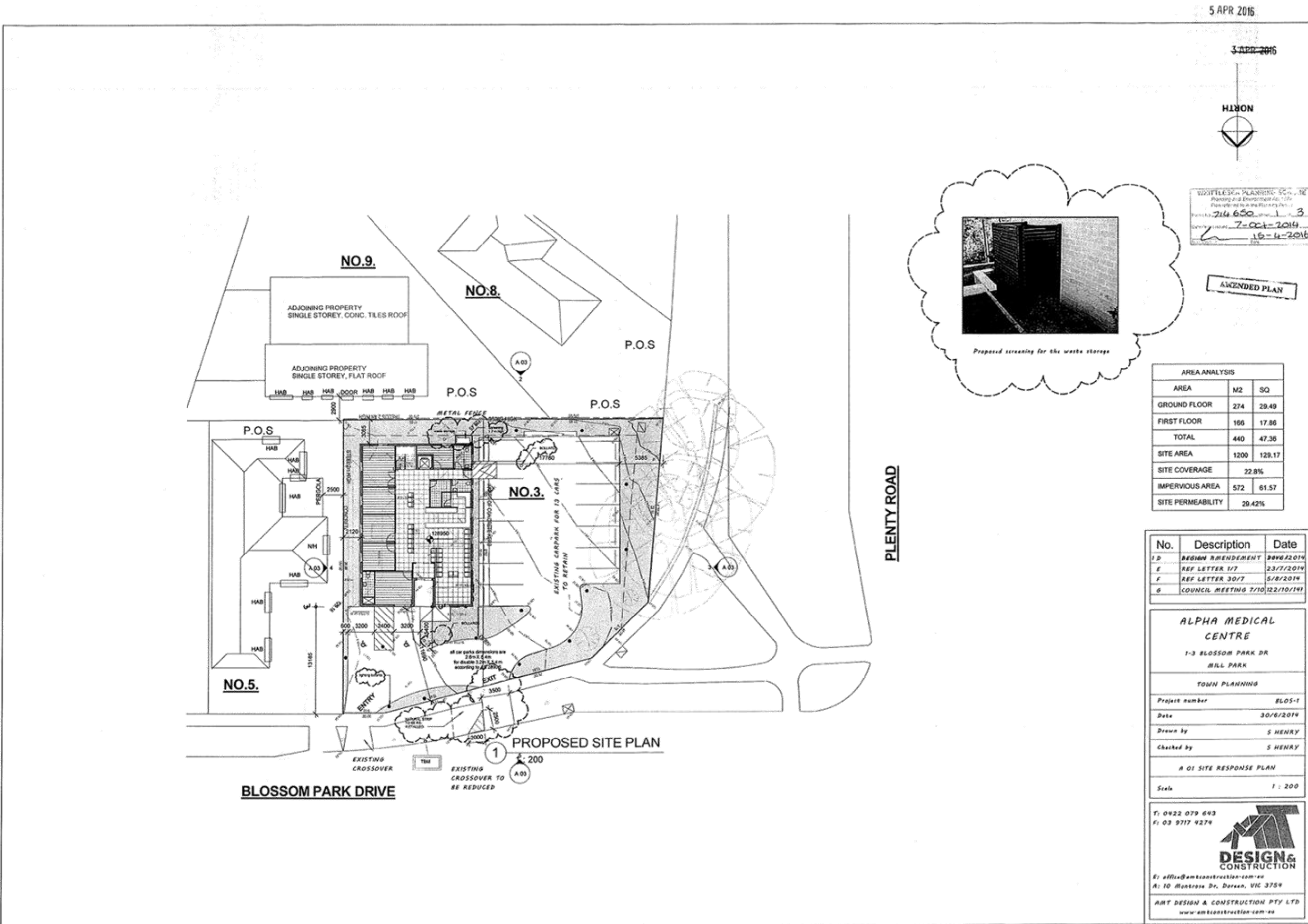


 Subject Land

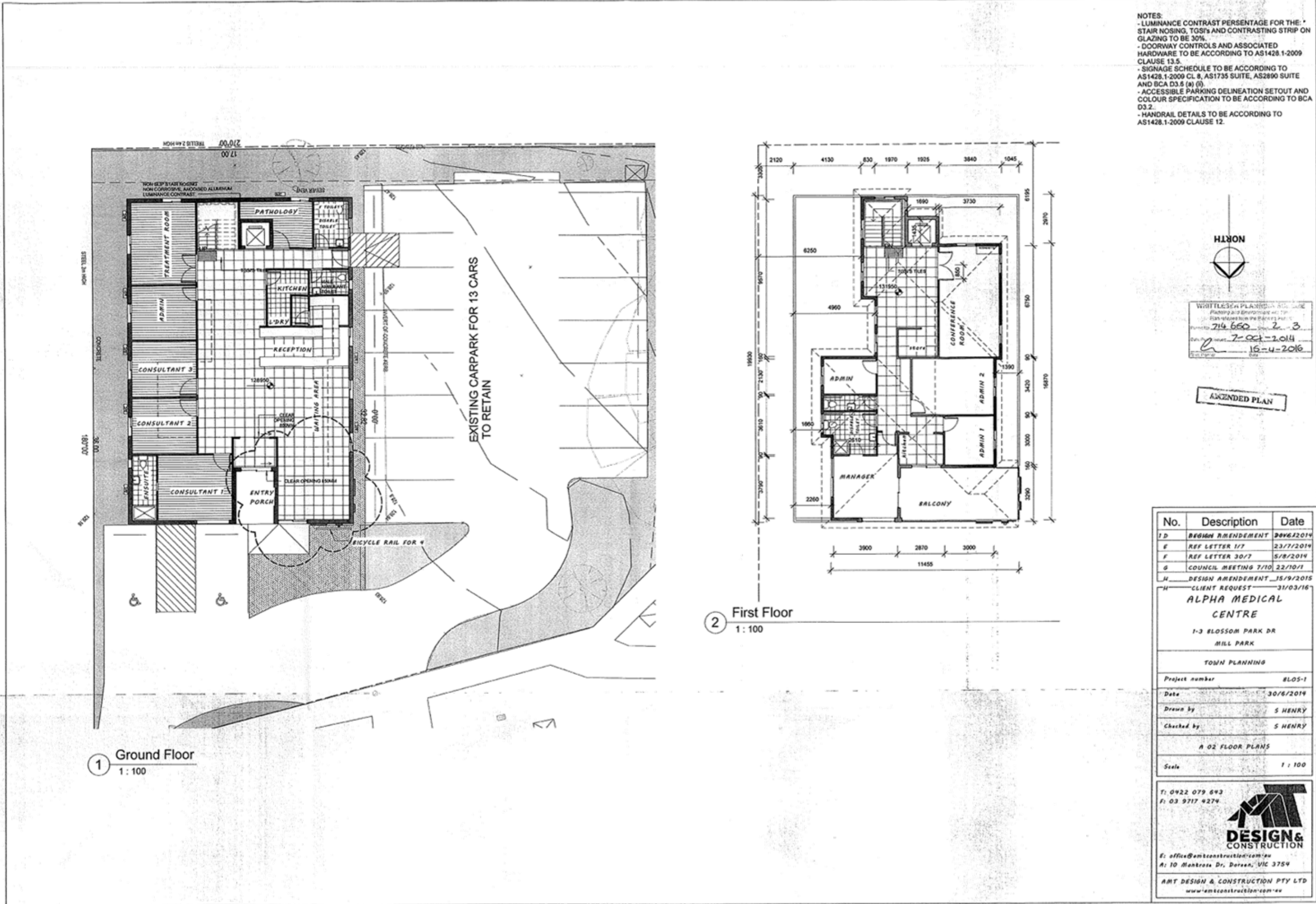


City of  
Whittlesea

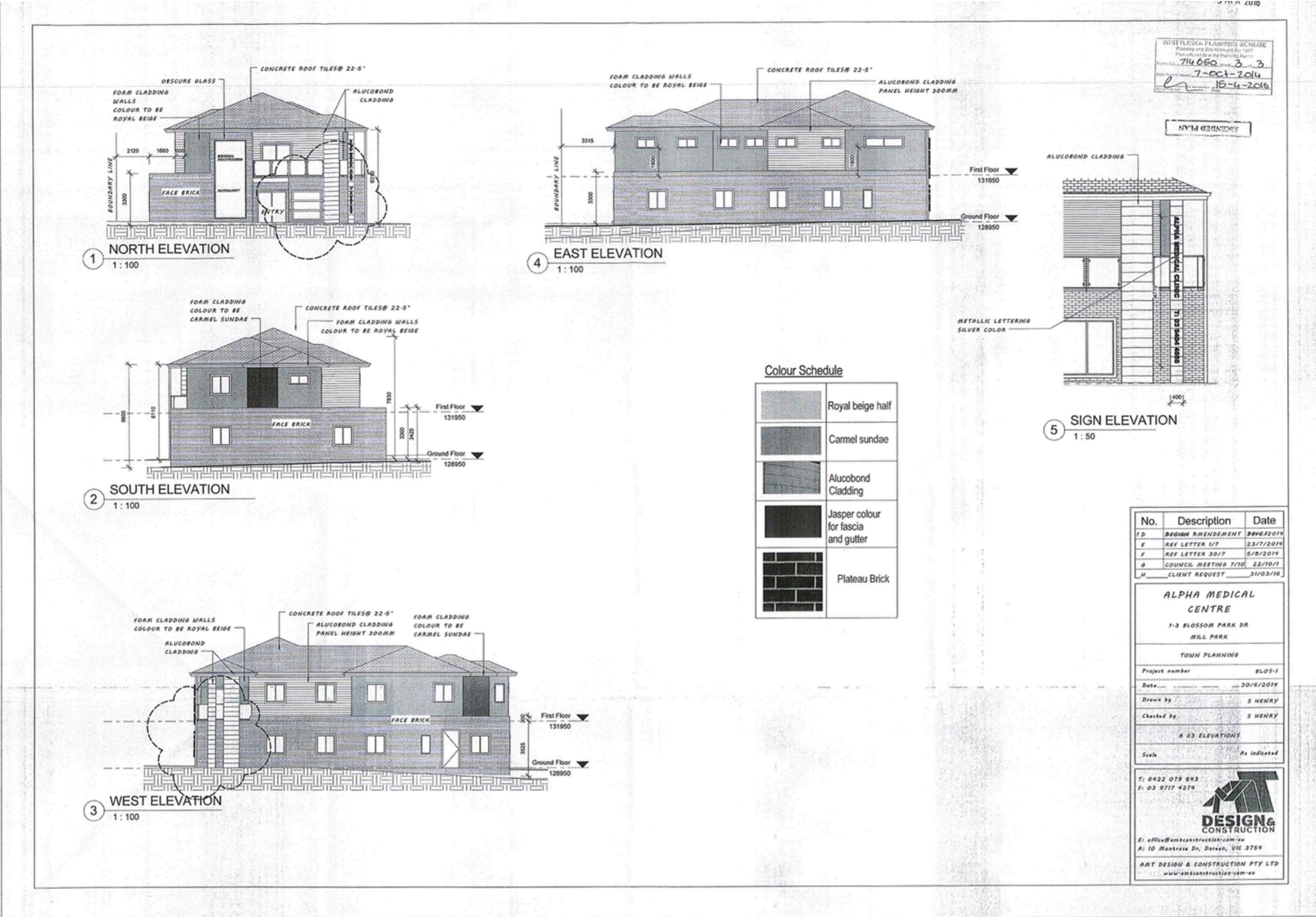
## DEVELOPMENT ASSESSMENT REPORT















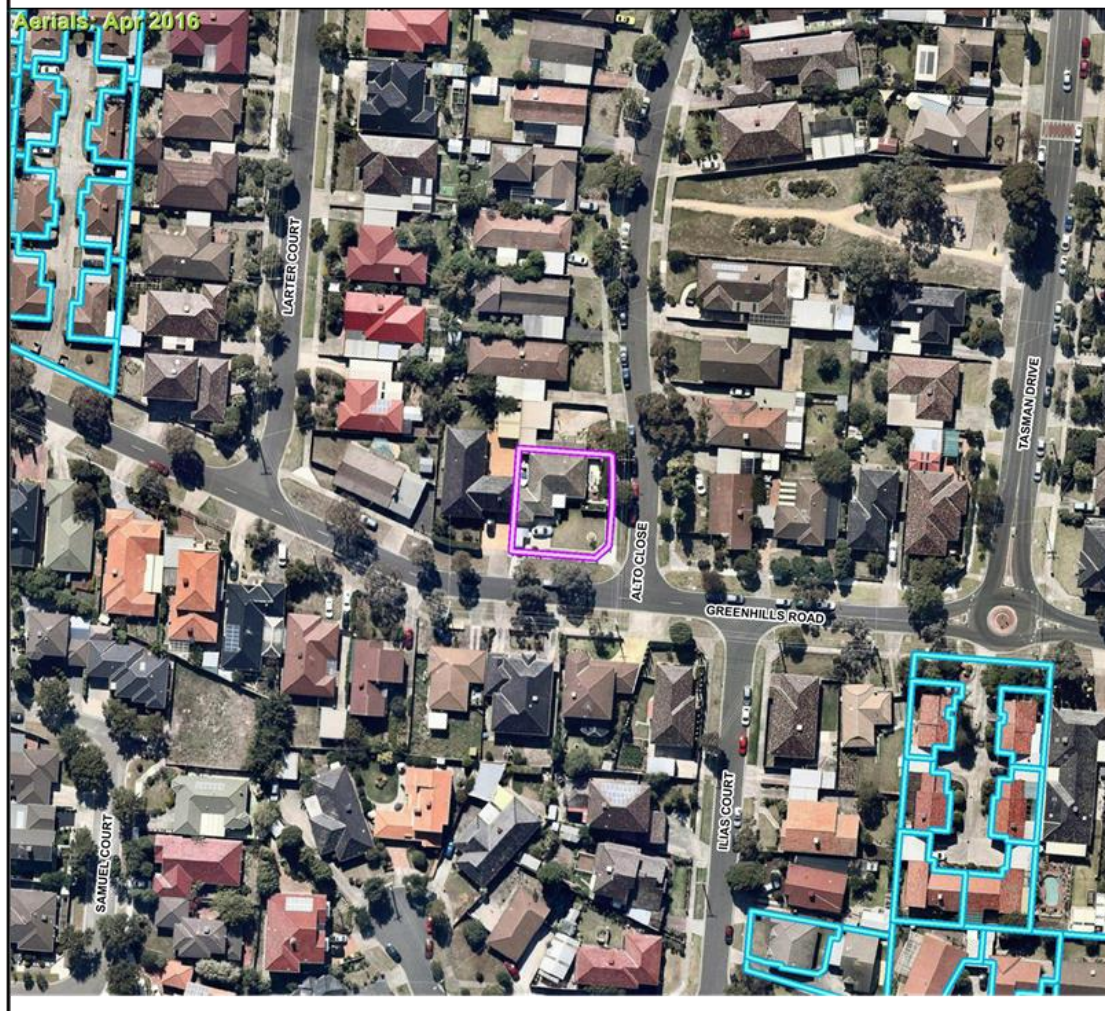


**City of  
Whittlesea**

**DEVELOPMENT ASSESSMENT REPORT**



# PLANNING APPLICATION NO. 709913



Subject Land

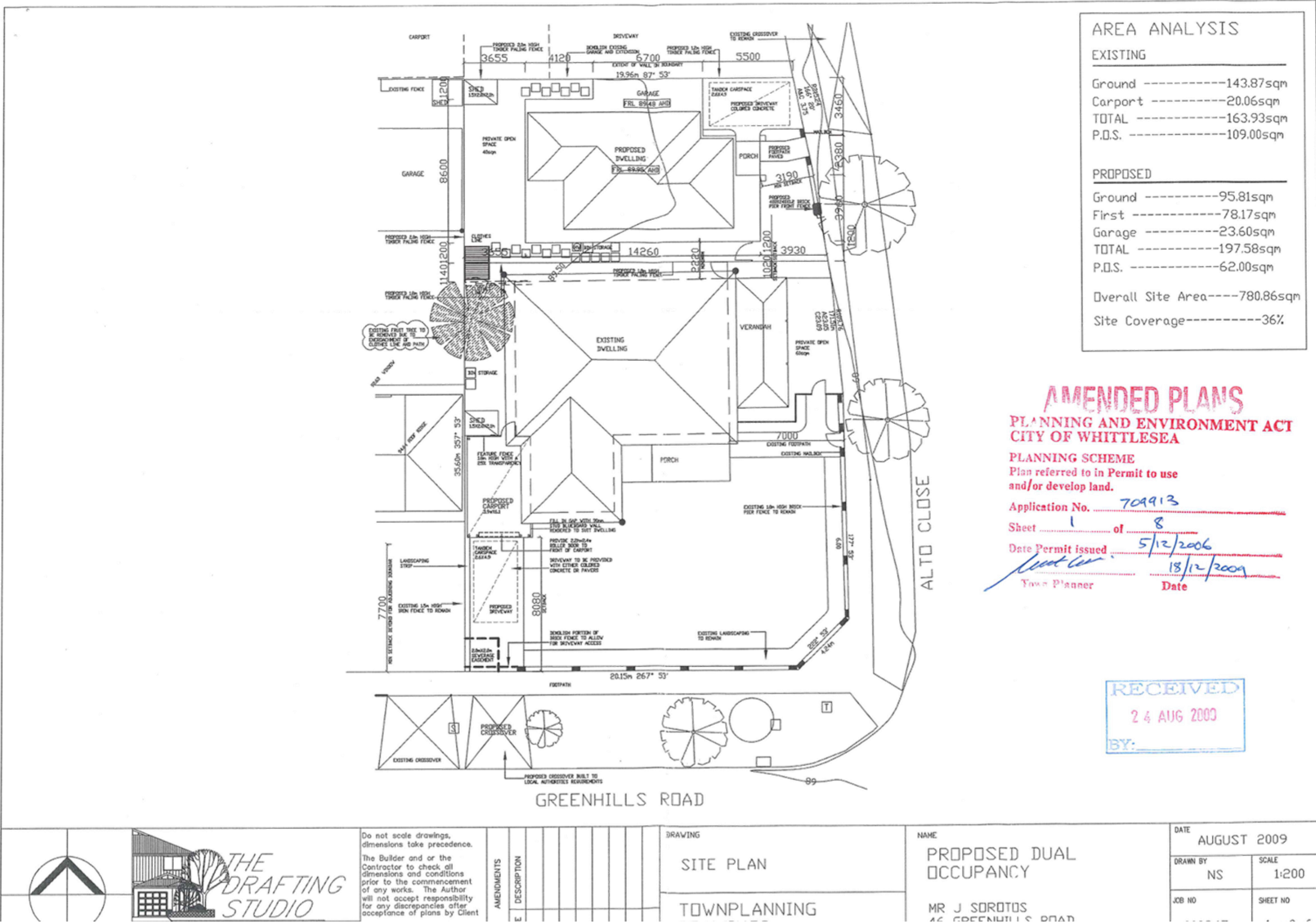


Medium Density Housing

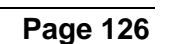


City of  
Whittlesea

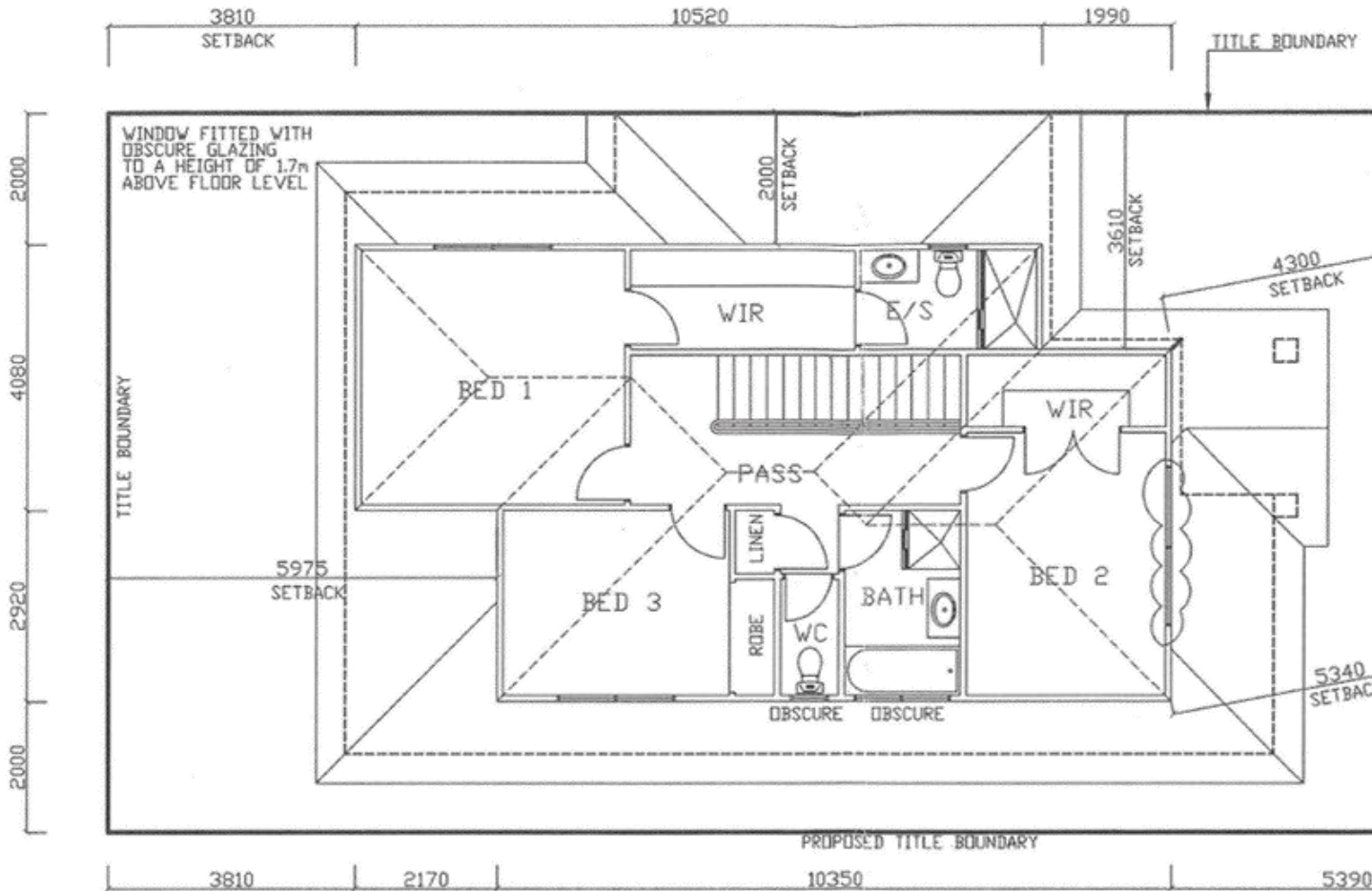
## DEVELOPMENT ASSESSMENT REPORT











**AMENDED PLANS**  
**PLANNING AND ENVIRONMENT ACT**  
**CITY OF WHITTLESEA**  
**PLANNING SCHEME**  
Plan referred to in Permit to use  
and/or develop land.  
Application No. 709913  
Sheet 4 of 8  
Date Permit issued 5/12/2006  
18/12/2009  
Town Planner Date

FIRST FLOOR PLAN

RECEIVED  
26 AUG 2009



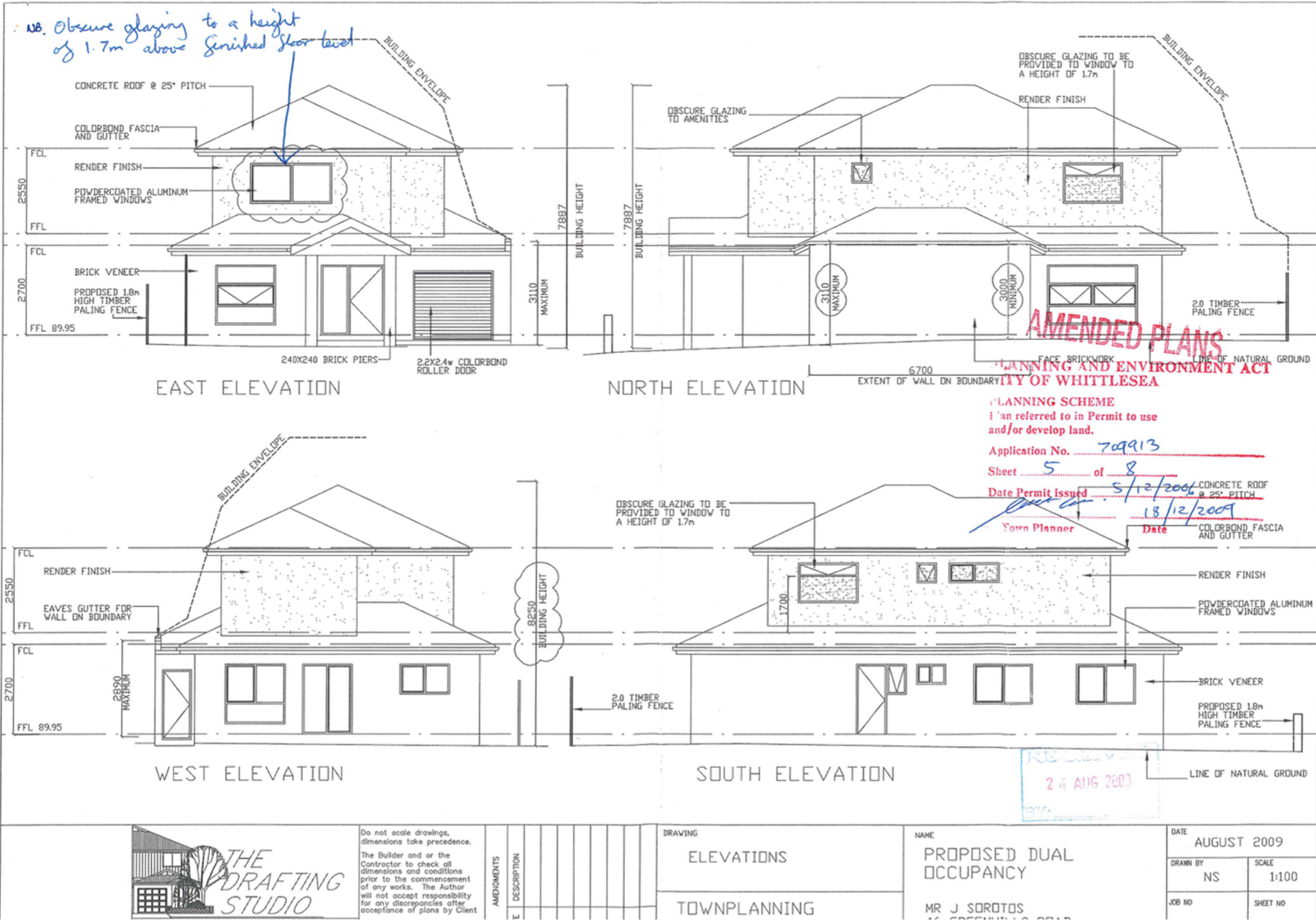
Do not scale drawings, dimensions take precedence.  
The Builder and or the Contractor to check all dimensions and conditions prior to the commencement of any works. The Author will not accept responsibility for any discrepancies after acceptance of plans by Client

AMENDMENTS	DESCRIPTION	DATE

DRAWING  
PROPOSED FIRST FLOOR PLAN  
TOWNPLANNING

NAME  
PROPOSED DUAL OCCUPANCY  
MR J SOROTOS  
46 COLEMAN ROAD

DATE	AUGUST 2009
DRAWN BY	NS
SCALE	1:100
JOB NO	SHEET NO





Page 129



© COPYRIGHT IN THIS DRAWING AND ALL WORK EXECUTED FROM IT REMAINS THE PROPERTY OF THE DESIGNER. CONTRACTORS MUST SET OUT ALL WORK AND VERIFY ALL CONDITIONS, LEVELS AND DIMENSIONS ON SITE PRIOR TO COMMENCEMENT OF ANY WORK. OR MAKING OF ANY SHOP DRAWINGS. ALL WORK MUST BE EXECUTED IN ACCORDANCE WITH THE RULES, BYLAWS, REGULATIONS AND REQUIREMENTS OF ALL AUTHORITIES HAVING JURISDICTION OVER ANY PART OF THE WORK.

**WHITTLESEA PLANNING SCHEME**  
 Planning and Environment Act 1987  
 Plan referred to in the Planning Permit  
 Permit No. 709913 Sheet 1 of 2  
 Date Permit issued 5 DECEMBER 2006  
 Town Planner [Signature] Date 04/03/15

**MATERIAL / FINISHES SCHEDULE**  
 WALLS - FC SHEET - DULUX - BOUTIQUE STONE  
 BRICK - BORAL - FLAME RED, PRESSED CLAY  
 BRICKS WITH OF WHITE MORTAR JOINTS  
 FASCIA - COLORBOND - CLASSIC CREAM  
 GUTTERS - COLORBOND - CLASSIC CREAM  
 WINDOWS - ALUMINIUM - CLASSIC CREAM  
 CONCRETE TILES - BORAL - CHARCOAL GREY (MACQARIC)

**PROPOSED SOUTH ELEVATION**  
 1:100 @ A3

**PROPOSED NORTH ELEVATION**  
 1:100 @ A3

**PROPOSED EAST ELEVATION**  
 1:100 @ A3

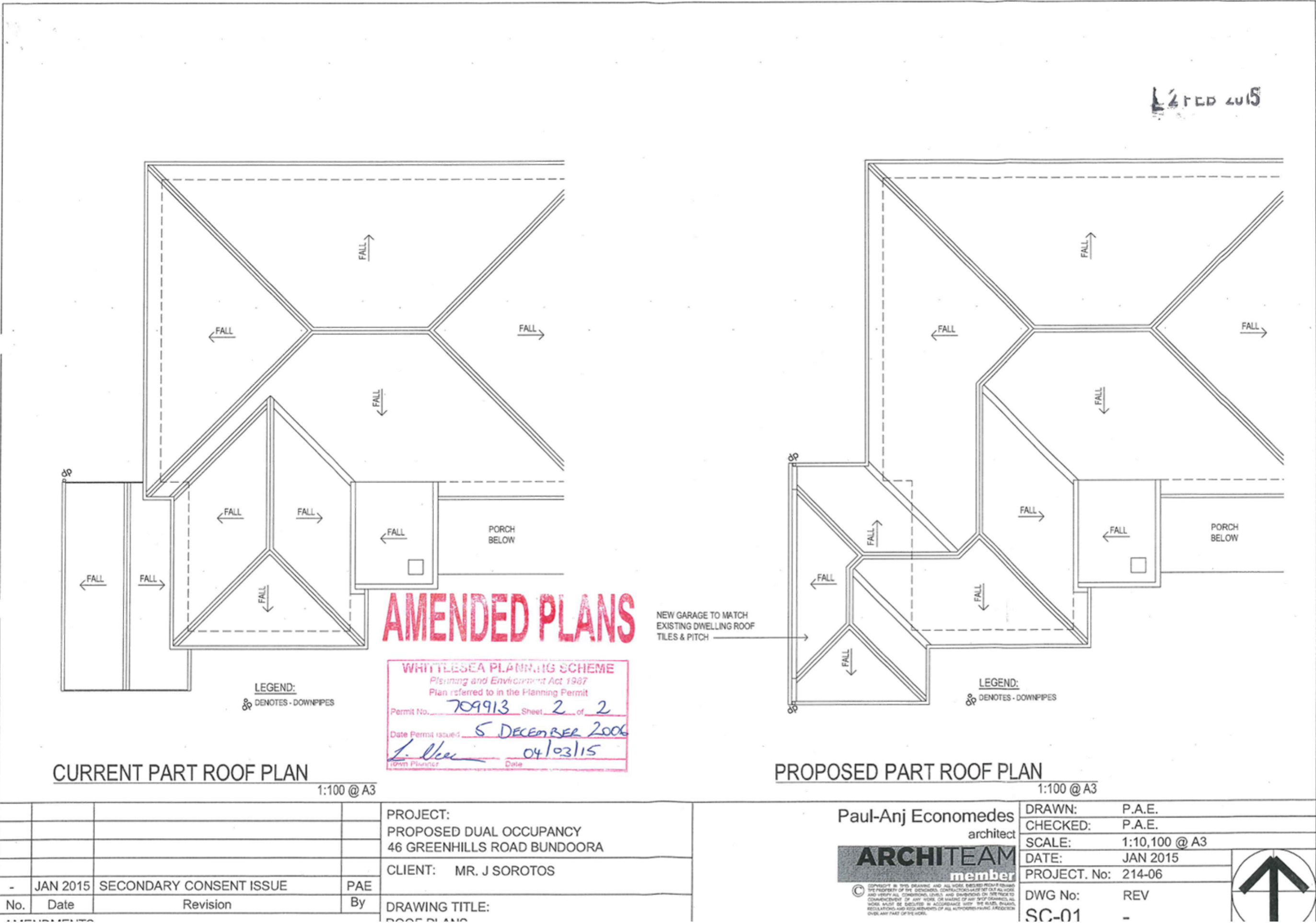
**DOMESTIC NOTES:**  

THE SITE SHALL BE ADEQUATELY FENCED OFF DURING CONSTRUCTION TO PREVENT PUBLIC ACCESS  
 NO PART OF THE BUILDING SHALL PROJECT BEYOND THE BUILDING LINE I.e.: INCLUDING MOLDS ARCHITECTURAL FEATURES AND THE LIKE  
 THE PREMISES SHALL TREAT FOR TERMITE PROTECTION USING A CHEMICAL BARRIER IN ACCORDANCE WITH AS 3680. ALTERNATIVELY PHYSICAL BARRIERS MAY BE ADOPTED, SUCH AS ANT CAPES OR GRANITGARD. IF ADOPTING ANT CAPS ENSURE THERE IS SUB FLOOR CLEARANCE OF 400mm  
 EAVES WITH 900mm (BUT NO CLOSER THAN 450mm) OF THE BOUNDARY SHALL BE CONSTRUCTED USING NON COMBUSTIBLE MATERIAL  
 EAVES EXPOSED WITHIN 450mm OF THE BOUNDARY SHALL BE PROTECTED BY EITHER A WING WALL OR CORBELLED BRICKWORK  
 RAINWATER HEADS, FASCIAS, GUTTERS, DOWN PIPES AND THE LIKE WITHIN 450mm OF THE BOUNDARY SHALL BE CONSTRUCTED OF NON COMBUSTIBLE MATERIALS  
 OPENINGS IN EXTERNAL WALLS WITHIN 900mm FROM THE SIDE FACING THE BOUNDARY REQUIRE TO BE PROTECTED I.e.: WINDOWS SHALL BE FIXED, FIRE RATED WINDOWS NO LESS THAN 400-  
 REFER TO ENERGY RATING REPORT FOR THERMAL INSULATION REQUIREMENTS  
 OPENABLE WINDOWS WITHIN 1 METER OF THE FINISHED FLOOR LEVEL SHALL BE RESTRICTED SO THAT THE OPENINGS DO NOT PERMIT A 125mm SPHERE TO PASS THROUGH IF THE FALL IS GREATER THAN 1 METER FROM THE NATURAL GROUND LEVEL

THE DOOR TO A FULLY ENCLOSED SANITARY COMPARTMENT SHALL OPEN OUTWARDS, SLIDE OR BE READILY REMOVABLE FROM THE OUTSIDE OF THE COMPARTMENT I.e.: REMOVABLE HINGES  
 THE STUMPS ARE REQUIRED TO BE BRACED IN ACCORDANCE WITH AS 1684. IF THE HEIGHT OF THE STUMP FROM NATURAL GROUND LEVEL TO THE UNDERSIDE OF THE BEARER EXCEEDS THE RATIO OF 15 TIMES THE WIDTH OF THE STUMP I.e.: 190X100 STUMP HAS A MAX. HEIGHT OF 1500mm  
 VERTICAL ARTICULATION JOINTS MUST BE PROVIDED IN UN-REINFORCED MASONRY WALLS EXCEPT WALLS BUILT WHERE THE SITE SOIL CLASSIFICATION IS A OR S. ARTICULATION JOINTS MUST HAVE A WIDTH NOT NOT LESS THAN 10mm AND BE PROVIDED  
 IN STRAIGHT, CONTINUOUS WALLS HAVING NO OPENING, AT NOT MORE THAN 6m CENTERS AND NOT CLOSER THAN THE HEIGHT OF THE WALL AWAY FROM CORNERS AND WHERE THE HEIGHT OF THE WALL CHANGES BY MORE THAN 20% AT THE POSITION OF CHANGE IN HEIGHT AND WHERE OPENINGS MORE THAN 900X900mm OCCUR AT NOT MORE THAN 6m CENTERS AND POSITIONED IN LINE WITH ONE OF THE OPENINGS AND WHERE WALL CHANGE IN THICKNESS AND AT CONTROL OR CONSTRUCTION JOINTS IN FOOTING SLABS AND AT JUNCTIONS OF WALLS CONSTRUCTION OF DIFFERENT MASONRY MATERIALS AND AT DEEP CHASES (REBATES) FOR SERVICE PIPES  
 SUB FLOOR VENTILATION SHALL BE PROVIDED AT A RATE OF 7300mm<sup>2</sup>/m WHERE IN THE MINIMUM SUB FLOOR CLEARANCE IS 150mm  
 SMOKE DETECTORS SHALL BE A BRK 86 RA 250v SELF CONTAINED SMOKE ALARM OR THE EQUIVALENT TO BE CONNECTED TO THE CONSUMER POWER MAINS IN ACCORDANCE WITH AS3786

THE WALL SURFACE OF A SHOWER ENCLOSURE WITH A PRE FORMED SHOWER BASE SHALL BE WATER RESISTANT TO A HEIGHT OF 1.8m ABOVE THE FLOOR, WITH ALL JUNCTIONS AND FLOOR TO HORIZONTAL SURFACES COMPLETELY WATERPROOFED. THE SURFACE IMMEDIATELY ADJACENT OR BEHIND A BATH, BASIN OR SINK SHALL BE WATER RESISTANT TO A HEIGHT NO LESS THAN 150mm ABOVE THE FLOOR WITH ALL JUNCTIONS TO HORIZONTAL SURFACES COMPLETELY WATERPROOFED. FURTHERMORE, AN ENCLOSED SHOWER WITH FLOOR SUBSTRATE  
 ALL GLAZING SHALL BE IN ACCORDANCE WITH AS 1288 WHEREIN GLAZING WITHIN 500mm OF THE FLOOR LEVEL SHALL BE 5mm THICKENED ANNEALED, GLAZED DOORS AND ASSOCIATED SIDE PANELS SHALL BE 5.38mm LAMINATED SAFETY GLASS AND BATHROOM WINDOWS WITHIN 1.5m OF THE BATH OR 500mm FROM THE SHOWER ENCLOSURE SHALL BE 3mm TOUGHENED SAFETY GLASS  
 LAUNDRIES, BATHROOMS, WATER CLOSETS AND POWDER ROOMS THAT ARE NOT NATURALLY VENTILATED SHALL BE PROVIDED WITH MECHANICAL VENTILATION  
 THE SELECTION OF GUTTERS AND DOWNPIPES SHALL BE IN ACCORDANCE WITH AS 3500.3.2 ARE ALSO LOCATED WITHIN 1.2m OF A VALLEY OR PROVISION FOR OVERFLOW MUST BE MADE  
 THE STORM WATER DISCHARGE SHALL BE CONNECTED TO THE EXISTING LEGAL POINT OF DISCHARGE  
 COMBUSTIBLE ROOF LIGHTS, SKYLIGHTS OR THE LIKE INSTALLED IN A ROOF OR PART OF A ROOF REQUIRE TO HAVE A NON COMBUSTIBLE COVERING MUST HAVE A AGGREGATE AREA NOT MORE THAN 20% OF THE ROOF OR PART OF THE ROOF AND BE NOT LESS THAN 900mm FROM THE ALLOTMENT BOUNDARY OTHER THAN THE BOUNDARY ADJOINING A ROAD ALIGNMENT OR OTHER PUBLIC SPACE

		<b>PROJECT:</b>		<b>Paul-Anj Economedes</b> architect  MOBILE: 0413 314 630 EMAIL: paul@economedes.com.au POSTAL: PO Box 929 Richmond VIC 3121 WEBSITE: www.economedesarchitectural.com.au ARIA: 35 929 325 602	<b>DRAWN:</b> P.A.E.	
		<b>PROPOSED DUAL OCCUPANCY</b>			<b>CHECKED:</b> P.A.E.	
		<b>46 GREENHILLS ROAD BUNDOORA</b>			<b>SCALE:</b> 1:100 @ A3	
		<b>CLIENT: MR. J SOROTOS</b>			<b>DATE:</b> AUG 2014	
1	JUN.15	BUILDING PERMIT VARIATION	PAE			
-	AUG.14	BUILDING PERMIT ISSUE	PAE			
No.	Date	Revision	By			
<b>DRAWING TITLE:</b>						<b>PROJECT. No:</b> 214-06  <b>DWG No:</b> REV <b>A-05</b> 1









**City of  
Whittlesea**

## DEVELOPMENT ASSESSMENT REPORT

PLANNING APPLICATION NO. 713356



Subject Land



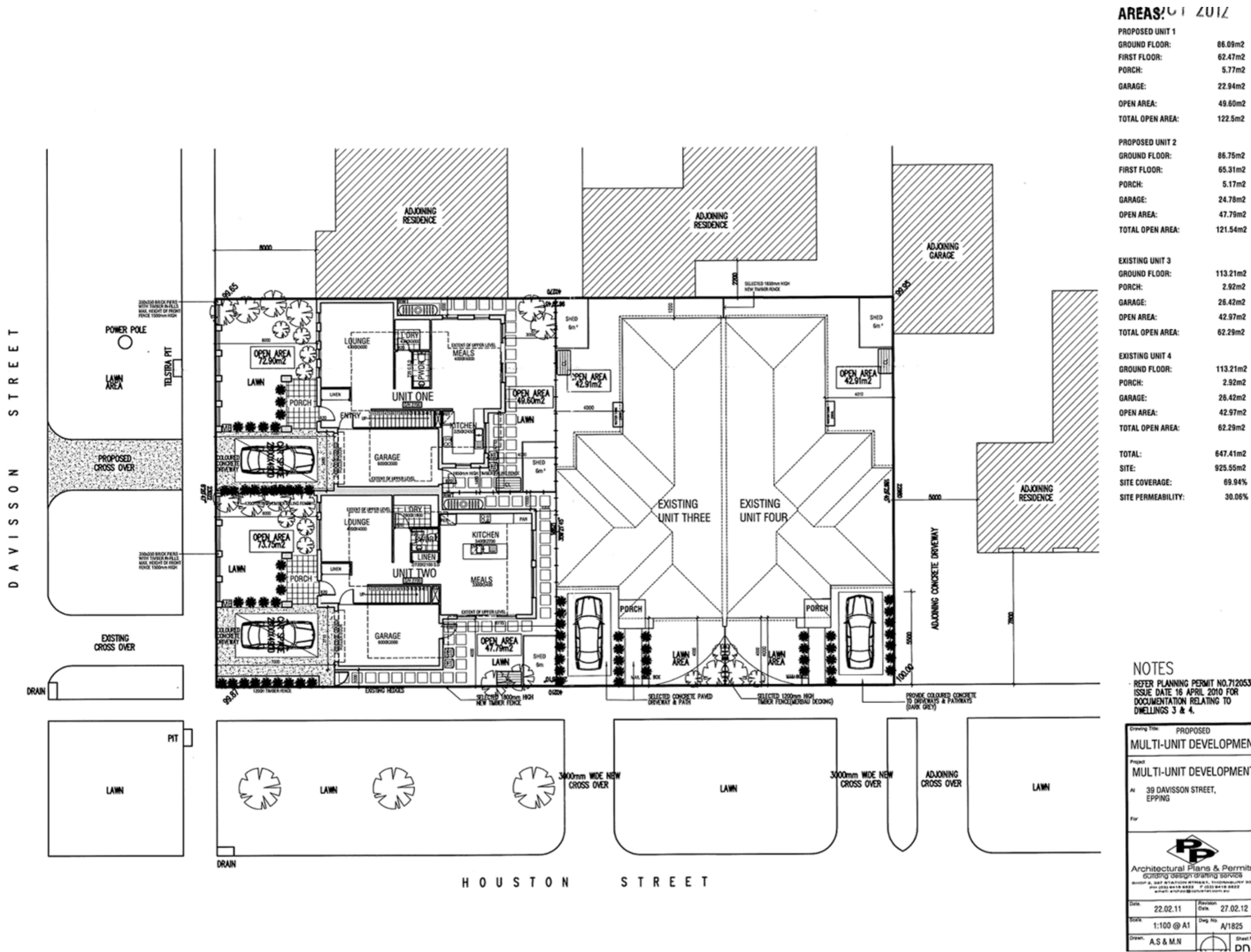
Medium Density Housing



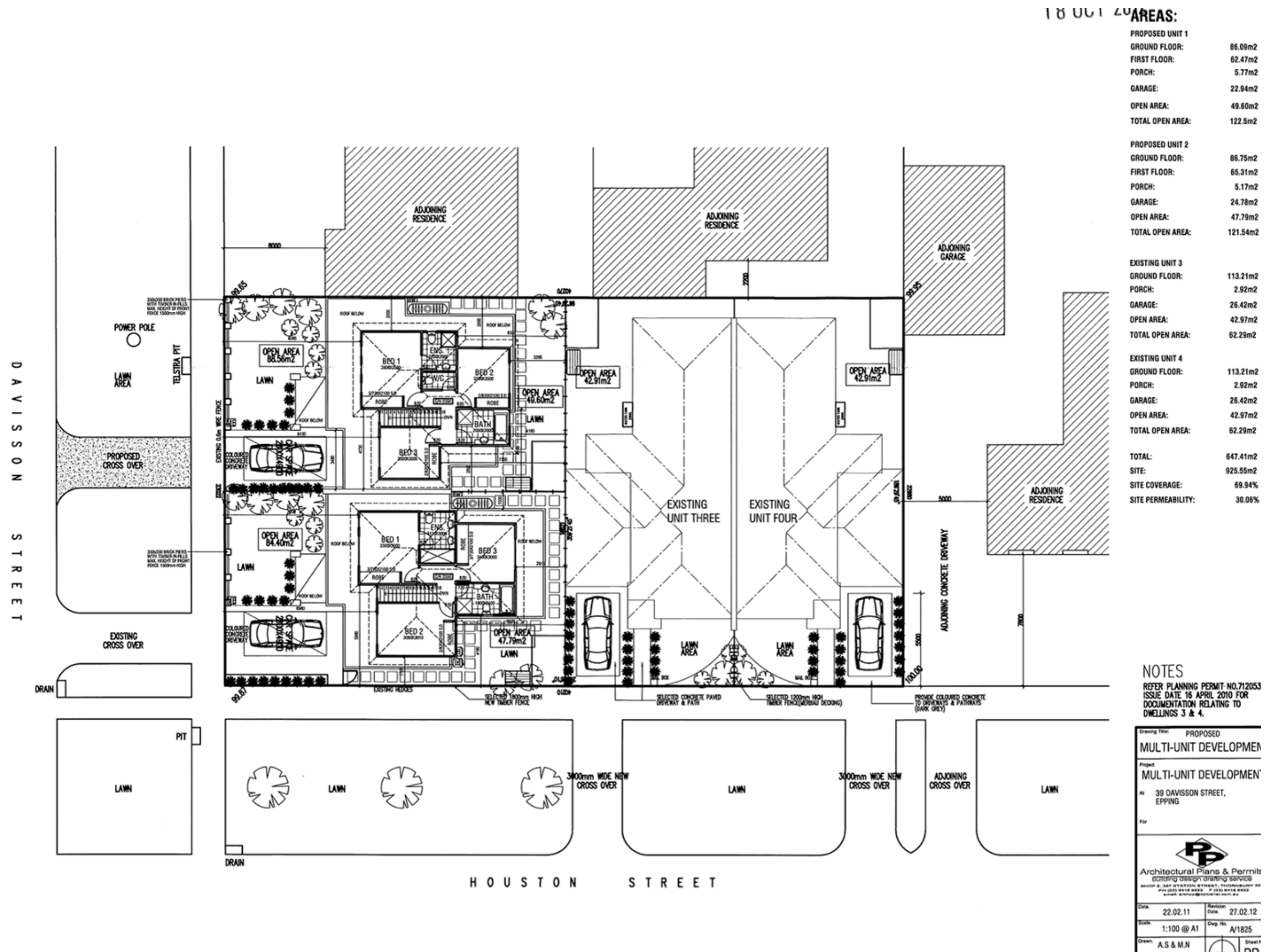
City of  
Whittlesea

DEVELOPMENT ASSESSMENT REPORT

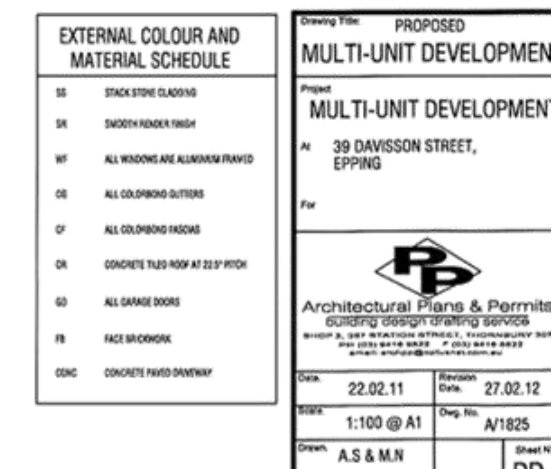






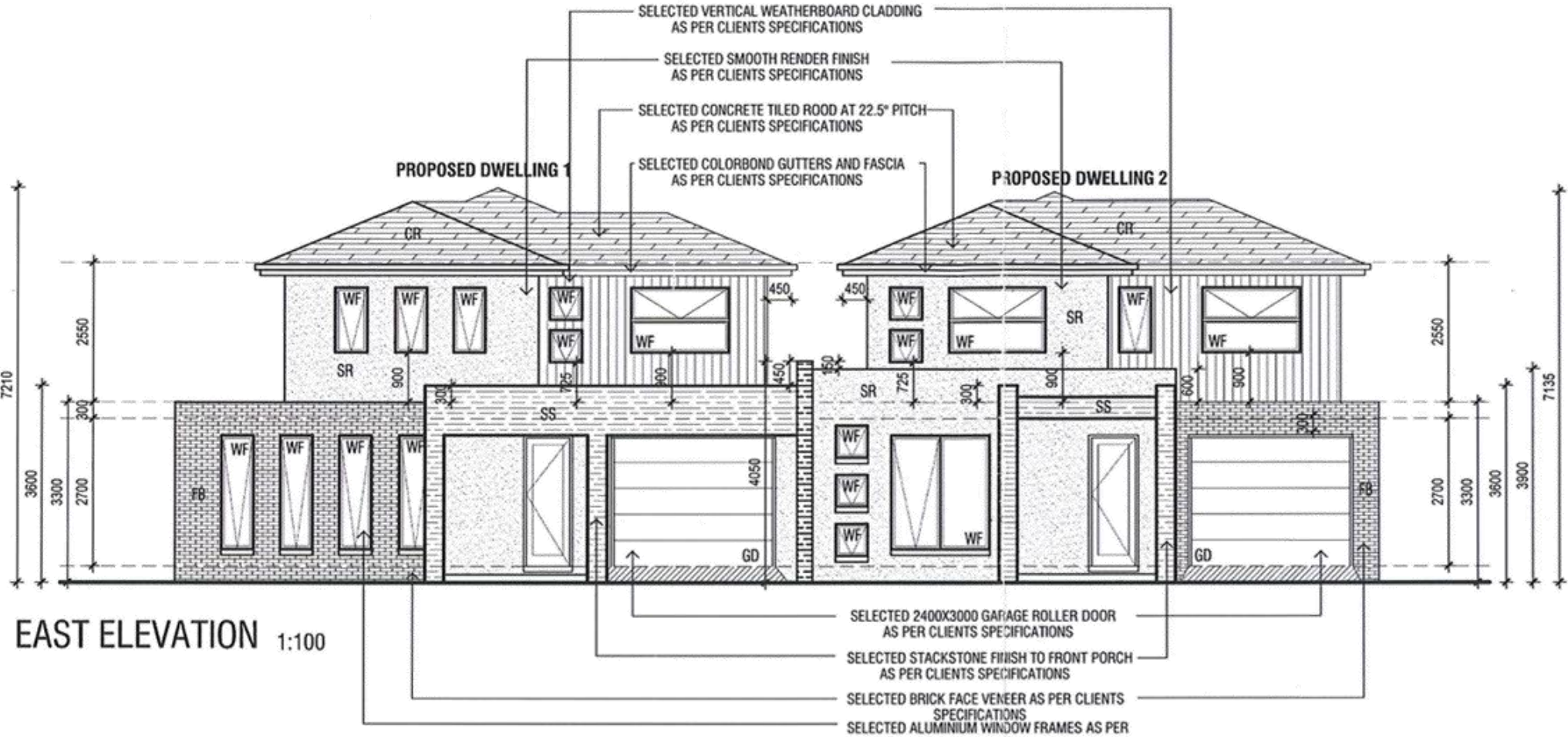


18 OCT 2012





18 OCT 2012



NOTES

REFER PLANNING PERMIT NO.712053  
ISSUE DATE 16 APRIL 2010 FOR  
DOCUMENTATION RELATING TO  
DWELLINGS 3 & 4.

Drawing Title: **PROPOSED  
MULTI-UNIT DEVELOPMEN**

Project **MULTI-UNIT DEVELOPMENT**

At **39 DAVISSON STREET,  
EPPING**

For



**Architectural Plans & Permits**  
building design drafting service  
SHOP 2, 387 STATION STREET, THORNBURY 307  
PH (03) 9416 9822 F (03) 9416 8622  
email: archpp@optusnet.com.au

Date.	03.10.12	Revision	Date.	-
Scale.	1:100 @ A3	Dwg. No.	A/1825	
Drawn.	M.N	Sheet N°	PD.	
Checked.	P R			

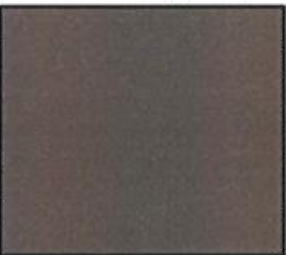
PROPOSED COLOUR SCHEDULE



SELECTED ROOFING, GUTTERS  
& FASCIA  
- DARK GREY



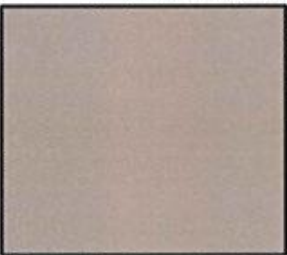
SELECTED FACE BRICKWORK  
- DARK BROWN



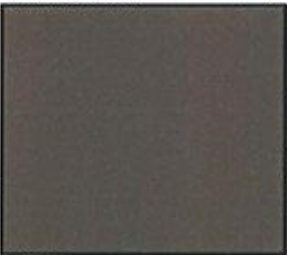
SELECTED GARAGE  
ROLLER DOORS  
- MEDIUM BROWN



SELECTED STACK STONE  
- BROWN



SELECTED SMOOTH RENDER  
- LIGHT BROWN



SELECTED WEATHERBOARDS  
- MEDIUM BROWN



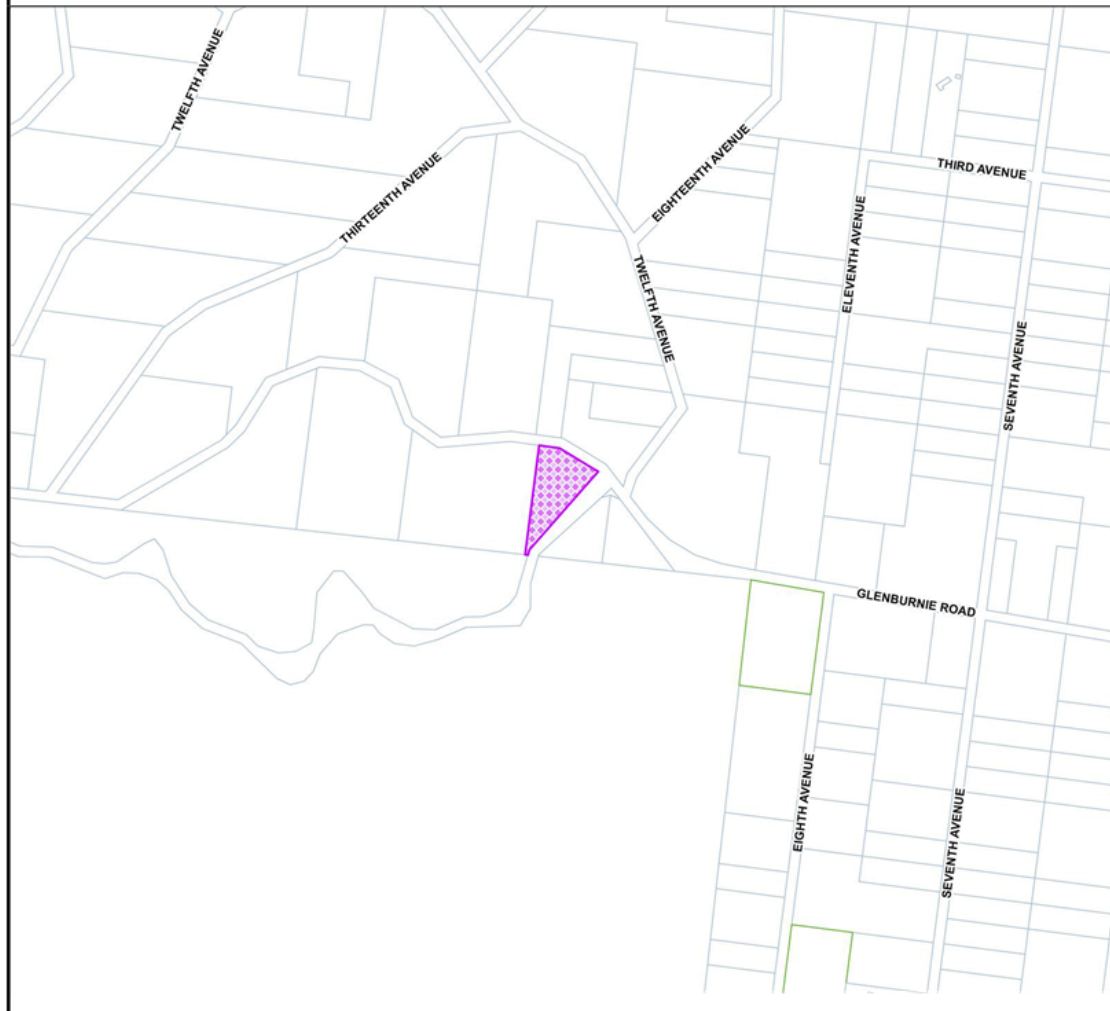
SELECTED WINDOW  
FRAMES  
- ALLOY BRUSHED



SELECTED COLOURED  
CONCRETE DRIVEWAY  
- CHARCOAL



# PLANNING APPLICATION NO. 711068



**Subject Land**



**City of  
Whittlesea**

## **DEVELOPMENT ASSESSMENT REPORT**

**PLANNING APPLICATION NO. 711068**

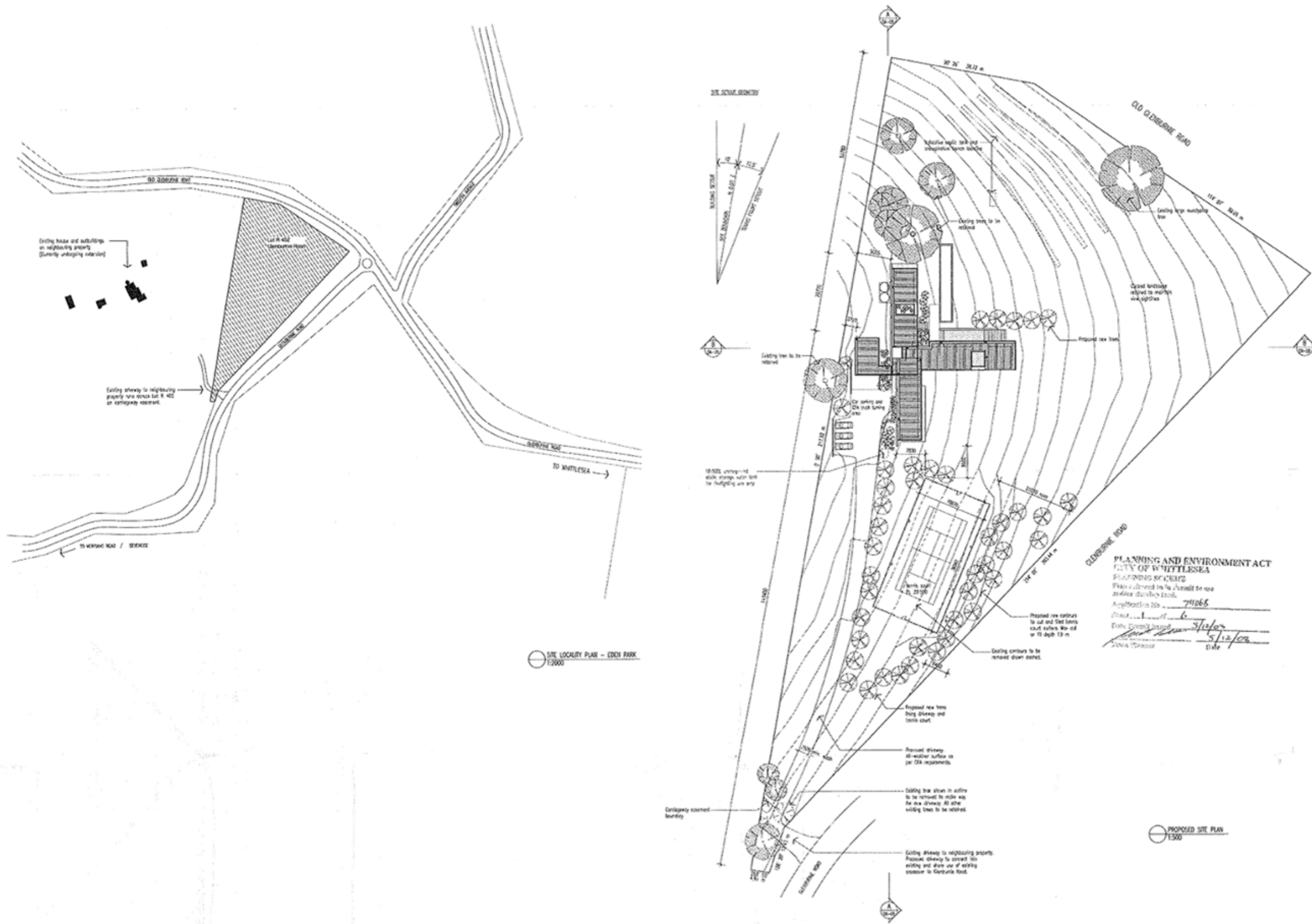


**Subject Land**

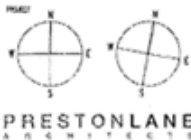


**City of  
Whittlesea**

**DEVELOPMENT ASSESSMENT REPORT**



AREA SCHEDULE	
SITE AREA	1,058 m <sup>2</sup>
BASELINE ROAD AREA	278 m <sup>2</sup>
TOTAL COVERED AREA (Includes site & roads, excludes trees shed)	736 m <sup>2</sup>
TOTAL COVERED	886 m <sup>2</sup>
USE COVERAGE (Excludes trees shed)	5.12 %



PRESTONLANE  
ARCHITECTS

1. PREPARED BY: Prestonlane Architects  
2. APPROVED BY: [Signature]  
3. APPROVED BY: [Signature]  
4. APPROVED BY: [Signature]  
5. APPROVED BY: [Signature]  
6. APPROVED BY: [Signature]  
7. APPROVED BY: [Signature]  
8. APPROVED BY: [Signature]  
9. APPROVED BY: [Signature]  
10. APPROVED BY: [Signature]

Seen Collins

NAME: Collins Residence  
Glenburnie Road  
Eden Park

Job Number: 01047

Drawing: Site Locality Plan and  
Proposed Site Plan

Scale: 1:500

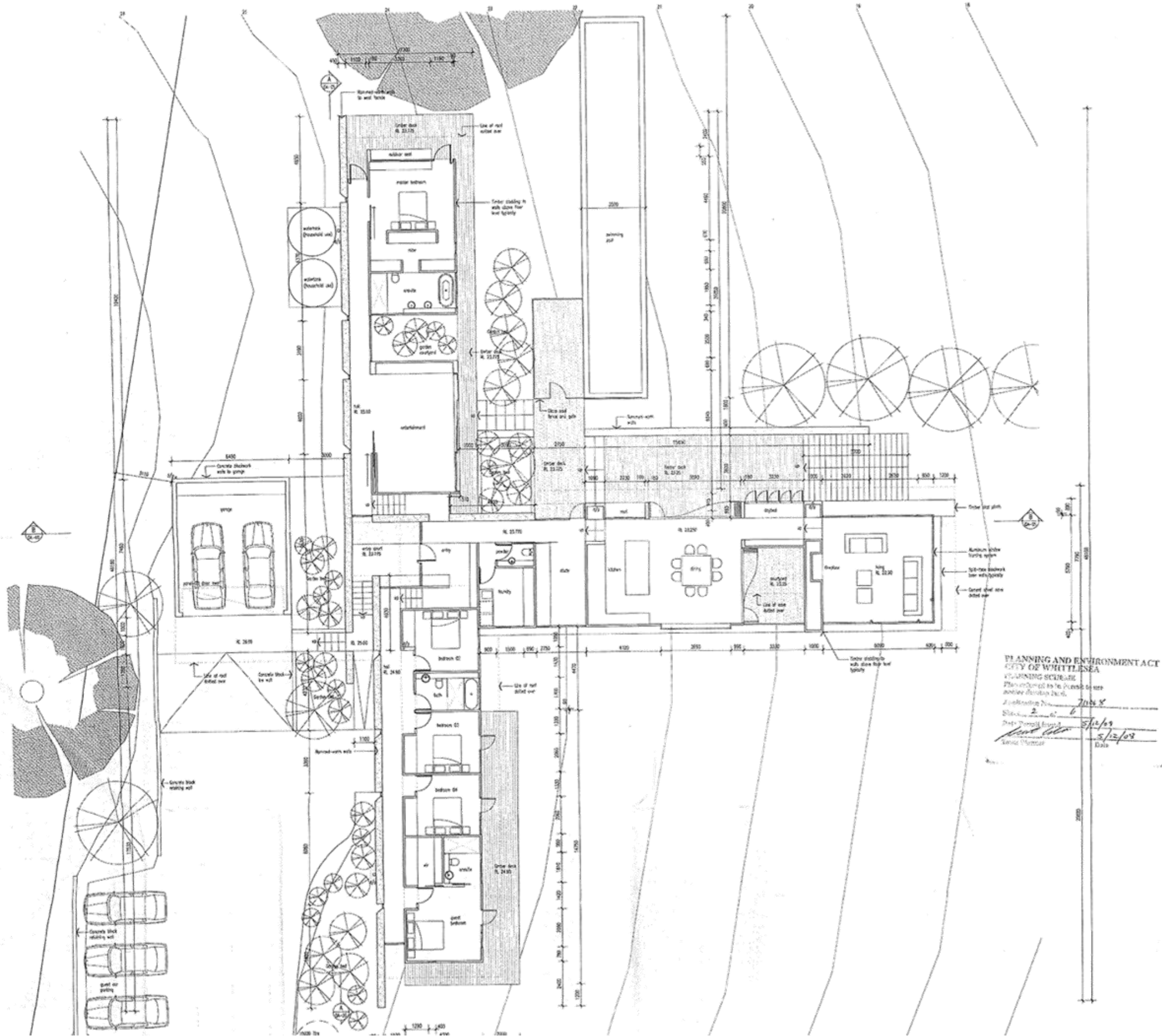
Date: May 2008

Author: [Signature]

Drawn by: [Signature]

DA-01

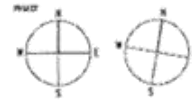




GENERAL NOTES  
All notes refer to existing height datum as per survey drawings.  
1.0000  
a) Sample section

PLANNING AND ENVIRONMENT ACT  
CITY OF WHITLESEA  
PLANNING SCHEME  
Plan number: 1010/10/10  
Date: 10/10/10  
Author: [Signature]  
Date: 10/10/10

AREA AND SOCIAL  
AREA: 1010  
SOCIAL: 1010



PRESTONLANI  
ARCHITECT

1.0000  
1.0000  
1.0000  
1.0000

Author: [Signature]  
Date: 10/10/10

Project: [Signature]

Client: [Signature]

Proposed Floor Plan

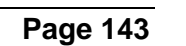
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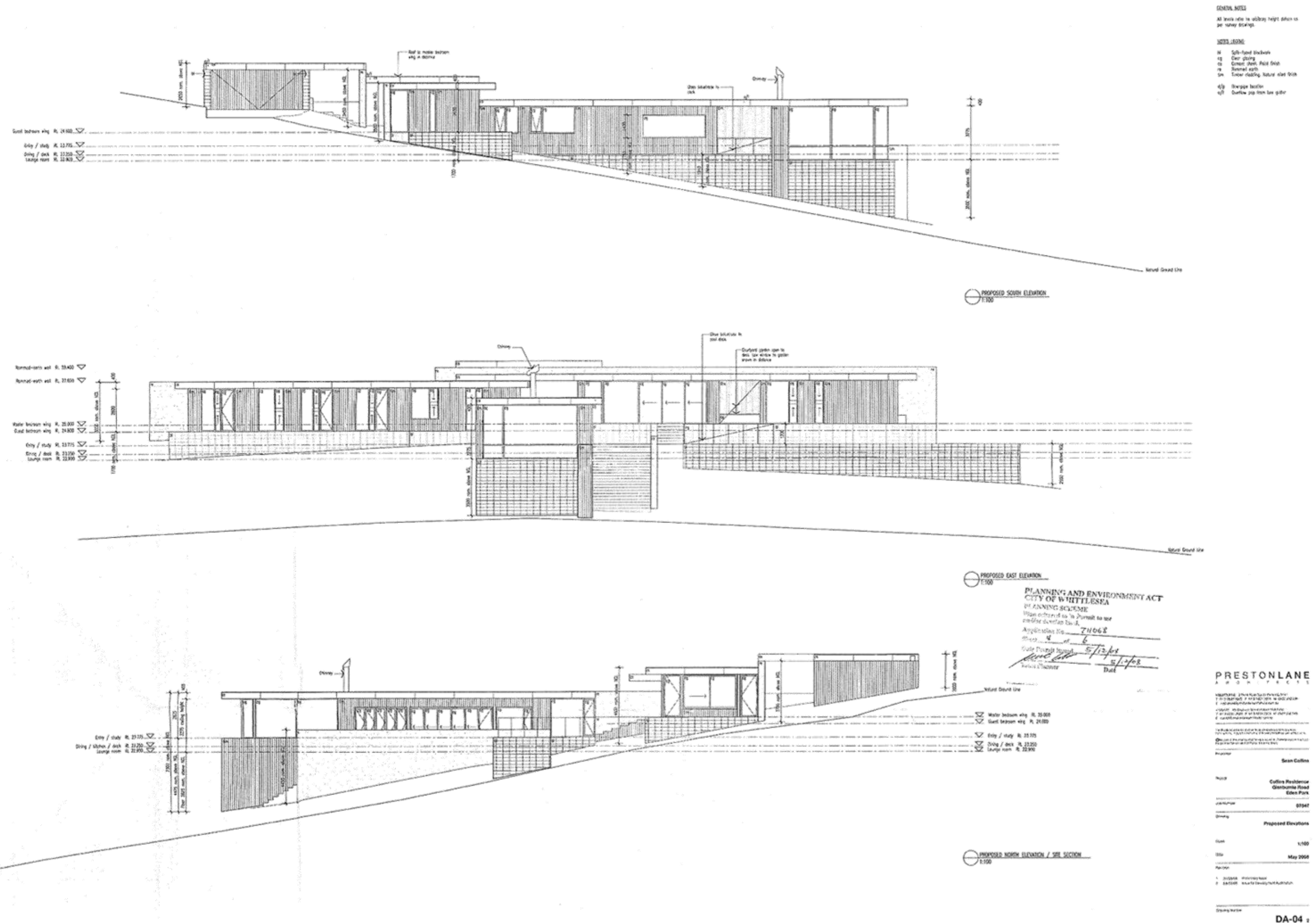
Date: May 20

Author: [Signature]

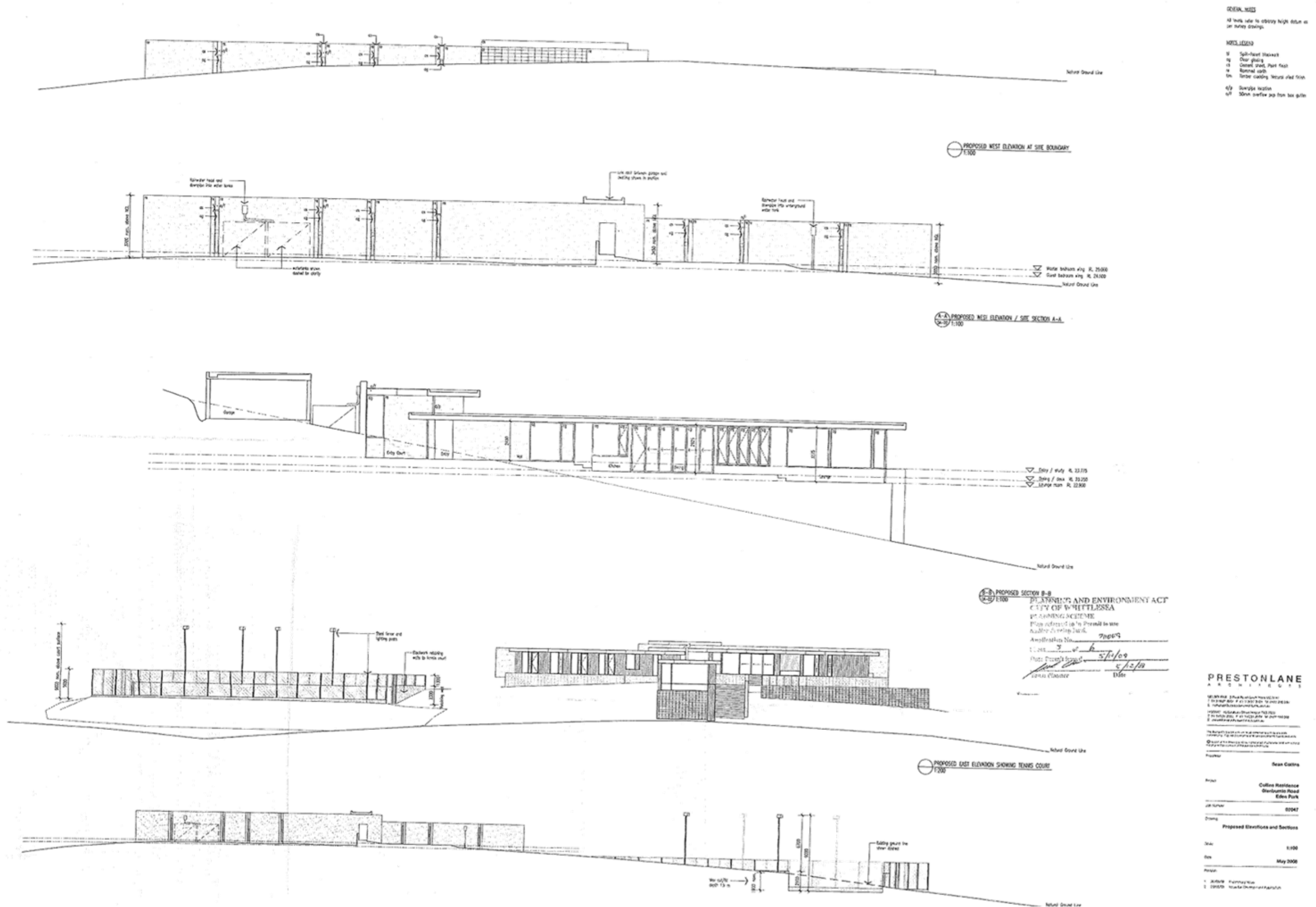
Project: [Signature]

Client: [Signature]

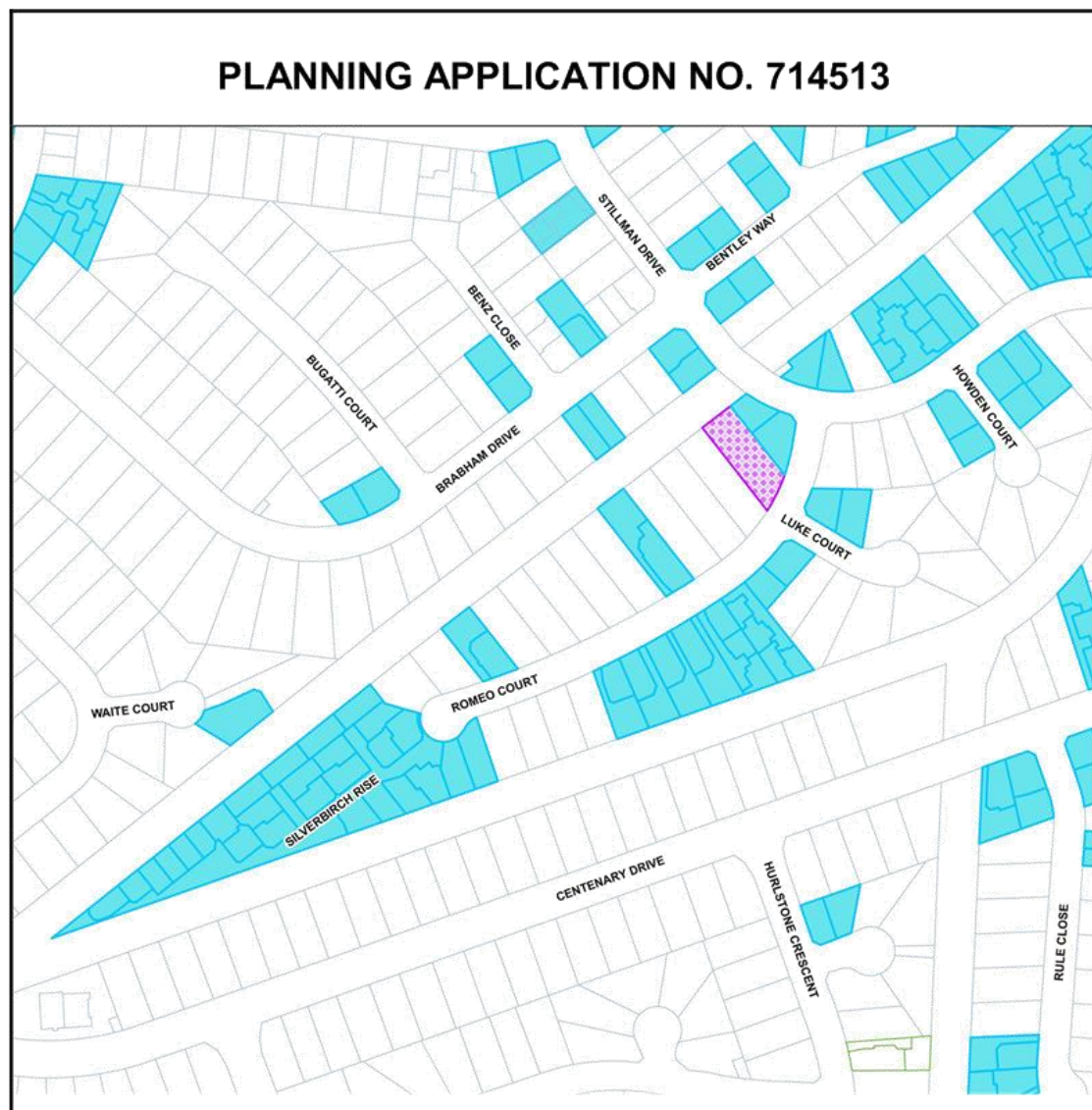












**Subject Land**



**Medium Density Housing**



**City of  
Whittlesea**

**DEVELOPMENT ASSESSMENT REPORT**



# PLANNING APPLICATION NO. 714513

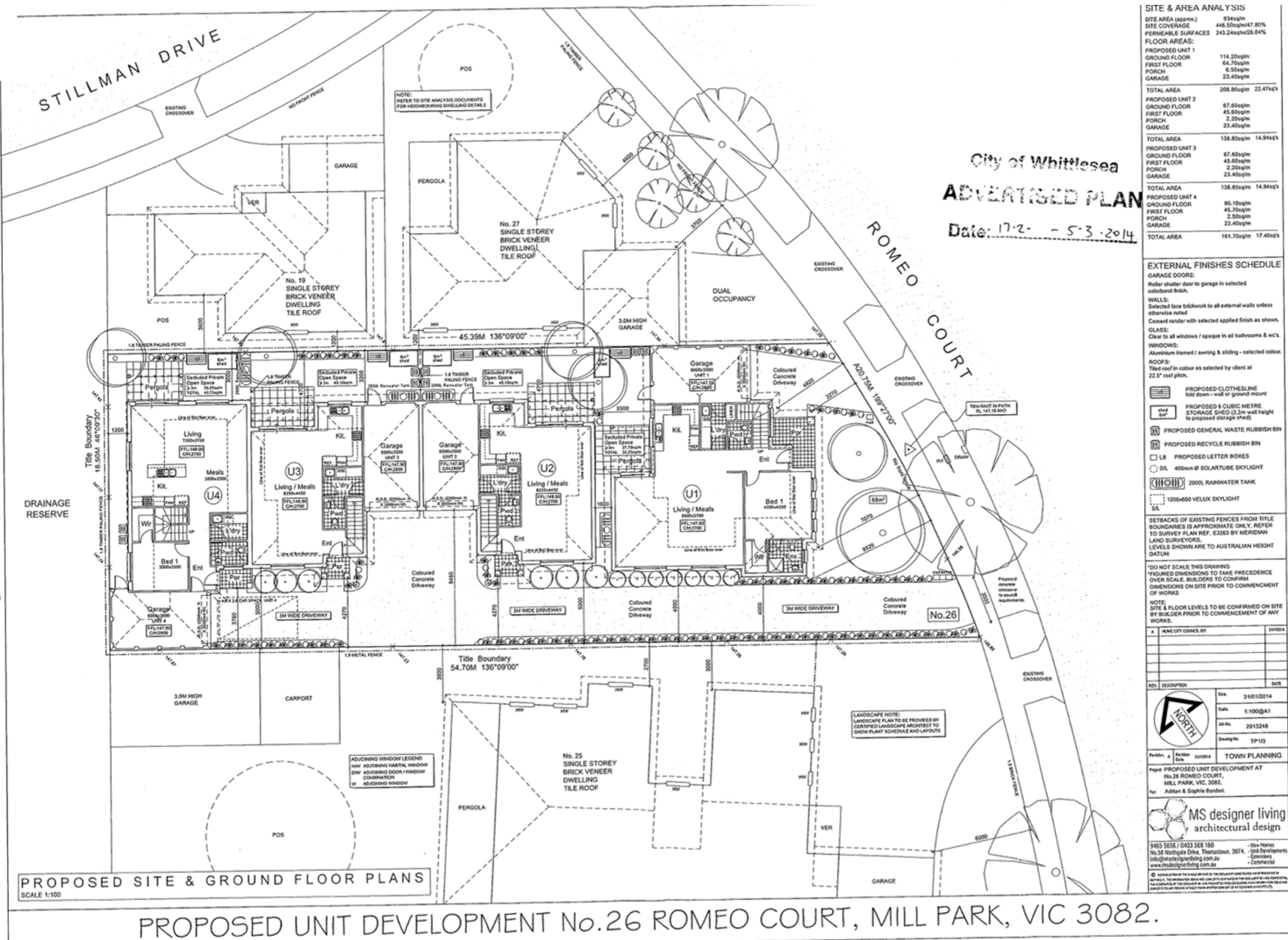


-  Subject Land
-  Medium Density Housing

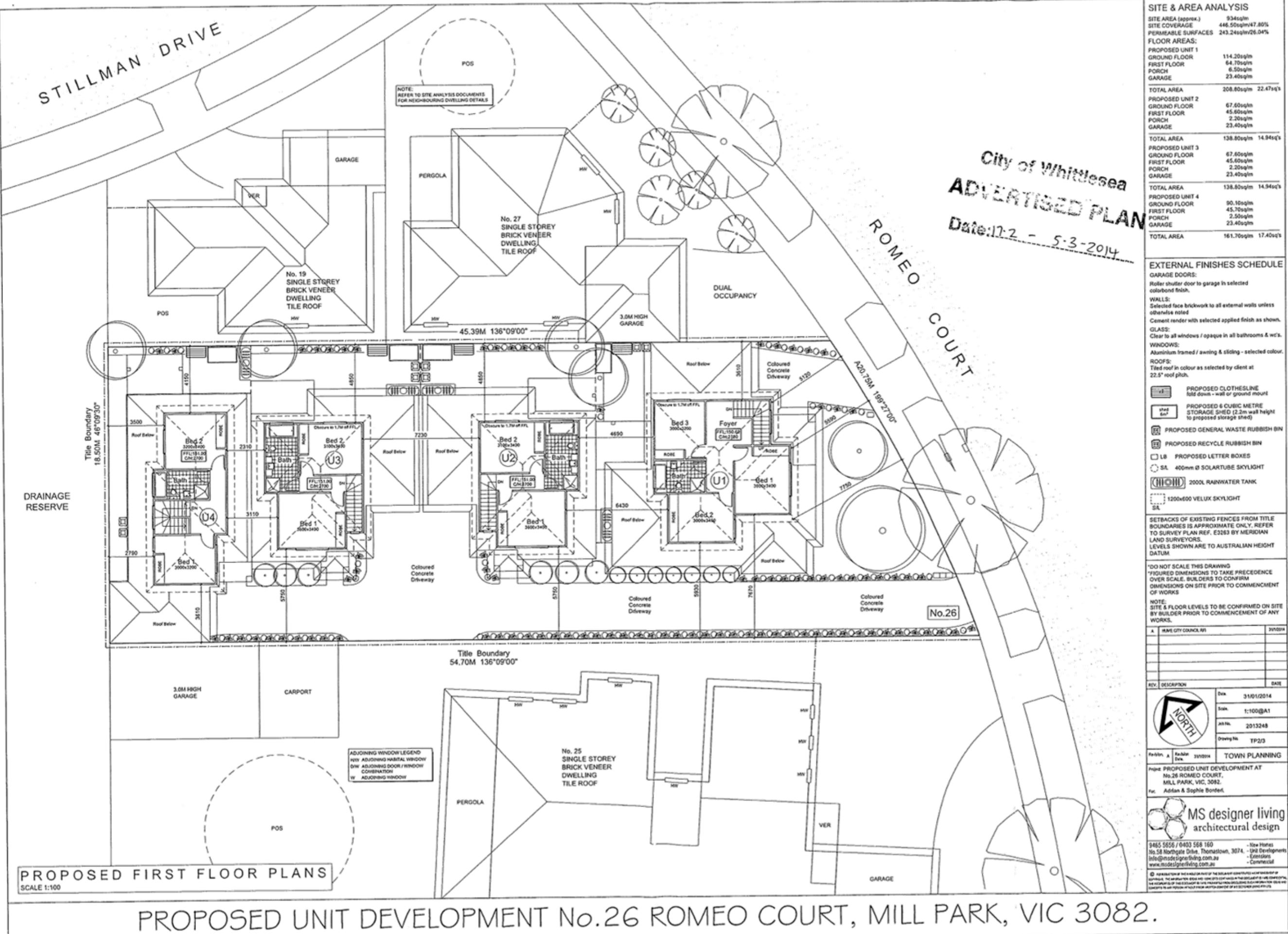


City of  
Whittlesea

## DEVELOPMENT ASSESSMENT REPORT











EXTERNAL COLOUR AND MATERIAL SCHEDULE	
SR	SMOOTH RENDER FINISH
CS	CEMENT SHEET BOARDS
WF	ALL ALUMINIUM FRAMED WINDOWS
CG	ALL COLORBOND GUTTERS
CF	ALL COLORBOND FASCIA
CR	ROOF TILES AT 22.5° PITCH
GD	ALL GARAGE DOORS
FB	FACE BRICKWORK
CONC	CONCRETE PAVED DRIVEWAY

SITE & AREA ANALYSIS	
SITE AREA (approx.)	934sqm
SITE COVERAGE	446.50sqm/47.80%
PERMEABLE SURFACES	243.24sqm/26.04%
FLOOR AREAS:	
PROPOSED UNIT 1	
GROUND FLOOR	114.20sqm
FIRST FLOOR	64.70sqm
PORCH	6.50sqm
GARAGE	23.40sqm
TOTAL AREA	208.80sqm 22.47sq%
PROPOSED UNIT 2	
GROUND FLOOR	67.60sqm
FIRST FLOOR	45.60sqm
PORCH	2.20sqm
GARAGE	23.40sqm
TOTAL AREA	138.80sqm 14.94sq%
PROPOSED UNIT 3	
GROUND FLOOR	67.60sqm
FIRST FLOOR	45.60sqm
PORCH	2.20sqm
GARAGE	23.40sqm
TOTAL AREA	138.80sqm 14.94sq%
PROPOSED UNIT 4	
GROUND FLOOR	90.10sqm
FIRST FLOOR	45.70sqm
PORCH	2.50sqm
GARAGE	23.40sqm
TOTAL AREA	161.70sqm 17.40sq%

EXTERNAL FINISHES SCHEDULE	
GD	GARAGE DOORS:
	Roller shutter door to garage in selected colorbond finish.
WALLS:	
	Selected face brickwork to all external walls unless otherwise noted.
	Cement render with selected applied finish as shown.
GLASS:	
	Clear to all windows / opaque in all bathrooms & wc's.
WINDOWS:	
	Aluminium framed / awning & sliding - selected colour.
ROOFS:	
	Tiled roof in colour as selected by client at 22.5° roof pitch.
LE	PROPOSED CLOTHESLINE
	fold down - wall or ground mount
SHED	PROPOSED 6 CUBIC METRE
	STORAGE SHED (2.2m wall height to proposed storage shed)
GB	PROPOSED GENERAL WASTE RUBBISH BIN
RB	PROPOSED RECYCLE RUBBISH BIN
LB	PROPOSED LETTER BOXES
SL	400mm Ø SOLARTUBE SKYLIGHT
RT	2000L RAINWATER TANK
SL	1200x600 VELUX SKYLIGHT

SETBACKS OF EXISTING FENCES FROM TITLE BOUNDARIES IS APPROXIMATE ONLY, REFER TO SURVEY PLAN REF. E3263 BY MERIDIAN LAND SURVEYORS. LEVELS SHOWN ARE TO AUSTRALIAN HEIGHT DATUM

\*DO NOT SCALE THIS DRAWING  
\*FIGURED DIMENSIONS TO TAKE PRECEDENCE OVER SCALE. BUILDERS TO CONFIRM DIMENSIONS ON SITE PRIOR TO COMMENCEMENT OF WORKS

NOTE:  
SITE & FLOOR LEVELS TO BE CONFIRMED ON SITE BY BUILDER PRIOR TO COMMENCEMENT OF ANY WORKS.

A	HAVE CITY COUNCIL RP	31/03/2014

REV	DESCRIPTION	DATE
1		
2		
3		
4		
5		

Revision	A	Revision Date	31/03/2014
Project	TOWN PLANNING		
Project	PROPOSED UNIT DEVELOPMENT AT		
	No.26 ROMEO COURT,		
	MILL PARK, VIC, 3082.		
For	Adrian & Sophie Borden,		



9465 5656 / 0403 568 160  
No.58 Northgate Drive, Thornhill, VIC, 3074.  
info@msdesignerliving.com.au  
www.msdesignerliving.com.au

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PROPOSED ELEVATIONS  
SCALE 1:100

PROPOSED UNIT DEVELOPMENT No.26 ROMEO COURT, MILL PARK, VIC 3082.



**EXHIBITED VERSION**  
WHITTLESEA PLANNING SCHEME

**21.07 ENVIRONMENTAL RISK**

27/03/2014  
C177

**21.07-1 Floodplains**

27/03/2014  
C177

Refer to Clause 13.02

**21.07-2 Erosion and Landslip**

27/03/2014  
C177

Refer to Clause 13.03-2

**21.07-3 Salinity**

27/03/2014  
C177

Refer to Clause 13.03-3

**21.07-4 Bushfire**

DD/MM/YYYY  
Proposed C198

Bushfires and grassfires have had significant social, economic, and environmental impacts on the City of Whittlesea and its residents. Council's aim is to strengthen community resilience to bushfire and ensure that fire risks are carefully considered throughout the planning and development process.

The *Regional Bushfire Planning Assessment* (Melbourne Metropolitan Region, 2012) outlines that bushfire hazard is high in the northern rural reaches of the municipality in addition to areas along the Plenty River adjacent to established and emerging residential estates. This hazard is due to the nature and extent of vegetation, topography, settlement patterns and road access arrangements.

Grassland areas also present a fire hazard, particularly at the urban-grassland interface of existing settlements. Residential areas that are bordered by non-urban edges including conservation areas, cleared agricultural land and open space where new development is expected to occur are at a higher risk of grassfire that can spread rapidly and put lives and property in danger.

**Objective 1 To ensure that strategic and settlement planning decisions prioritise the protection of human life.**

Strategy 1.1 Direct growth to areas of lower bushfire risk having regard to the municipal and local context.

Strategy 1.2 Avoid locations where bushfire risk cannot be reduced to an acceptable level.

**Objective 2 To reduce the impact of bushfire risk.**

Strategy 2.1 Ensure that decision-making considers issues of strategic and settlement planning as a primary consideration, followed by precinct and lot level responses.

Strategy 2.2 Consider alternate options for development in areas where required bushfire protection measures may be incompatible with environmental and cultural heritage objectives.

**21.07-5 Implementation**

DD/MM/YYYY  
Proposed C198

**Resource Documents**



**EXHIBITED VERSION**  
WHITTLESEA PLANNING SCHEME

- Clause 22.03 Bushfire Management Local Planning Policy
- Department of Environment, Land, Water and Planning Regional Bushfire Planning Assessment – Metropolitan Melbourne Region (2012).
- City of Whittlesea Municipal Fire Management Plan (2012-2015), as amended from time to time.
- Country Fire Authority Preferred Requirements: Water supplies and access for subdivisions in Residential 1 and 2 and Township Zones (2006).
- Country Fire Authority Preferred Requirements: Water supplies and access for subdivisions in Rural Zones (2006).
- Australian Standard AS 3959: 2011, Construction of Buildings in Bushfire Prone Areas

EXHIBITED VERSION  
WHITTLESEA PLANNING SCHEME

### 22.03 BUSHFIRE MANAGEMENT POLICY

DD/MM/YYYY  
Proposed  
C198

This policy applies to all land in the municipality within a designated Bushfire Prone Area as outlined in the *Building Regulations* 2006, which is not already covered by a Bushfire Management Overlay (BMO).

The policy applies to residential subdivisions of 10 lots or more, new residential and commercial uses, and development applications for:

- Accommodation
- Child Care Centre
- Education centre
- Office
- Place of Assembly
- Emergency services facility
- Hospital
- Research centre
- Cinema-based entertainment facility
- Shop
- Food and drink premises
- Gambling premises
- Animal keeping
- Major sports and recreation facility
- Indoor recreation facility
- Or any application for development that will result in people congregating in large numbers.

#### 22.03-1 Policy Basis

Bushfires and grassfires have had significant social, economic and environmental impacts on the City of Whittlesea and its residents. This policy seeks to strengthen the community's resilience to bushfire and ensure that fire risks are carefully considered in the planning and development process.

Areas in the municipality of high-to-extreme bushfire risk have been mapped and are covered by the Bushfire Management Overlay (BMO). However, fire hazard in the municipality is not limited to areas of extreme bushfire hazard as mapped by the BMO, and areas of low-to-medium fire hazard are more prevalent throughout the municipality. These areas are identified in the State-wide designated Bushfire Prone Area (BPA) map as outlined in the *Building Regulations* 2006.

In particular, the City of Whittlesea has extensive undeveloped areas located at the urban/rural interface of existing settlements. Residential areas that are bordered by non-urban edges including conservation areas, cleared agricultural land and open space where new development is expected to occur are at a higher risk of grassfire that can spread rapidly and put lives and property in danger.

Where residential settlement is permitted, particular attention to fire issues is needed in the design of subdivisions, the siting and design of buildings, and the management of land in areas that are not covered by a Bushfire Management Overlay (BMO).

Decision-making around fire risk will consider strategic and settlement planning as a primary consideration followed by precinct, neighbourhood and site level responses.

#### 22.03-2 Objectives

- To give effect to the relevant objectives and strategies of Clause 13.05 (Bushfire) over all land identified as being at risk of bushfire.

**EXHIBITED VERSION**  
**WHITTLESEA PLANNING SCHEME**

- To ensure that land use planning and development planning applications are informed by a broad consideration of bushfire.
- To avoid intensifying bushfire risk to people and property through inappropriately located, designed or managed uses or developments.
- To ensure that in areas outside of the BMO, bushfire and grassfire protection measures are considered for use and development which may be occupied by potentially vulnerable people and populations.
- To strengthen community resilience to bushfire by ensuring that bushfire and grassfire protection measures are considered and given effect to in large and/or isolated subdivisions outside the BMO.
- To ensure that the threat of grassfire in interface areas is considered at a precinct, sub-precinct, and street level through the provision of appropriate fire breaks and road network arrangements.
- To ensure the protection of environmental and cultural values from bushfire and grassfire risks.

**22.03-3 Policy****Strategic and Settlement Planning**

It is policy to:

- Ensure that the views and relevant publications (as outlined in Clause 21.16-6) of the Country Fire Authority (CFA) are taken into account in decision-making.
- Prioritise bushfire management outcomes that are not at the detriment of environmental and cultural heritage assets.
- Consider the views of the Municipal Fire Management Committee as outlined in the *City of Whittlesea Municipal Fire Management Plan* on potential risks to life, property and community infrastructure, and the ongoing resources which will be necessary to maintain those activities in conjunction with the ongoing use of land. Require that potentially vulnerable development outside of the Bushfire Management Overlay:
  - Considers the fire risk at a local level.
  - Incorporates passive fire risk mitigation measures, including through the siting, landscaping and layout of new development, to enhance resilience.

**Subdivisions**

It is policy to:

- Ensure that the subdivision of land outside of the Bushfire Management Overlay into more than 10 lots:
  - Consider the need for multiple points of access and egress to the existing and proposed road network.
  - Consider the need for perimeter roads and hard-edges at the urban-hazard interface (including grassland).
  - Require adequate access and egress opportunities for early residents, construction workers and emergency vehicles in emergent developments.
  - Implement a 60 metre fire break (buffer) between a grassland fire threat edge and a residential development edge (as specified in the Australian Standard AS3959: 2011), unless it has been determined that a reduced fire break is appropriate. The 60m fire break includes the widths of hard surfaces such as perimeter roads, footpaths, nature strips (non-vegetated) in addition to any closely managed grasslands or vegetation that is under 10cm in length. The fire break should not be comprised of land with high ecological values.



**EXHIBITED VERSION**  
**WHITTLESEA PLANNING SCHEME**

**Land Management**

It is policy to:

- Require that non-urban grasslands are maintained to a length of 10cm or under (or as specified in a municipal fire prevention notice), for any development where a Site Management Plan is required under a subdivision permit.

**Siting and Design**

It is policy to:

- Require that all new development considers the likely bushfire behaviour on a site and in the wider landscape.
- Strongly discourage the siting of buildings on north or north-west facing slopes, particularly steeper slopes and the ridges above these slopes.

**Water Supply and Utility**

It is policy to:

- Ensure that rural properties provide adequate water supply with suitable flow rates for fire fighting purposes.
- Ensure that the location of electricity and gas limits the possibility of ignition of surrounding bushland, grassland, or the fabric of buildings.

**Access**

It is policy to:

- Require multiple access and egress opportunities to open space and parklands.
- Require that all development provide appropriate fire authority access to and on the land.
- Require adequate access and egress opportunities for early residents, construction workers and emergency vehicles in emergent developments.

**22.03-4 Decision Guidelines**

Before deciding on an application to use or develop land the responsible authority will consider:

- The bushfire hazard on the land and in the surrounding landscape and the level of risk it poses to human life, property and community infrastructure.
- Whether necessary bushfire protection measures can be established and maintained in conjunction with the ongoing use of the land, including any mechanisms required to achieve this.
- The layout of subdivisions so as to provide for a clear fire break and to optimise the opportunity for escape from bushfires and access.
- The siting and design of buildings so as to reduce fire risk and damage.
- The impact of bushfire management measures on biodiversity and cultural heritage.
- Consistency with the approach to planning and decision-making in bushfire prone areas as outlined in this policy and Clause 21.07-4 of the Municipal Strategic Statement, which seeks to prioritise strategic and settlement planning followed by localised site based responses.
- Any relevant approved State, regional and municipal fire prevention plans.

**22.03-5 Policy Reference**

EXHIBITED VERSION  
WHITTLESEA PLANNING SCHEME

- *Bushfire Prone Area Map* determined under the *Building Regulations 2006* (as amended from time to time).
- *Regional Bushfire Planning Assessment – Metropolitan Melbourne Region* (Department of Environment, Land, Water and Planning, 2012).
- *Municipal Fire Management Plan* (City of Whittlesea, 2012-2015) (as amended from time to time).
- CFA preferred requirements: *Water supplies and access for subdivisions in Residential 1 and 2 and Township Zones* (2006).
- CFA preferred requirements: *Water supplies and access for subdivisions in Rural Zones* (2006).
- Australian Standard AS 3959: 2011, *Construction of Buildings in Bushfire Prone Areas*.

POST EXHIBITION VERSION  
WHITTLESEA PLANNING SCHEME

**21.07 ENVIRONMENTAL RISK**

27/03/2014  
C177

**21.07-1 Floodplains**

27/03/2014  
C177

Refer to Clause 13.02

**21.07-2 Erosion and Landslip**

27/03/2014  
C177

Refer to Clause 13.03-2

**21.07-3 Salinity**

27/03/2014  
C177

Refer to Clause 13.03-3

**21.07-4 Bushfire**

DD/MM/YYYY  
Proposed C198

Bushfires and grassfires have had significant social, economic, and environmental impacts on the City of Whittlesea and its residents. Council's aim is to strengthen community resilience to bushfire and ensure that fire risks are carefully considered throughout the planning and development process.

The *Regional Bushfire Planning Assessment* (Melbourne Metropolitan Region, 2012) outlines that bushfire hazard is high in the northern rural reaches of the municipality in addition to areas along the Plenty River adjacent to established and emerging residential estates. This hazard is due to the nature and extent of vegetation, topography, settlement patterns and road access arrangements.

Grassland areas also present a fire hazard, particularly at the urban-grassland interface of existing settlements. Residential areas that are bordered by non-urban edges including conservation areas, cleared agricultural land and open space where new development is expected to occur are at a higher risk of grassfire that can spread rapidly and put lives and property in danger.

**Objective 1 To ensure that strategic and settlement planning decisions prioritise the protection of human life.**

Strategy 1.1 Direct growth to areas of lower bushfire risk having regard to the municipal and local context.

Strategy 1.2 Avoid locations where bushfire risk cannot be reduced to an acceptable level.

**Objective 2 To reduce the impact of bushfire risk.**

Strategy 2.1 Ensure that decision-making considers issues of strategic and settlement planning as a primary consideration, followed by precinct and lot level responses.

Strategy 2.2 Consider alternate options for development in areas where required bushfire protection measures may be incompatible with environmental and cultural heritage objectives.

**21.07-5 Implementation**

DD/MM/YYYY  
Proposed C198

**Reference Documents**

- Clause 22.03 Bushfire Management Local Planning Policy



**POST EXHIBITION VERSION**  
WHITTLESEA PLANNING SCHEME

- Department of Environment, Land, Water and Planning Regional Bushfire Planning Assessment – Metropolitan Melbourne Region (2012).
- City of Whittlesea Municipal Fire Management Plan (2012-2015), as amended from time to time.
- Country Fire Authority Preferred Requirements: Water supplies and access for subdivisions in Residential 1 and 2 and Township Zones (2006).
- Country Fire Authority Preferred Requirements: Water supplies and access for subdivisions in Rural Zones (2006).
- Australian Standard AS 3959: 2011, Construction of Buildings in Bushfire Prone Areas

POST EXHIBITION VERSION  
WHITTLESEA PLANNING SCHEME

**22.03 BUSHFIRE MANAGEMENT POLICY**

DD/MM/YYYY  
Proposed  
C198

This policy applies to all land in the municipality within a designated Bushfire Prone Area as outlined in the *Building Regulations* 2006, which is not already covered by a Bushfire Management Overlay (BMO).

The policy applies to residential subdivisions of 10 lots or more, new residential and commercial uses, and development applications for:

- Accommodation
- Child Care Centre
- Education centre
- Office
- Place of Assembly
- Emergency services facility
- Hospital
- Research centre
- Cinema-based entertainment facility
- Shop
- Food and drink premises
- Gambling premises
- Animal keeping
- Major sports and recreation facility
- Indoor recreation facility
- Or any application for development that will result in people congregating in large numbers.

**22.03-1 Policy Basis**

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Areas in the municipality of high-to-extreme bushfire risk have been mapped and are covered by the Bushfire Management Overlay (BMO). However, fire hazard in the municipality is not limited to areas of extreme bushfire hazard as mapped by the BMO, and areas of low-to-medium fire hazard are more prevalent throughout the municipality. These areas are identified in the State-wide designated Bushfire Prone Area (BPA) map as outlined in the *Building Regulations* 2006.

In particular, the City of Whittlesea has extensive undeveloped areas located at the urban/rural interface of existing settlements. Residential areas that are bordered by non-urban edges including conservation areas, cleared agricultural land and open space where new development is expected to occur are at a higher risk of grassfire that can spread rapidly and put lives and property in danger.

Where residential settlement is permitted, particular attention to fire issues is needed in the design of subdivisions, the siting and design of buildings, and the management of land in areas that are not covered by a Bushfire Management Overlay (BMO).

Decision-making around fire risk will consider strategic and settlement planning as a primary consideration followed by precinct, neighbourhood and site level responses.

**22.03-2 Objectives**

- To give effect to the relevant objectives and strategies of Clause 13.05 (Bushfire) over all land identified as being at risk of bushfire.

**POST EXHIBITION VERSION**  
WHITTLESEA PLANNING SCHEME

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It is policy to:

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WHITTLESEA PLANNING SCHEME

closely managed grasslands or vegetation that is under 10cm in length. The fire break should not be comprised of land with high ecological values.

- [Require that non-urban grasslands are maintained to a length of 10cm or under \(or as specified in a municipal fire prevention notice\), for any development where a subdivision permit is required under other provisions of the planning scheme to include a Site Management Plan.](#)

**Buildings and works**

It is policy to:

**Land Management**

It is policy to:

- ~~Require that non-urban grasslands are maintained to a length of 10cm or under (or as specified in a municipal fire prevention notice), for any development where a Site Management Plan is required under a subdivision permit.~~

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- [The impact of bushfire management measures on biodiversity and cultural heritage.](#)
- The layout of subdivisions so as to provide for a clear fire break and to optimise the opportunity for escape from bushfires and access.

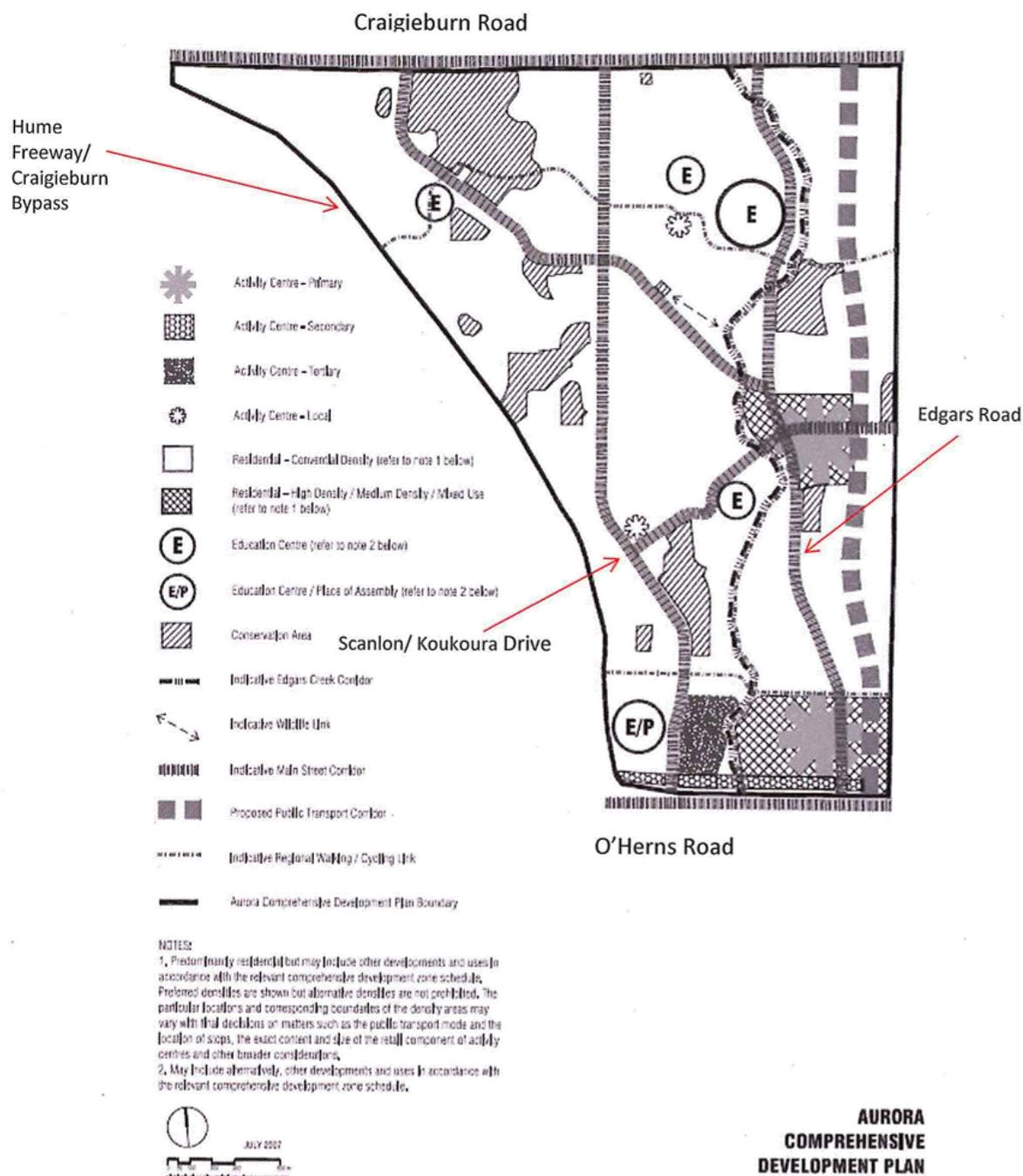
POST EXHIBITION VERSION  
WHITTLESEA PLANNING SCHEME

- The siting and design of buildings so as to reduce fire risk and damage.
- ~~The impact of bushfire management measures on biodiversity and cultural heritage.~~
- Consistency with the approach to planning and decision-making in bushfire prone areas as outlined in this policy and Clause 21.07-4 of the Municipal Strategic Statement, which seeks to prioritise strategic and settlement planning followed by localised site based responses.
- Any relevant approved State, regional and municipal fire prevention plans, [Site Management Plan, or Precinct Structure Plan applying to the land.](#)

**22.03-5 Policy Reference [Reference Documents](#)**

- [Australian Standard AS 3959: 2011, Construction of Buildings in Bushfire Prone Areas \(as amended from time to time\).](#)
- *Bushfire Prone Area Map* determined under the *Building Regulations 2006* (as amended from time to time).
- [CFA preferred requirements: Water supplies and access for subdivisions in Residential 1 and 2 and Township Zones \(2006\).](#)
- [CFA preferred requirements: Water supplies and access for subdivisions in Rural Zones \(2006\).](#)
- [Municipal Fire Management Plan \(City of Whittlesea, 2012-2015\) \(as amended from time to time\).](#)
- [Planning Practice Note 64: Local Planning for bushfire protection \(September 2015\) \(as amended from time to time\)](#)
- ~~Regional Bushfire Planning Assessment – Metropolitan Melbourne Region (Department of Environment, Land, Water and Planning, 2012).~~
- ~~Municipal Fire Management Plan (City of Whittlesea, 2012-2015) (as amended from time to time).~~
- ~~CFA preferred requirements: Water supplies and access for subdivisions in Residential 1 and 2 and Township Zones (2006).~~
- ~~CFA preferred requirements: Water supplies and access for subdivisions in Rural Zones (2006).~~
- ~~Australian Standard AS 3959: 2011, Construction of Buildings in Bushfire Prone Areas.~~

Attachment 1: Aurora Comprehensive Development Plan (July 2007)







Attachment 2: Locality Plan – Subject Site outlined in red

*Subject site shown in red*

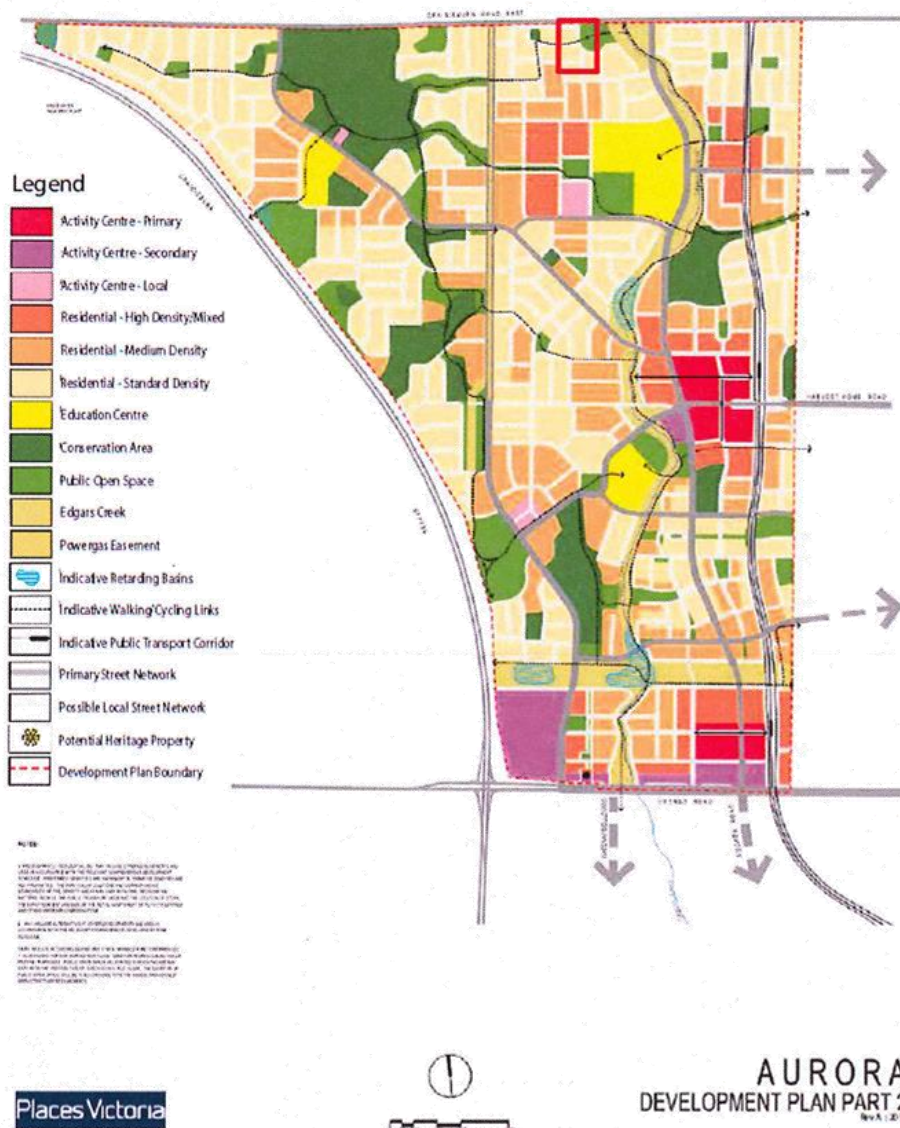






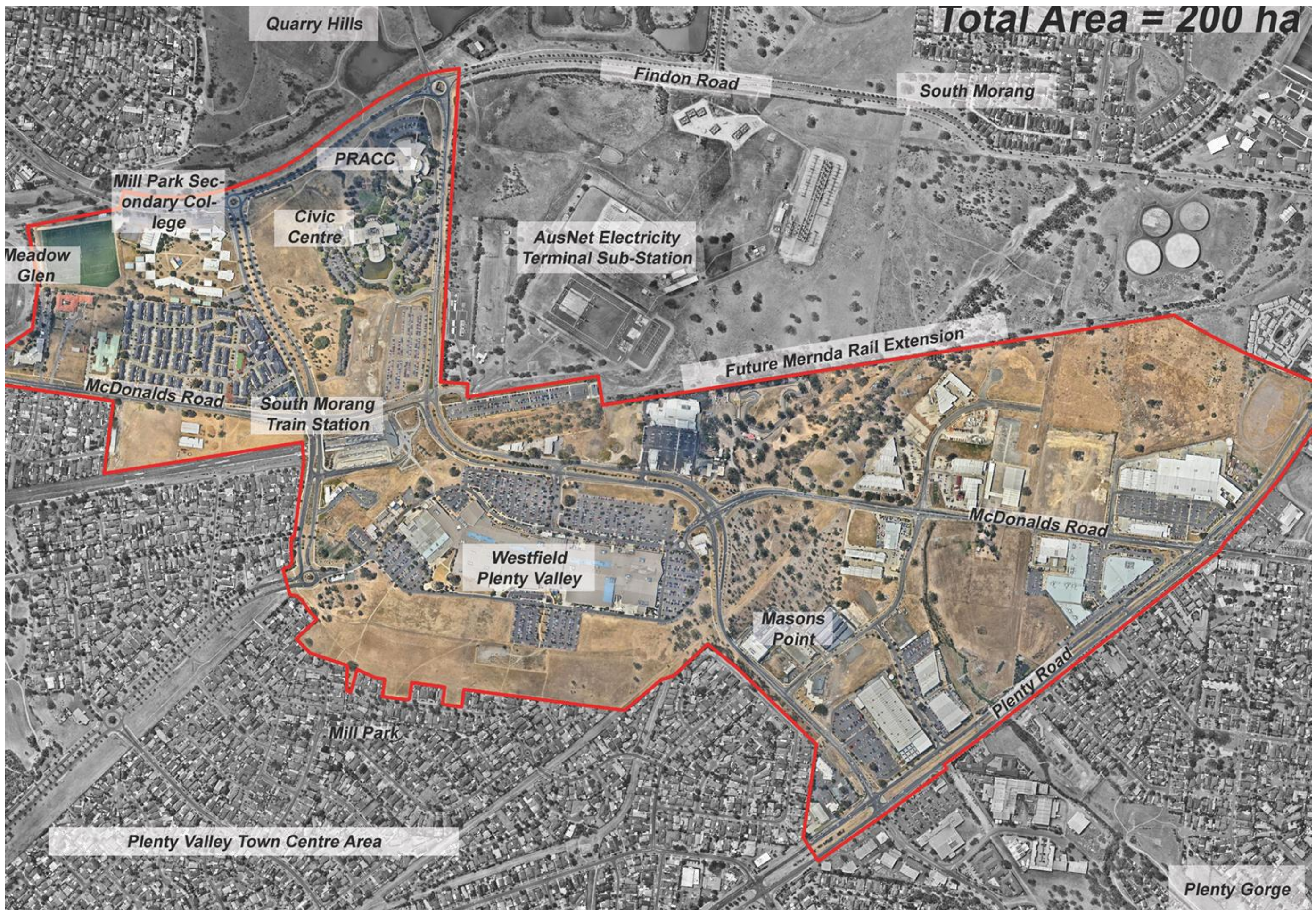
### Attachment 3: Aurora Development Plan Part 2

*Approximate location of subject site outlined in red*





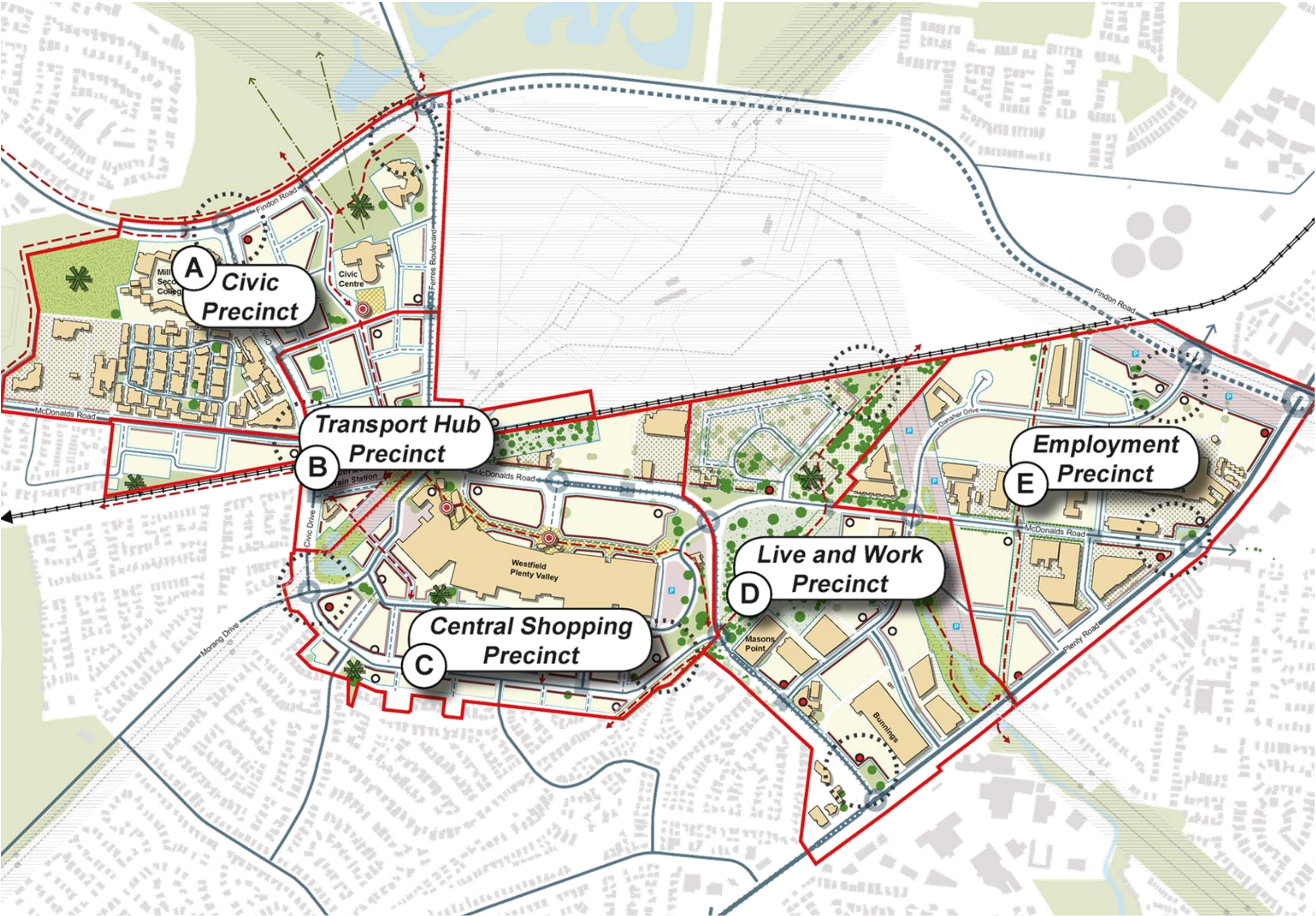
























**Disclaimer**

Information presented in the Plenty Valley Town Centre Structure Plan Draft Structure Plan is based on site analysis and visits undertaken from 2013 to 2015 and background reports by specialist consultants.

This analysis is provided as a broad resource only and while care has been taken to ensure the content of the report is accurate, we cannot guarantee that the report is without flaw of any kind. There may be errors and omissions or it may not be wholly appropriate for your particular purposes.

The City of Whittlesea accepts no responsibility and disclaims all liability for any error, loss or other consequence which may arise from reliance on any information contained in this report.

**City of Whittlesea - December 2016**

**Acknowledgment**

The City of Whittlesea would like to thank all who contributed to the preparation of Structure Plan and provided input and feedback during consultation processes.

Front Cover Illustration:  
Artist interpretation of proposed heritage park, incorporating partial exposure of the currently buried bluestone reservoir, Yan Yean Shared path, seating and other facilities.



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Henry Valley Town Centre Draft Structure Plan



1



## Executive Summary

The Plenty Valley Town Centre Structure Plan will provide a framework for planning and investment decisions for Plenty Valley Town Centre over the next 20 years. The City of Whittlesea (Council) have led the development of the Structure Plan in order to establish a coherent vision for the Town Centre and to enable its evolution to a highly integrated, mixed-use, vibrant centre supporting the Plenty Valley growth corridor.

As a designated Activity Centre within Plan Melbourne, the Plenty Valley will be required to have a key role in the concentrated provision of housing, commercial and community uses in proximity to transport nodes, services and sources of employment.

The Structure Plan also considers:

- The conservation and integration of River Red Gums and other natural assets,
- Where opportunities exist for the creation of public squares and parks,
- How the design of the built form is able to contribute positively towards enclosing and enlivening public streets and spaces,
- The creation of a 'pedestrian spine' connecting key activity nodes,
- The delivery of better road, cycle and public transport links.

The Structure Plan has been created in collaboration with key stakeholders, government agencies, Council officers, elected members and is underpinned by a vision for place drawn from early community engagement.

The Structure Plan will be implemented via a variety of means outlined in the Implementation Section of the report including incorporation into the Whittlesea Planning Scheme.









# Introduction

## INTRODUCTION

Plenty Valley Town Centre Draft Structure Plan

## 1.1 Structure Plan Area

Plenty Valley Town Centre is located at the heart of the Plenty Valley Growth Corridor. It sits between the established suburbs of Epping and Mill Park to the south and the more recently established suburbs of South Morang and Mernda to the north.

Although the Town Centre remains relatively undeveloped, a number of large-scale uses have become established over the last 20 years. This includes a number of government and educational facilities, including the City of Whittlesea's Civic Centre, which help to define the area's emergent identity as a Civic hub for the Plenty Valley and the wider municipality.

The Plenty Valley Town Centre is anchored by the Westfield shopping centre which draws customers from across a broad and rapidly growing catchment area. The shopping centre's future as a major destination will be further consolidated as Westfield intend to deliver a cinema, public space and restaurant cluster as part of ongoing long term development of the Town Centre.

The extension of the rail line to South Morang Station in 2012 created a valuable transport hub, catalysing a greater pace and variety of development and initiating a new style of mixed-use residential development within the Town Centre. Recent development at Mason Point integrates apartments and townhouses with commercial units at the ground floor and is focussed on providing a positive interface to the public street. In taking this approach, it exemplifies the kind of integrated outcome considered to be most supportive of the shared vision for the Town Centre.

The natural landscape continues to exert a strong influence on the character of the Town Centre with the significant number of remnant River Red Gums playing a critically important role in supporting this. The presence of the elevated Quarry Hills Regional Parkland to the north of the Town Centre further underpins this relationship to landscape, while Plenty Gorge regional parkland sits within close reach of the Centre's eastern boundary.

Infrastructure has also determined the built form and landscape of the Centre. Electrical easements and water pipetracks cut through the area, influencing the urban form while also providing opportunities for open space and movement. The current transport network prioritises vehicular movements through the Town Centre over more local walking and cycling modes of transport.



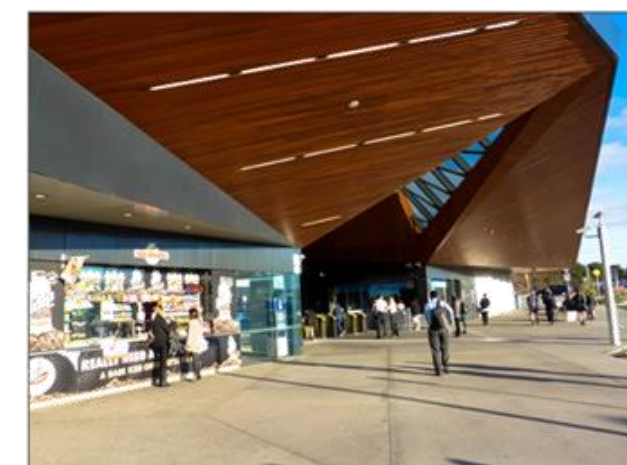
*River Red Gum*



*Mason Point*



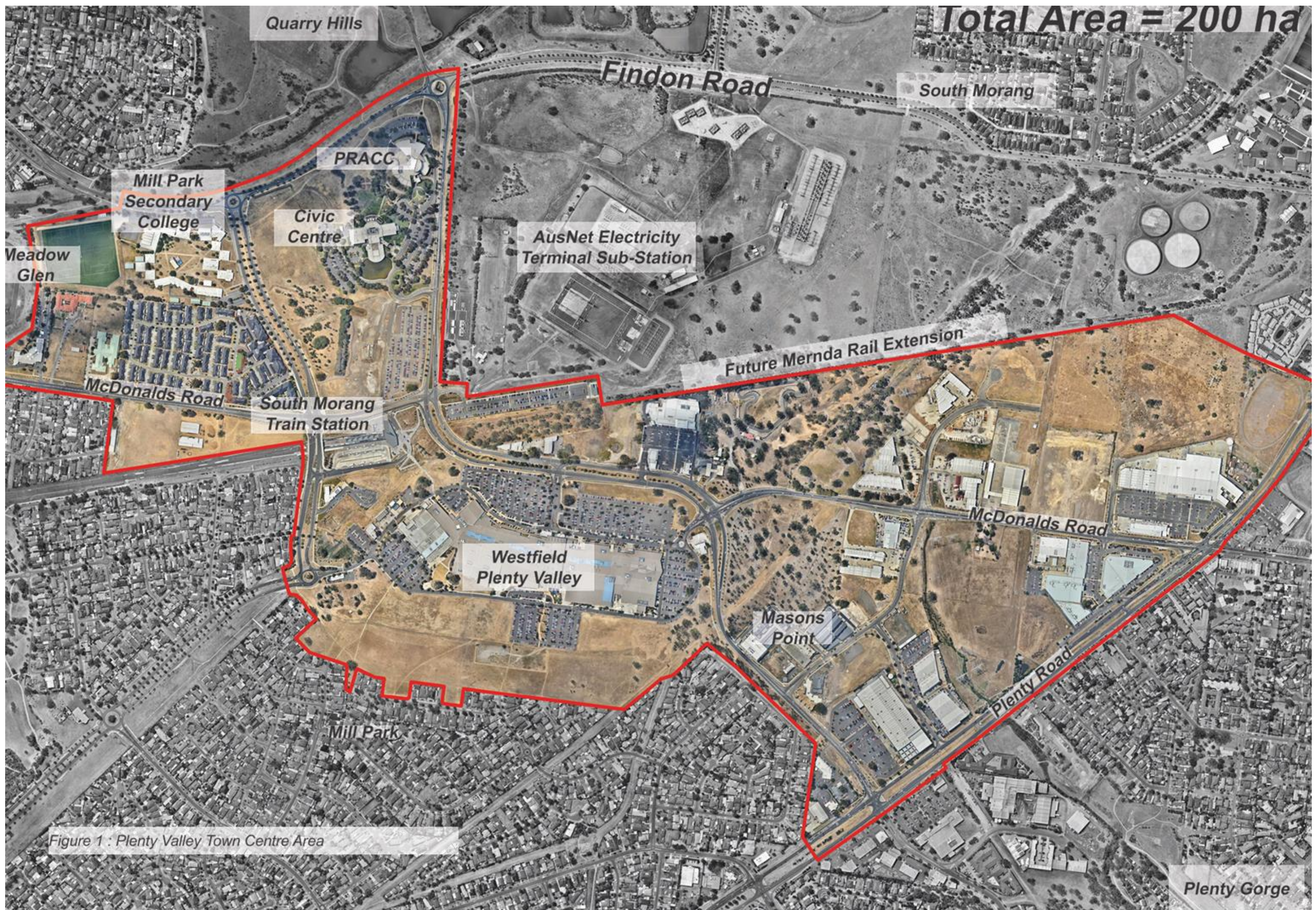
*Westfield Plenty Valley*



*South Morang Train Station*











## 1.2 Background

### Structure Plan Purpose

Plenty Valley Town Centre has been identified by the Victorian State Government as an Activity Centre in Plan Melbourne.

Activity Centres play an important role in providing access to employment, a diversity of housing and a wide range of goods and services. The primary purpose of an Activity Centre is to provide a focus for community, civic and commercial life in support of surrounding residential neighbourhoods.

Delivering a diverse, integrated mix of uses and activities is a complex task. The ultimate success of this is partly dependent upon a high level of strategic direction and the right set of statutory tools.

State Government policy requires that Structure Plans are prepared for activity centres in order to meet these objectives while ensuring that sustainable design and development principles are accounted for in their future growth.

The Structure Plan will provide both an evidence base and a statutory framework upon which planning and investment decisions will be made within the Town Centre to shape the next 20 years of the Centre's development.

The current statutory planning framework for Plenty Valley Town Centre falls short of facilitating the kind of development that meets state and local policy objectives. A Structure Plan is intended to marry these state and local objectives into a desired strategic direction for the area.

### A Rapidly Developing Place

Much of the Town Centre presently remains undeveloped. However, with the principal transport and services infrastructure now largely in place, development interest is increasing. Further investment in the extension of Findon Road through to Plenty Road and the railway line to Mernda in the coming years will only serve to provide more impetus for development activity.

The Structure Plan offers the City of Whittlesea an opportunity, during a pivotal moment in its growth as a place, to consider to what extent Plenty Valley Town Centre can achieve its potential as a focus for civic, community, and commercial life for the municipality. Balancing this growth against the Centre's sensitive environmental assets also poses a key challenge.

### Taking a Strategic Approach

A number of development projects are currently already planned for Plenty Valley Town Centre. As such, the strategic challenge of the Structure Plan is not so much to stimulate urban renewal as it might be in a more established area, but to ensure that individual projects are able to contribute to a cohesive and agreed shared vision.

One of the central aims of the Structure Plan is to identify where projects are able to support and enhance the Structure Plan's objectives in order to ensure that the Town Centre evolves into a cohesive, diverse and vibrant place.

## 1.3 Structure of this report

### Section 1: Introduction

The Purpose, Vision, Key Principles for Development and Spatial Directions are set out in Section 1.

### Section 2: Centre Wide Strategic Response

In Section 1, ten key principles are identified which are common to the whole of the Town Centre. Section 2 sets out the centre wide objectives, strategies and actions which support these principles. They are broadly arranged according to the three guiding themes established by the shared vision.

These include:

- **An attractive Town Centre** considers building design and form as well as public streets, squares and open spaces
- **A vibrant Town Centre** considers land uses and activities. It also addresses future community and recreation infrastructure provision.
- **An accessible Town Centre** considers the future movement network and includes sub-chapters on pedestrian and cycle networks, public transport and private vehicles.

Each of the three themed chapters set out the Structure Plan's Ten Key Principles for development in terms of the following:

- **Objectives** - A number of overarching target outcomes are described for each Key Principle.
- **Strategies** - Each objective is implemented by strategies which set out a range of approaches to achieving the key outcomes. Strategies will also provide the main point of reference in the shaping of planning decisions.

- **Actions** - Actions are specific measurable that relate to activities carried out in support of the objectives and strategies. Not all objectives and strategies have actions attributed to them as these policies will sometimes be considered sufficient to produce the desired outcome. Actions are assigned to a range of responsible parties.

- **Centre wide plans** - These are intended to be specific policy requirements and will be used to determine planning and investment decisions across the Town Centre. Where plans are identified as indicative then a level of discretion is possible, but only in specific circumstances substantiated within the Strategies section of each chapter.

### Section 3 : Precinct Strategies and Policies

A number of distinct precincts have been identified within the Town Centre. Section 3 sets out illustrative masterplans along with more detailed objectives and directions for each individual precinct. While the plans in this section are intended to guide development, a level of general accordance is possible should specific circumstances warrant a departure from the illustrated masterplans. These precincts will also inform the zone schedules.

A chapter is dedicated to each precinct within this section. These are as follows:

- **Civic Precinct**
- **Transport Hub Precinct**
- **Central Shopping Precinct**
- **Live and Work Precinct**
- **Employment Precinct**

## 1.4 Structure Plan Process

### Section 4: Implementation

The Final Section identifies the means of implementing the vision and actions identified in the Structure Plan. The implementation section identifies the following:

- incorporation of the Structure Plan into the Whittlesea Planning Scheme
- specific actions arising from the Structure Plan
- infrastructure items and funding mechanisms
- monitoring and review.

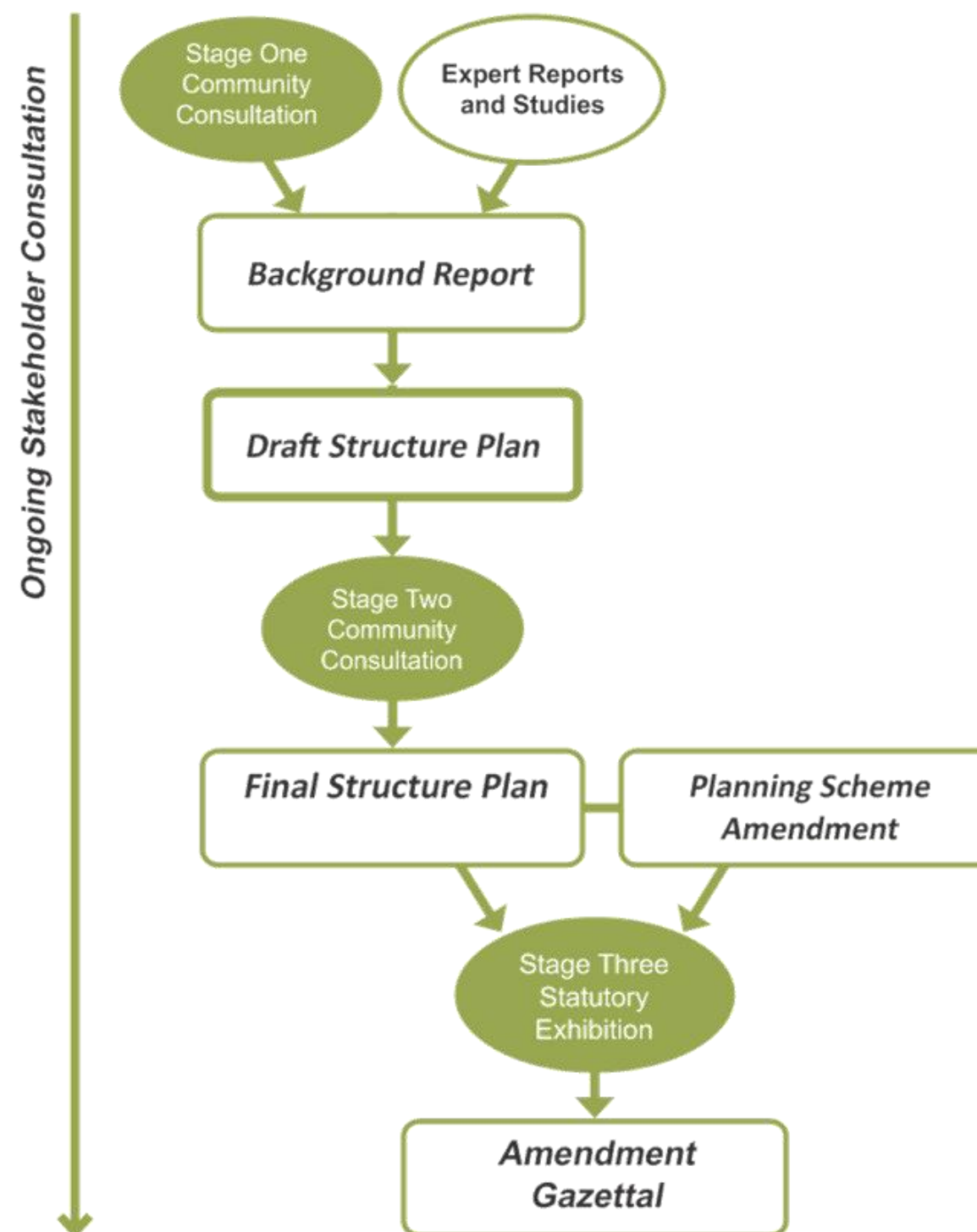
The Structure Plan process has been informed by the commissioning of a number of expert reports and studies. These were produced alongside extensive consultation which focussed on how the local community use the area and their general preferences for the Town Centre. The data gathered at this stage has been consolidated into a separate Background Report which forms the evidence base for the Structure Plan and should be considered its principal reference document. Rather than repeat the Background Report's findings this draft Structure Plan cites relevant sections in support of its principles.

The draft Structure Plan will be consulted on as part of a non-statutory exhibition process with stakeholders and the local community. The results of this consultation will feed into the production of a Final Structure Plan.

As well as providing strategic direction for the Town Centre, the Structure Plan will also contain a Statutory Framework and Implementation Program.

Following endorsement, the Structure Plan will inform the preparation of an amendment to the Whittlesea Planning Scheme. This will give statutory effect to the strategic direction of the Structure Plan. Both the Final Structure Plan and the Planning Scheme Amendment will be placed on Statutory Exhibition.

Depending on the nature and extent of submissions received, an independent Planning Panel may need to be arranged to consider unresolved issues. Ultimately the Minister for Planning will make the final decision on the approval of the Structure Plan.





## 1.5 The Vision

The Vision for the Plenty Valley Town Centre emerged through engagement with local stakeholders and the community. Three principal themes emerged from this consultation that highlighted the need for:

- **An Attractive Town Centre** - Protecting the landscape and built form character and improving public space.
- **An Accessible Town Centre** - Improved access to and within the Plenty Valley Town Centre by a range of transport modes
- **A Vibrant Town Centre** - More activity concentrated within the Plenty Valley Town Centre, particularly shops and entertainment.

These themes have been used to structure each of the main sections comprising the Structure Plan.

Stemming directly from these themes, the following vision sets out a preferred future for the Town Centre over the next 20 years and is intended to build upon the places' inherent strengths.

***“Plenty Valley Town Centre will have a unique civic and cultural identity supporting a vibrant mix of uses and activities.***

***It will be well connected, accessible via a range of transport options and have an attractive public realm with strong links to the natural landscape”.***



INTRODUCTION

Twenty Valley Town Centre Draft Structure Plan

### *An Attractive Town Centre...*



### *An Accessible Town Centre...*



### *A Vibrant Town Centre...*





## 1.6 Key Principles for Development

**Ten Key Principles** have been identified as being important for the future of Plenty Valley Town Centre. These principles relate directly to the outcome of community and stakeholder engagement as well as the key findings of the background report.

Each Key Principle relates to a theme and is supported by more detailed Objectives.

The Key Principles will guide development over the 20 year life of the Structure Plan.

### An Attractive Town Centre...

- Connected to the landscape
- High quality buildings
- Vibrant and active streets and spaces

#### 1. Maintain and Enhance Plenty Valley's Unique Landscape Character

Development will build on Plenty Valley Town Centre's strong connection to the landscape, with River Red Gums and Views to Quarry Hills underpinning its character.



#### 2. Expand and Connect Plenty Valley's Public Squares and Parks and Promote its Streets as Public Spaces

Visitors to the Plenty Valley Town Centre and a growing local community will be well provided for by a range of both urban and natural public spaces for gathering, play and recreation.



#### 3. Establish Excellence in Design Quality Across the Town Centre Area

Development of a medium scale will respond to the overall urban design context. Each part will make a positive contribution to the quality of place.



### An Accessible Town Centre...

- Fast and regular links
- A complete bike network
- Great streets for walking

#### 4. Promote Walking and Cycling Access to Plenty Valley

Many opportunities exist for increased walking and cycling to and within the Town Centre - increasing access and the health and well-being of residents.



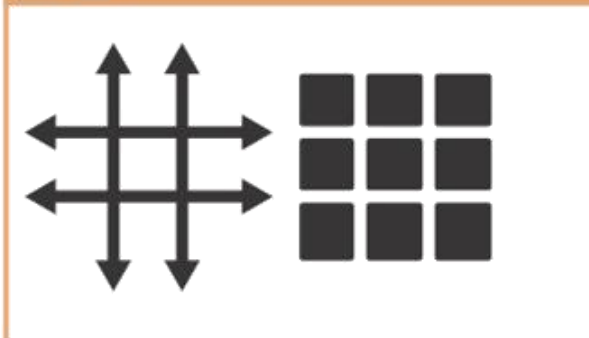
#### 5. Deliver an Integrated and Accessible Public Transport System

The extension of the rail line to Mernda, regular bus services and a tram connection to University Hill will eventually link the Town Centre across the Plenty Valley corridor.



#### 6. Provide a Permeable Street Network and Encourage a Modal Shift Towards non-Vehicular Forms of Transport

Improvements in existing transport infrastructure will improve each road's function and encourage walking, cycling and Public Transport.





A Vibrant Town Centre...

- The centre of community life
- A thriving centre for business
- A popular residential neighbourhood

**7. Provide Additional Housing Close to Shops, Services and Public Transport**

As new areas of housing are developed the population of the Town Centre will increase to a level where it is possible to support a broader range of local facilities and services.



**8. Support Local Trade and Employment and Economic Diversity**

As the Town Centre develops it will cater for the expansion of local jobs, accommodating a greater variety of employment opportunities within the outer suburban context.



**9. Support Plenty Valley's Emergence as a Major Civic Precinct within the Region by Clustering Key Local and Subregional Services**

The co-location of civic and community facilities at Plenty Valley will allow people to easily access a range of important services in a single trip.



**10. Support the Integration of Mixed Uses and Activities Across Plenty Valley Town Centre**

As the Town Centre evolves more people will live in close proximity to shops, workplaces and transport. Public spaces will be activated through their use for festival, market and community events.



## 1.7 Key Spatial Directions

The following spatial concepts together express the key overarching design and development principles which underpin the Structure Plan.

These concept plans provide an organisational framework and are a key reference point for the multiple themes used to structure this document.



**Activity Nodes** will be further enhanced with additional activities and public spaces to encourage people to visit and spend time in the Town Centre.



**Development Intensity Areas** will be encouraged to intensify within the areas up to 400m (5 minute walk) of Activity Nodes.



**A Pedestrian Spine** will provide a direct connection between Activity Nodes. Along this route the walking experience will be given the highest priority.

**Figure 2 : The Pedestrian Spine:  
Concentrating and Connecting Activity**



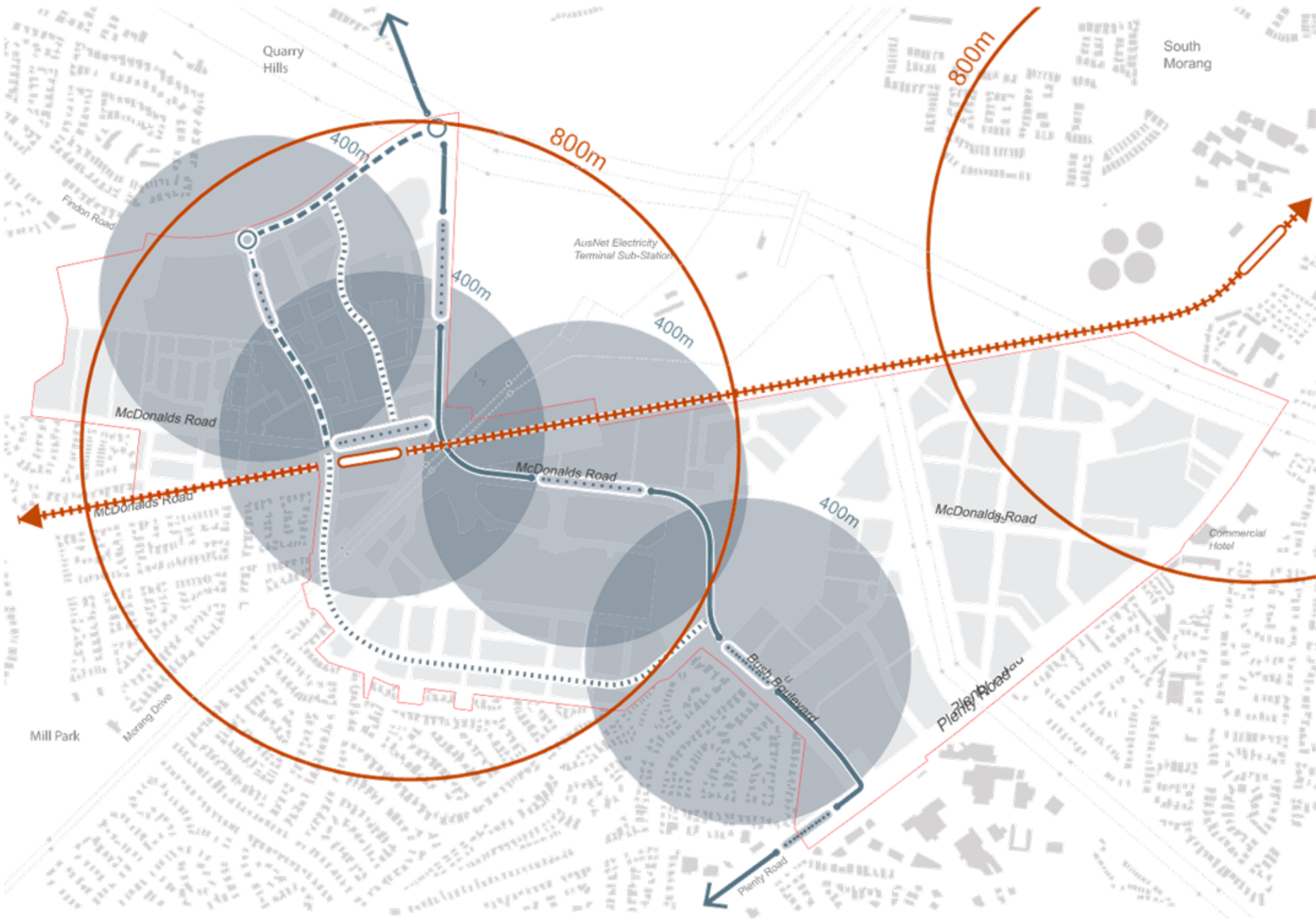


**Figure 3 : A Networked Landscape:  
Enhancing and Linking Open Spaces**





Figure 4 : An Easily Accessible Place:  
Linked by Public Transport



- The Railway Line will be Extended** to Mernda improving access and helping to reduce the number of car parks required for the existing station.
- An Additional Station** will be introduced north east of the Structure Plan area making the east of the Town Centre more accessible by Public Transport.
- New Tram Stops** will be located approximately 200m from one another and adjacent to the most in-demand destinations.
- The Tram Network Will Be Extended** from University Hill into the Centre.
- The tram could take a number of routes**, including along a newly extended Civic Drive.
- Development Intensity Areas** will be encouraged within close proximity to public transport connections.

# 1.8 Development Framework

## The Development Framework

The Development Framework provides a consolidated plan for the Structure Plan area. As such, it:

- Establishes the fundamental relationship between built form, movement, open space and public realm.
- Creates a flexible, legible and permeable development form for the Town Centre.
- Provides a spatial expression of the objectives and strategies outlined in the Centre Wide Strategic Direction section of the Structure Plan.
- Sets out how each of the conceptual plans contained in the Centre Wide Strategic Direction section of the document integrate with one another across the Town Centre.
- Brings together existing development plans and masterplans, establishing a strong spatial connection between previously unrelated visions for different parts of the Town Centre.
- Provides the strategic basis for each of the illustrative masterplans contained within the Precinct Plans section, ensuring that the centre wide character, movement and land use objectives are consistently applied throughout the Structure Plan area.

It is important that the Development Framework is not considered a predetermined and fixed built form outcome for the Town Centre. The development blocks and access points indicated in the plan are intended to be indicative of 'high level' built form.

While having a degree of flexibility at the centre wide scale, the Development Framework nevertheless sets out clear expectations in terms of the spatial objectives upon which the Structure Plan is based.

More detailed precinct plans in Section 3 provide greater clarity and direction at a more detailed scale.





Figure 5 : Development Framework



## INTRODUCTION

## Plenty Valley Town Centre Draft Structure Plan

**Transformative Projects**

Plenty Valley Town Centre will continue to evolve over the short to medium term with a range of Council, State and Private-sector interests planning a broad range of further investments into the area. In effect these projects are the principal levers through which the main objectives of the Structure Plan will be made possible.

**① Extension of Rail Line to Mernda**

The rapidly growing suburbs of Mernda and Doreen are presently reliant upon South Morang station as the end of line connection into the metropolitan railway network. Once the rail line has been extended to the site of the future Mernda Town Centre and additional station(s) provided at Marymede and Hawkestone, commuter pressure on the Town Centre's car parks and roads is expected to experience a significant decrease. The rail extension will also enable a larger catchment to access the Town Centre's services and facilities with greater ease, supporting the local economy. It is expected that services will be operating to Mernda by the end of 2019.

**② Extension and Future Duplication of Findon Road**

The completion of this key radial arterial route will ultimately allow for an efficient road connection from Plenty Road through to the Hume Freeway. Importantly for the Town Centre, its construction will have a significant impact on the volume of through traffic currently using McDonalds Road for the same purpose. With this road in place, many of the streetscape improvements contained in the Structure Plan will take on a new level of impetus, helping to affect modal shift towards more active modes of transport and improving the amenity of the Centre.

**③ Extension of 86 Tram from University Hill**

The potential extension of the 86 Tram from University Hill to Plenty Valley provides the opportunity to connect a number of City of Whittlesea's Town Centres. The connection will significantly improve the ability of residents to access the services and facilities afforded at each of the centres and centres further afield including the La Trobe Employment Cluster. This will significantly expand the housing, employment, education and retailing options and opportunities for residents.

**④ Development of the Civic Precinct Masterplan**

As the location for the headquarters of City of Whittlesea around half the jobs within the Town Centre area are presently associated with local government. The Council is also one of the major landowners making it a significant presence in the area. The Council is directly advocating for the further concentration of civic and community uses within the Town Centre and has prepared a draft Civic Precinct Masterplan, the finalisation of which would provide a catalyst for further high density, mixed use development within the Town Centre.

**⑤ Extension of Civic Drive**

One of the last remaining undeveloped areas, within close proximity to the train station, lies between the Westfield Shopping Centre and Buick Crescent. The opening up of this area to development with a new connector road would unlock its potential for higher density residential alongside commercial/retail uses and assist in providing the amount of housing

**⑥ McDonalds Road pedestrianisation**

Currently, McDonalds Road acts as a barrier for pedestrians between South Morang train station, Westfield Shopping Centre and the Civic Precinct. The downgrade of vehicular function of this road and prioritisation of pedestrians will significantly improve walkability throughout the centre and between activity nodes.

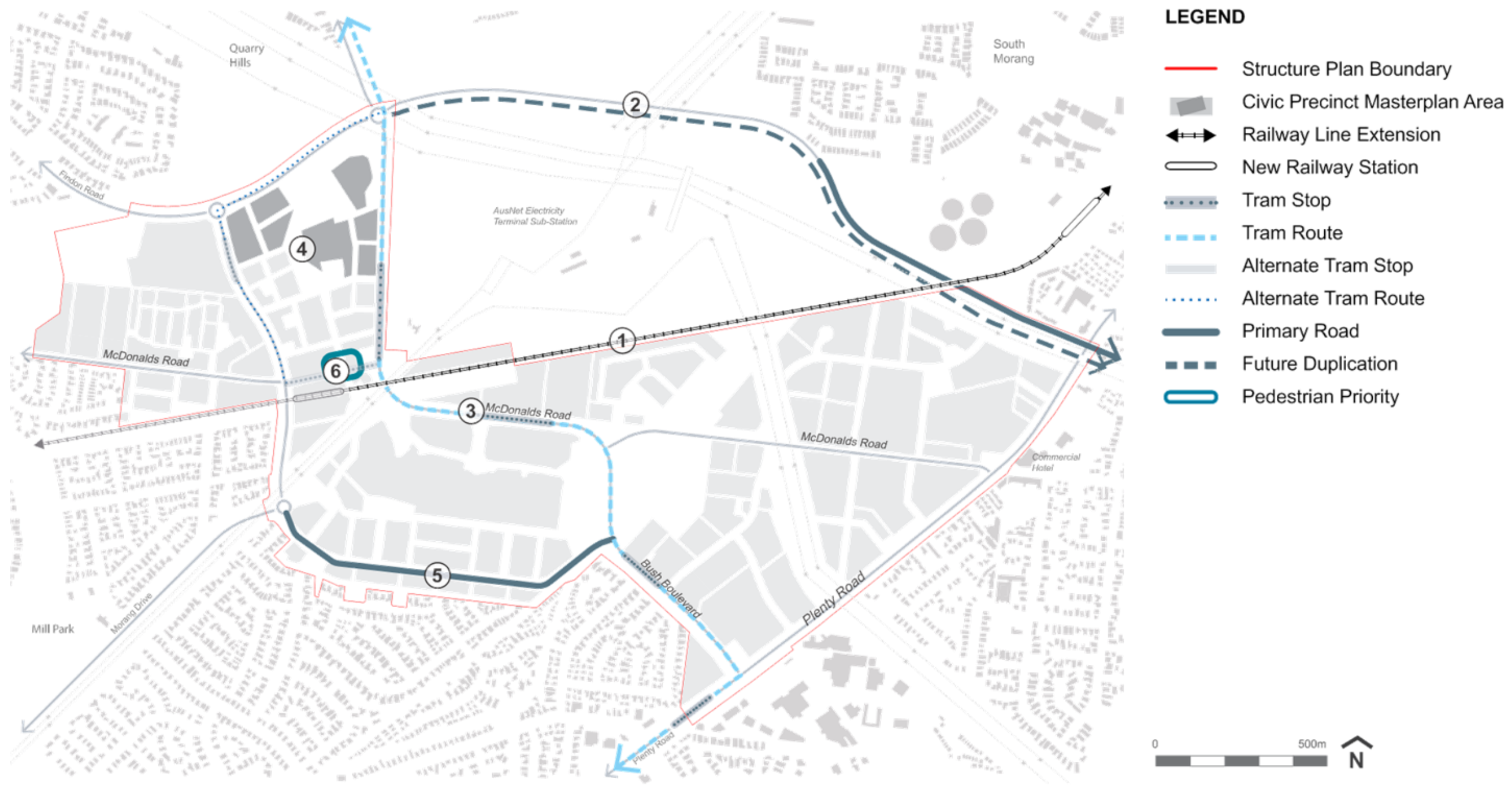


Figure 6 : Transformative Projects







# Centre-Wide Strategic Direction

## 2.1 An Attractive Town Centre

### Landscape Principle & Vision

#### 1 Maintain and Enhance Plenty Valley's Unique Landscape Character

The landscape character of Plenty Valley Town Centre will continue to have a strong basis in the presence of its natural features. River Red Gums will be sensitively and positively incorporated into public open spaces, streets and parks while new planting will provide visual relief and character to public streets and spaces.

The planting of landscaping will assist the Plenty Valley Town Centre in responding to the urban island heat effect and a changing climate.

Viewlines to the Quarry Hills will be preserved.

### Landscape Objectives and Strategies

**LA1:** *Retain and enhance existing River Red Gums in order to ensure their ongoing contribution towards the identity of the Town Centre.*

**LA1.1** Ensure existing remnant trees are retained within publicly accessible streets and open spaces. In the exceptional circumstances where retention is not an option then offsets should be accommodated within the Town Centre area.

**LA2:** *Protect and Sensitively Incorporate Areas of High Landscape and Biodiversity Value into the Town Centre.*

**LA2.1** Retain areas in the Town Centre that have environmental values such as the Conservation Reserves and areas of open waterway.

**LA2.2** Ensure a sensitive visual interface with the Conservation Reserves to protect the contribution they make in terms of 'borrowed character'.

**LA2.3** Carefully manage public interaction with conservation reserves and limit physical connections through them to only those set out in the Structure Plan, or in an approved Conservation Management Plan.

**LA2.4** Ensure key viewlines towards Quarry Hills parkland are retained from the public space adjacent to the Civic Centre (Figure 7).

**LA2.5** Carefully design effective stormwater management systems to integrate into the existing landscape and public realm as a visual and environmental asset.

**LA3:** *Enhance the Character and Legibility of the Town Centre through the Landscaping of Streets and Reserves.*

**LA3.1** Promote the planting of street trees across the Town Centre to enhance canopy cover and provide shade and reduce the urban heat island effect.

**LA3.2** Ensure that key boulevard streets into the Town Centre feature strong avenue planting through central medians and nature strips/areas of parallel parking (Figure 7).

**LA3.3** Promote and maintain a landscape street edge character to McDonalds Road to strengthen the landscape character and to mitigate against visual impact of large setbacks or areas of car parks and display areas.

**LA3.4** Require low-level landscaping (up to 3m in height) within electrical easements to ensure non-developable land makes a positive visual contribution towards the Town Centre.

**LA3.5** Ensure that the impact of parking is softened by appropriate landscaping, particularly within visually sensitive and open areas such as pipetracks. Car parking areas should have a minimum provision of one space of landscaping per eight parking spaces.

### Landscape Actions

#### Action

**LAA1:** Develop a strategic landscaping planting plan to determine an approach to tree planting for streets and spaces across the Centre.

**LAA2:** Amend the Conservation Management Plan for the Central Conservation Reserve in order to incorporate a pedestrian connection to Mason Point.

**LAA3:** Apply for Living Rivers funding to support the further embellishment of Hendersons Creek and other creeks and wetlands in the Town Centre.

**LAA4:** Undertake an assessment of Heath Courts Drain to establish its biodiversity values and the potential for incorporating Water Sensitive Urban Design into a publicly accessible open space.

**LAA5:** Develop guidelines for the incorporation of River Red Gums into useable public spaces as part of a review of River Red Gum policy.



Figure 7 : Landscape Plan



## Public Space Principle & Vision

### 2 Expand and Connect Public Squares and Parks and Promote Streets as Public Spaces

As the Town Centre develops, existing and future streets will facilitate a greater level of comfort and safety for pedestrians, promoting their use as vibrant public areas. A pedestrian spine connecting key nodes of activity will be the focus for public realm improvements and will feature new squares at key destinations. Other developments across the Town Centre will help facilitate the development of public spaces with varied purpose and character.

Comfortable connections into and between existing parkland and open spaces will emerge. These routes will take the form of tree-lined streets and new pathways utilising easements and reservations.

A stronger connection between Quarry Hills and Plenty Gorge will be assisted by a network of well signposted routes encouraging cycling and walking through safe, comfortable and interesting streets and parks.

## Public Space Objectives and Strategies

### PS1: Provide Safe and Accessible Parks for Residents, Workers and Visitors

**PS1.1** Ensure sufficient designation and delivery of passive and active open spaces to meet the needs of local residents. Provision of open space will be consistent with any relevant sections of Councils Open Space Strategy.

**PS1.2** Deliver a high standard of landscape design in new parks which accounts for natural and physical features of visual and historic interest.

**PS1.3** Ensure buildings address parks and open spaces positively by providing activated edges and passive surveillance as depicted in Figure 8.

Large areas of accessible parkland in the Town Centre will include:

- ① **Mill Park Sports Pitches** - The facility contains two synthetic soccer pitches association with secondary school and operates with a shared user agreement with the Department of Education. The facility adjoins the Meadowglen International Athletics Stadium. Removal of existing barb wire fencing and new pathways will improve accessibility.
- ② **The Common Green** - Open Space area located in proximity to the Quarry Hills Regional Park. Retention and enclosure of the existing open space with retained views towards the Quarry Hills will ensure that large scale civic events are able to continue to use it. The precise size and function of this space will be determined as part of an ongoing masterplan process.
- ③ **Pipehead Reservoir** - Open space area which forms part of the Yan Yean Pipetrack. Utilising the historic bluestone reservoir will create a feature of a new one hectare park within the Yan Yean Pipetrack.

### PS2: Introduce New Urban Squares as Gathering Spaces

**PS2.1** Promote the establishment of urban spaces that respond to and support the local context and are capable of hosting a variety of uses and activities.

**PS2.2** Ensure public spaces are designed to support social gathering and interaction by providing safe, attractive and comfortable environments. Public spaces should feature active edges, places for sitting, playing and for performance.

**PS2.3** Ensure buildings address and enclose urban squares with > 90% activated edges (Figure 8).

**PS2.4** Key Urban Spaces along the Pedestrian Spine include:

- (A) **The Community Plaza** - Located on the approach from the Train Station towards the Civic Centre, this urban space will function as a primary civic space for the municipality and will provide direct access to Council and Regional Civic and Community Facilities.
- (B) **The Train Station Plaza** - As the area of the Town Centre with the greatest pedestrian activity this space will be enlarged and feature more active edges to encourage people to linger and undertake less necessary activities.
- (C) **The Entertainment and Leisure Precinct** - This space acts as a gateway between the train station and the shopping centre's main street and mall. Activity will be generated by the multiple restaurants and a cinema which form its edges.
- (D) **The Restaurant Cluster** - Already a popular destination, this will form a more complete public space as it becomes the central node within a new main street.



*Dandenong provides a good example of a successful Civic Square*



*Peppercorn Park at University Hill provides an example of a Public Open Space that is positively addressed and overlooked by residential buildings*





Figure 8 : Proposed Open Space Plan

## Public Space Objectives and Strategies

### PS3: Ensure Parks and Open Spaces are Linked to form an overall Network

**PS3.1** Sensitively utilise utility easements to form a network of linking open spaces to support connectivity across the Town Centre. Where possible provide community facilities and embellishments.

**PS3.2** Strengthen visual and physical connections to and between the Town Centre, Quarry Hills Parkland and the Plenty Gorge Parkland.

**PS3.3** Create stronger connections between natural features within the Town Centre to form a green network of landscaped open spaces and parks.

**PS3.4** Introduce wayfinding elements into the public realm to direct people towards key destinations and pathways.

### PS4: Design for Safe and Active Public Spaces

**PS4.1** Require buildings to sensitively address pipe tracks, conservation areas and other open spaces in order to activate these spaces and provide passive surveillance.

**PS4.2** Ensure that buildings address streets with finer grain uses to increase activation at street level, particularly along the pedestrian spine.

**PS4.3** Ensure building entry points are highly visible and accessible from public streets, squares and open spaces.

**PS4.4** Encourage passive surveillance from upper levels of buildings by providing windows and balconies facing public streets, squares and open spaces.

**PS4.5** Clearly delineate the hierarchy of public open space, shared and private open space via fencing, landscaped buffers, level and material changes.

**PS4.6** Encourage buildings provide sufficient shelter from the climate to reduce the impact of wind, rain and sun on the useability of key public spaces.

**PS4.7** Ensure public spaces are designed for people of all ages and abilities.

### PS5: Ensure Streets are Designed for People

**PS5.1** Ensure buildings address and enhance the amenity of the pedestrian spine with a consistent building line and highly activated edge at the street interface. Setbacks (Figure 13) must be minimal to provide a high degree of enclosure.

**PS5.2** Emphasise the key pedestrian network through high quality, cohesive public realm treatments.

**PS5.3** Ensure pavement widths are sufficient to accommodate on-street dining where restaurants and cafes are located adjacent to public streets.

**PS5.4** Provide simple and direct connections between walking routes and public spaces.

**PS5.5** Provide a high level of amenity, accessibility and comfort for pedestrians for instance by requiring buildings to provide awnings over footpaths on to provide weather protection.

**PS5.6** Require that streets are designed in response to the natural and built context, incorporating natural features where present.

**PS5.7** Ensure that building street wall height to street width ratios are appropriate to the context to ensure a proportionate sense of enclosure (Figure 14).

**PS5.8** Ensure that streets are designed appropriately and according to their role (Figure 9) in facilitating movement and forming people-orientated places across the Town Centre. For example, higher order streets will be more focussed on movement, while local residential streets will have more of a place function.

## Public Space Actions

### Action

**PSA1** : Work with key agencies and developers to introduce active recreation and playspaces into the Yan Yean pipetrack.

**PSA2** : Develop public art strategies for the Town Centre.

**PSA3** : Negotiate a revised shared user agreement with the Department of Education to remove fencing and improve community access to the Mill Park Sports Pitches.

**PSA4** : Prepare a streetscape design framework for arterial, sub-arterial and collector streets which emphasise the public realm function of the street.

**PSA5** : Undertake a competitive design process for the provision of a civic space to be delivered as part of the Civic Precinct expansion.

**PS5.9** Redevelop existing streetscapes to improve their accommodation of different transport modes according to their role and their contribution to the public realm as public spaces and landscape corridors.

**PS5.10** Utilise generous nature strips to increase the amount of landscaping, green areas to improve amenity for pedestrians and reduce the urban heat island effect.





Figure 9 : Street Design Outcomes

*Design Principle and Vision*

### 3 Establish Excellence in Built Form Design Quality

As Plenty Valley Town Centre continues to develop the overall density of the Town Centre will increase and become more concentrated within walking distance of services, jobs, shops and public transport. Design guidelines established through the Structure Plan will safeguard existing and future amenity and ensure the ongoing enhancement of the public realm. The built form will be developed at a medium scale (2 to 8 stories) consistent with the Town Centre's context and setting.

New buildings will increasingly engage with public spaces by activating the edges of streets and squares. New buildings and structures will also adopt a higher standard of architectural finish, particularly where they form gateways to the Town Centre, anchor public spaces or act to terminate views and vistas. Minimal setbacks will facilitate the ability for new buildings to enclose and engage with public space. Best practice Environmentally Sustainable Design (ESD) elements will be encouraged to be incorporated into the design of new buildings.

*Design Objectives and Strategies*

#### DES1: *Protect and Enhance Amenity for Existing and Future Residents*

**DES1.1** Ensure building design responds sensitively to the amenity of established and future residential housing, particularly in relation to siting, height and massing of built form.

**DES1.2** Ensure that where the Town Centre boundary interfaces with rear boundaries of existing low-scale residential, a specific maximum height of 2 stories is applied to minimise overlooking, overshadowing and to minimise visual impact.

**DES1.3** Ensure new residential development incorporates sufficient useable private open space for residents.

#### DES2: *Ensure Building Height and Massing is Appropriate to its Context*

**DES2.1** Visual bulk should be minimised through the articulation and detailing of building façades with a public interface.

**DES2.2** Areas where variations in built form might be considered include key gateways, at the termination of view-lines or within close proximity of public transport nodes.

**DES2.3** Preferred minimum building heights vary from 2 to 8 stories across the Town Centre in accordance with Figure 11. Buildings with fewer storeys may be supported but only in those circumstances where significant employment generating uses are proposed and where the general design quality remains of the standard required by the Structure Plan. Single storey buildings or underdevelopment of sites identified for 'key marker and/or corner buildings' will not be supported.

**DES2.4** Ensure buildings set upper stories back from the street in accordance with Figure 14 to provide for an attractive streetscape and prevent overshadowing

**DES2.5** Development which delivers a significant public benefit (such as social housing and public open space) may be considered up to the maximum building height specified for the sub precinct on any site subject to complying with the built form outcomes and all relevant design objectives. This provision does not apply to land with a sensitive interface.

**DES2.6** It must be clearly demonstrated that amenity impacts upon adjacent areas of open space, housing and other sensitive land uses have been adequately addressed. Planning applications should have regard for:

- » Overshadowing,
- » Loss of Daylight; and,
- » Overlooking

**DES2.7** Building heights which have a proportionate relationship to the surrounding context and sit within the parameters indicated in Figure 11 and Figure 13 will generally be supported.

#### DES3: *Provide a High Standard of Architectural Design and Minimise Visual Clutter*

**DES3.1** Integrate signage within building design and restrict the use of large pylon signs.

**DES3.2** Prevent the use of large billboards or standalone signs.

**DES3.3** Encourage high quality contextual design that strengthens the Town Centre's character.

**DES3.4** Avoid unarticulated and inactivated building facade interfaces to public streets, squares and open spaces. Visual interest should be provided through:

- Breaking up the length with windows and doors;
- Strong vertical and horizontal elements;
- Façade articulation; and
- Varied façade materials.

**DES3.5** Screen unattractive interfaces with transmission line pylons and easements and the SP Ausnet Terminal Station where possible.

**DES3.6** Minimise visibility from public spaces of storage and refuse areas, service infrastructure, plant and equipment behind buildings, landscaping buffers or visual screening.



## Design Objectives and Strategies

### DES4: Support a Unique Identity For the Centre

**DES4.1** Ensure development at Gateway Locations (Figure 15) and sites identified for 'key marker buildings' (Figure 13) incorporate strong architectural forms or elements to create a sense of arrival and identity into the Town Centre.

**DES4.2** Pursue opportunities as part of development for the provision of local public art in the Town Centre to strengthen its community focus and enhance its sense of place. Placement of artworks will be encouraged within Public Squares and Parks (Figure 8) or at Gateway Locations (Figure 15).

**DES4.3** Promote an understanding of heritage and conservation values through interpretation and the sensitive incorporation of existing historic and landscape features, such as the Yan Yean Water System, into public space.

### DES5: Enhance the Legibility and Quality of Streets and Spaces

**DES5.1** Encourage minimal building setbacks to facilitate a high standard of enclosure to public streets, squares and open spaces.

**DES5.2** A high standard of enclosure to public streets, squares and open spaces must be considered.

**DES5.3** Discourage the development of private internal malls.

**DES5.4** Ensure buildings provide for active frontage and engage with the street to provide a vibrant and safe street.

**DES5.5** Avoid locating car-parking and loading bays in the front building setback.

### DES6: Design for a Sustainable Future

**DES6.1** Ensure new developments within the Town Centre support sustainability outcomes with the application of Environmentally Sustainable Design standards. Significant development proposals are to include a Sustainable Design Assessment and Sustainability Management Plan which addresses the following:

- Energy efficiency
- Transport
- Climate change adaptation
- Integrated water management
- Waste management
- Urban ecology
- Indoor environment quality
- Building materials
- Site and ongoing building management
- Innovations

**DES6.2** Require new significant developments to prepare a 'Green Travel Plan' to minimise the negative impact of travel and transport on the environment.

**DES6.3** Promote the principle of flexibility in the design of buildings to ensure they are adaptable and able to respond to changing demands in the future.

**DES6.4** Promote the integration of landscaping into built form through the use of 'green-walls' and other means of softening appearance.

**DES6.5** Work with residential developers to support development in the Town Centre to lead in sustainable design and technology innovation, at the development and precinct scale to improve environmental sustainability.

**DES6.6** Encourage new housing to be designed to maximise energy and water efficiencies to reduce living costs.

**DES6.7** Implement co-ordinated processes between developers and Council's statutory, strategic and sustainability planners to identify environmentally sustainable design and development options to help streamline the planning process.

## Design Actions

### Action

**DESA1** : Implement new built form controls in a schedule to the Planning Scheme in accordance with the design principles and guidelines identified in the Structure Plan.

**DESA2** : Introduce a Planning Application requirement for Sustainable Design Assessment (SDA) or Sustainability Management Plans (SMP) to accompany significant planning applications.

**DESA3** : Investigate the use of annual design awards for excellence for developments within the Town Centre.



CENTRE-WIDE STRATEGIC DIRECTION

Wentworth Valley Town Centre Draft Structure Plan

Sub-Precinct	Preferred Building Height	Maximum Building Height	Built Form Outcomes
1a	6 storeys (<21m)	8 storeys (<28m), site must be a key corner site	Viewlines to the Quarry Hills must be preserved.
1b	4 storeys (<14m)	6 storeys (<21m), site must be along an arterial or connector road (Figure 21)	Development should transition to lower scale development on local roads.
2a	8 storeys (<28m)		Development must transition to 2 storeys (<7m) at sensitive residential interfaces
2b	8 storeys (<28m)		Denser development, with key marker buildings at specified locations requiring high quality architectural design.
3a	6 storeys (<21m)	8 storeys (<28m), site must be along non-sensitive interfaces (e.g. power lines), adjoin an arterial road or be a key corner site	The scale of development in this area should take advantage of the available land, while maintaining permeability and a pedestrian oriented built form.  Standard "big box" developments should be wrapped and/or capped with active uses to limit blank walls facing the public realm.
3b	6 storeys (<21m) north of Civic Drive 4 storeys (<14m) south of Civic Drive		The scale of development in this area should take advantage of the available land, while maintaining permeability and a pedestrian oriented built form, as well as being compatible with the surrounding low-scale character  Development must transition to 2 storeys (7m) at sensitive residential interfaces
4a	4 storeys (<14m)	6 storeys (<21m), site must be a key corner site	Development must retain and integrate existing River Red Gums
4b	6 storeys (<21m)	8 storeys (<28m), site must be a key corner site	Standard "big box" developments should be wrapped and/or capped with active uses to limit blank walls facing the public realm.  Development must transition to 2 storeys (<7m) at sensitive residential interfaces
5	4 storeys (<14m)	6 storeys (<21m), site must be along non-sensitive interfaces  8 storeys (<28m), site must be a key corner site	Standard "big box" developments should be wrapped and/or capped with active uses to limit blank walls facing the public realm.
All	Development which delivers a significant public benefit (such as social housing and public open space) may be considered up to the maximum preferred building height specified for the sub precinct on any site subject to complying with the built form outcomes and all relevant design objectives to the satisfaction of Council. This provision does not apply to land with a sensitive interface.		

Figure 11 : Building Height Controls

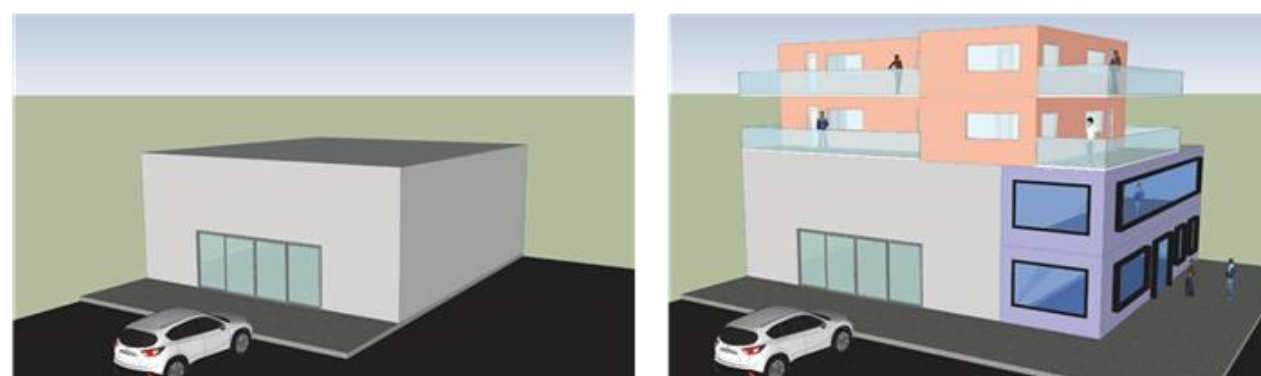


Figure 12 : Example of Wrapping and Capping of Standard "Big Box" Developments

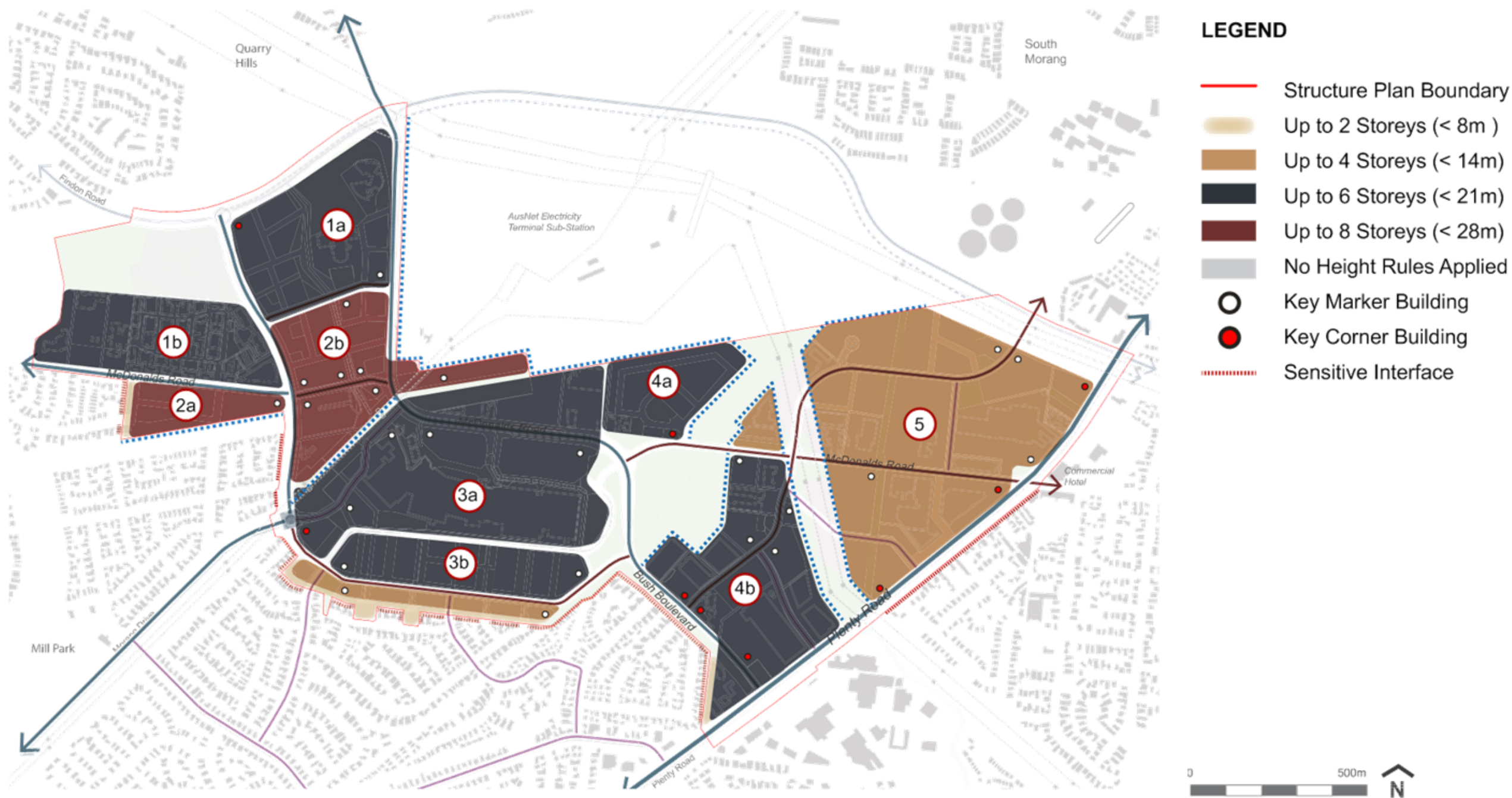


Figure 13 : Indicative Building Heights Plan



## CENTRE-WIDE STRATEGIC DIRECTION

Henty Valley Town Centre Draft Structure Plan

Design Element	Requirement	Built Form Outcomes
Street wall height	Street wall height must be no more than 6 storeys (<21m) on arterial and connector roads, and no more than 4 storeys (<14m) on local roads. Refer to Figure 21 for Street Hierarchy.  Where development occurs along a pedestrian priority street, the street wall height should be no more than 2/3 the width of the road reserve.	Street wall height is scaled to ensure: <ul style="list-style-type: none"> <li>daylight and skyviews from the street;</li> <li>minimal impacts of overshadowing;</li> <li>an appropriate sense of enclosure; and</li> <li>a human scale</li> </ul>
Upper level street setback	Where a building height exceeds the maximum street wall height, upper level setbacks must apply. Above the street wall height, upper levels must be set back a minimum of 5 metres from the street wall.	Buildings are setback at upper levels to ensure: <ul style="list-style-type: none"> <li>larger buildings do not visually dominate the street or public space;</li> <li>provision of adequate light, and privacy from habitable rooms, for both existing and proposed developments;</li> <li>minimal impacts of overshadowing and overlooking; and</li> <li>a consistent street wall scale is maintained.</li> </ul>
Front setback	Buildings should be setback from the front property boundary (or built to property boundary where applicable) as specified in Figure 15.  On all other streets, buildings must be setback consistent with the adjoining buildings, or a maximum of 7m, whichever is the lesser.	Buildings are located to ensure: <ul style="list-style-type: none"> <li>adequate passive surveillance and opportunities for activation of the public realm, especially when capping/wrapping "big box" developments (Figure 12);</li> <li>a consistent street wall for a pleasant pedestrian environment; and</li> <li>where applicable, an adequate landscape buffer to screen light industrial uses</li> </ul>
Setback(s) from side boundary	Where a building height exceeds the street wall height, upper levels of a building must be setback a minimum of 5 metres from a side boundary.  This requirement does not apply at sensitive interfaces (see below).	Buildings are setback from side boundaries at the upper levels to ensure: <ul style="list-style-type: none"> <li>provision of adequate light, and privacy from habitable rooms, for both existing and proposed developments;</li> <li>adequate passive surveillance and opportunities for activation of the public realm, especially when capping/wrapping "big box" developments (Figure 12);</li> <li>provision of adequate light to narrower local roads and laneways; and</li> <li>minimal impacts of overshadowing and overlooking</li> </ul>
Setback from rear boundary	Where the rear boundary is a local road or laneway, upper levels above 3 storeys (>10.5m) must be set back a minimum of 5 metres from a rear boundary. Where the rear boundary is at a non-sensitive interface (e.g. train lines and power lines), no setbacks are required.	Buildings are setback from rear boundaries at the upper levels to ensure: <ul style="list-style-type: none"> <li>provision of adequate light, and privacy from habitable rooms, for both existing and proposed developments;</li> <li>opportunities for passive surveillance of less active rear interfaces; and</li> <li>provision of adequate light to narrower local roads and laneways</li> </ul>
Sensitive interfaces	Where the side and/or rear boundary is a sensitive interface, buildings should be setback a minimum of 9m from the property boundary. Upper levels above 2 storeys (>7m) should be setback a minimum of 7m for every additional 2 storeys (7m) in height.	Buildings are setback from side and rear boundaries at sensitive interfaces to ensure: <ul style="list-style-type: none"> <li>provision of adequate light, and privacy from habitable rooms, for both existing and proposed developments;</li> <li>minimal impacts of overshadowing and overlooking</li> <li>where applicable, taller buildings transition down in height to adjacent areas that have a lower height limit, so as not to visually dominate the character of adjacent existing low-scale development areas.</li> </ul>

Figure 14 : Building Height Controls





Figure 15 : Indicative Building Setbacks and Gateways Plan

## 2.2 An Accessible Town Centre

### Walking and Cycling Principle & Vision

#### 4 Promote Walking and Cycling Access to and within Plenty Valley Town Centre

All precincts within the Town Centre will be connected by a comprehensive network of safe, interlinked pathways utilising streets, public spaces and utilities easements. Footpaths will be available along either side of main roads, be well-lit with regular road crossing opportunities and associated with key lines of desire. The highest quality walking route will be along a pedestrian spine linking all of the key nodes of activity.

Access from surrounding neighbourhoods will be less dependent on car transport and people will be more inclined to make walking journeys between key areas of activity.

Cycling provision across the Town Centre is currently very low with opportunities largely restricted to main roads. The incomplete path network also contributes to low cycling numbers although many opportunities exist to implement high quality local and regional cycling and shared path links. A focus of the Structure Plan will be to increase cycling connections to key destinations and transport infrastructure while also improving the useability of existing paths.

### Walking and Cycling Objectives and Strategies

#### WC1: Focus the Pedestrian experience along a Central spine

**WC1.1** Ensure that along the route of the Pedestrian Spine, as identified in Figure 2 on page 17, pedestrian movements are given the highest priority.

**WC1.2** Access to the pedestrian spine must be available 24 hours a day to ensure ongoing direct connections between public transport, leisure and entertainment precincts and local residences.

**WC1.3** Ensure street edges along the central spine are positively activated and addressed on both sides to provide for an engaging pedestrian experience.

**WC1.4** Locate the Town Centre's principal public squares along the central spine to concentrate activity and interest in frequently visited locations.

#### WC2: Complete a Permeable Pedestrian network

**WC2.1** Ensure footpaths are provided concurrent with development, on both sides of the street.

**WC2.2** Utilise existing infrastructure easements to facilitate local and regional walking routes to enable access from surrounding areas.

**WC2.3** Introduce signalised pedestrian crossings on boulevard and connector streets where the core pedestrian movement network requires them (Figure 16) within close proximity of active uses.

**WC2.4** Provide publicly accessible and obvious pedestrian connections through development blocks and areas of open space. Intervals of no larger than 100m should be aimed for as an ideal maximum distance between connections.

#### WC3: Prioritise Pedestrian Movement Within Public Streets

**WC3.1** Ensure that the safety and amenity of pedestrians is prioritised when designing or assessing the design of new streets.

**WC3.2** Encourage the redesign of existing roads to better facilitate access by walking, cycling and public transport and to create a safer environment for pedestrians.

**WC3.3** Ensure public transport investment such as new tram or bus stops facilitates improvements to the pedestrian movement network, particularly street crossings.

**WC3.4** Design boulevard and connector roads to balance provision for users and maximise space for pedestrians.

**WC3.5** Utilise on-street parking and separated cycle lanes as a means of creating a buffer between moving vehicles and pedestrians and slowing vehicle speeds.

**WC3.6** Encourage pedestrian activity and multiple trips through improved wayfinding and pedestrian network designed for all ages and abilities.

### Walking and Cycling Actions

#### Action

**WCA1:** Utilise a variety of funding sources to complete key missing pedestrian infrastructure aligned with 'Missing Links' strategy, across the Town Centre.

**WCA2:** Install safe crossing points on major roads to link key activity generating uses and provide access from the Town Centre to the Quarry Hills and Plenty Gorge regional parklands.

**WCA3:** Implement street upgrades including the introduction of additional street trees, furniture, wayfinding and public art along key routes.



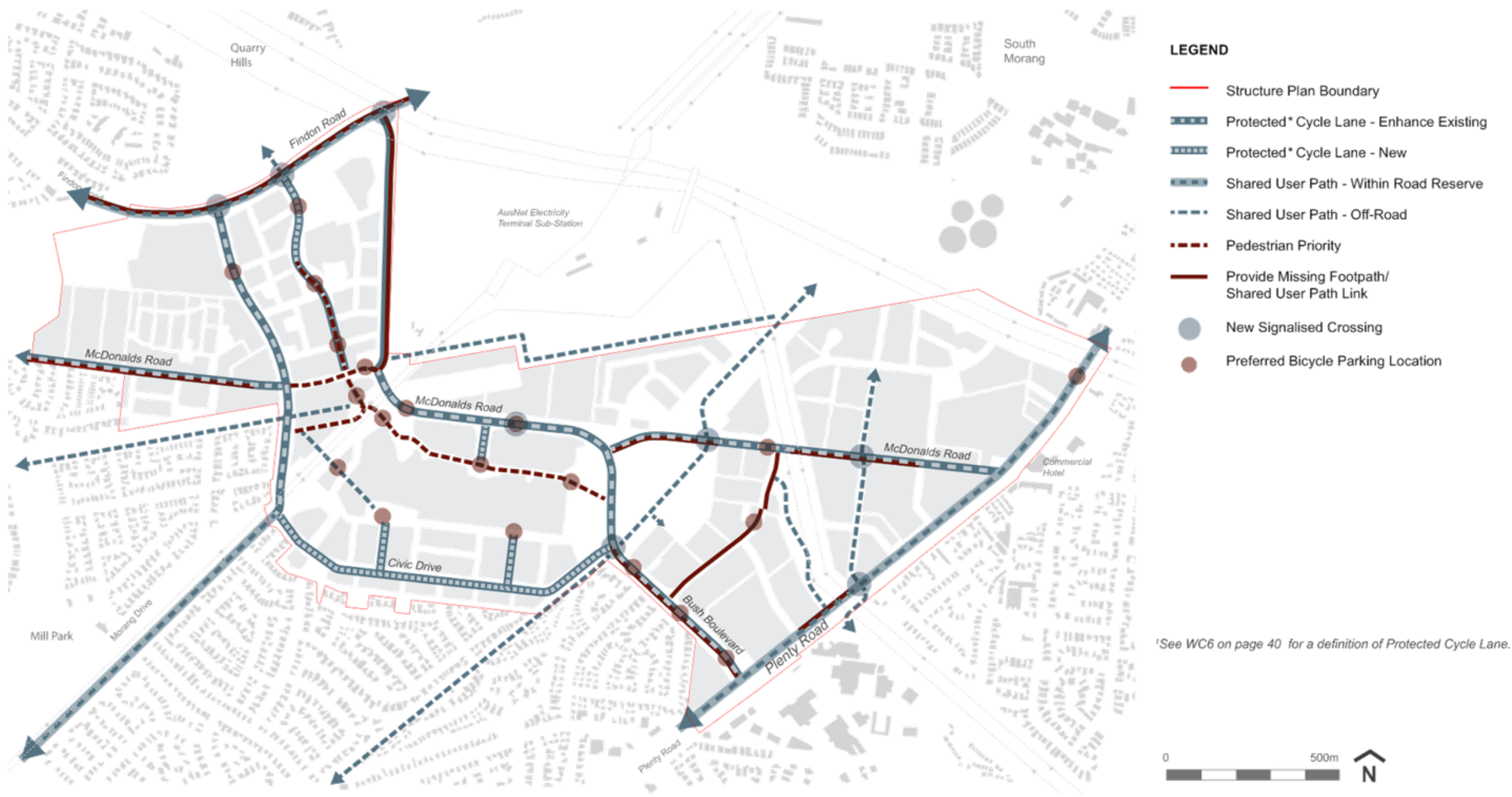


Figure 16 : Pedestrian and Cycling Infrastructure Plan



Cycling Objectives and Strategies

WC4: Ensure the Town Centre is easily accessed by bicycle within a 2.5km Radius

- WC4.1** Focus on connecting key destinations within 2.5km of the Town Centre in order to ensure the emergence of comprehensive network.
- WC4.2** Work with responsible agencies to facilitate the development of shared user paths on pipetrack and electrical easements to improve connectivity.
- WC4.3** Ensure that Findon Road includes provision for traffic separated cycle infrastructure along its length including the crossing of the Yan Yean pipetrack.

WC5: Establish new Leisure and Commuter Cycling Routes through the Town Centre and enhance existing routes.

- WC5.1** Focus on developing the Principal Bike Network (Figure 16) as priority linkages in the wider bicycle network.
- WC5.2** Ensure that the cycling network provides both on-street and off-street opportunities to access the Town Centre in accordance with Councils Bicycle Strategy.
- WC5.3** Adapt the existing road network in order to accommodate dedicated cycle routes to connect key destinations.
- WC5.4** Provide dedicated infrastructure to facilitate safe and comfortable cycle movement including kerb and parking separated cycleways where possible.

WC6: Encourage cycling through the provision of a safe, legible and connected network and dedicated facilities

- WC6.1** Ensure that sufficient cycle parking is provided in suitable locations adjacent to key destinations.
- WC6.2** Encourage cycling and multiple trips through improved wayfinding and cycling network designed for all ages and abilities.
- WC6.3** Encourage the establishment of cycling related businesses such as cafes, shops and mechanics within close proximity of well-used routes.
- WC6.4** Where on-street cycle lanes are provided on Connector and Arterial Roads, they should be protected from passing traffic by the following preferred means:
  1. Providing 'Copenhagen Style' cycle lanes to the opposing side of parallel parking bays from passing traffic. A horizontal buffer or raised kerb of at least 0.5m must be provided in order to avoid issues associated with 'car-dooring' (Figure 17).
  2. Introducing a vertical barrier, such as a raised kerb or bollards between cycle lanes and passing traffic. The barrier must be near-continuous to ensure the cyclist maintains a sense of safety and comfort.
  3. Providing a horizontal buffer between cycling lanes and passing traffic of at least 0.5m.
- WC6.5** Ensure sufficient bicycle parking facilities are provided as part of developments for residents, visitors and employees.

Cycling Actions

Action
<b>WCA4:</b> Produce initial design and development briefs for Shared User Paths to be developed within electrical and pipetrack easements in order to establish in-principle agreements with landowners and key referral agencies.
<b>WCA5:</b> Produce a wayfinding and integrated cycling infrastructure strategy for the connection of Quarry Hills to Plenty Gorge Regional Parks via the Town Centre.
<b>WCA6:</b> Publish a cycling network plan as a guide to assist cyclists in locating routes into the Town Centre.

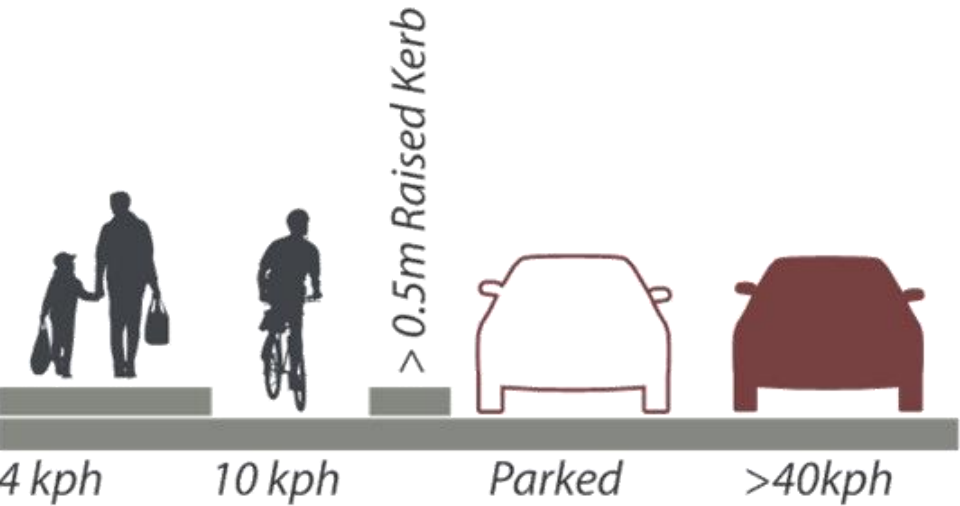


Figure 17 : 'Copenhagen Style' Protected Cycle Lanes.

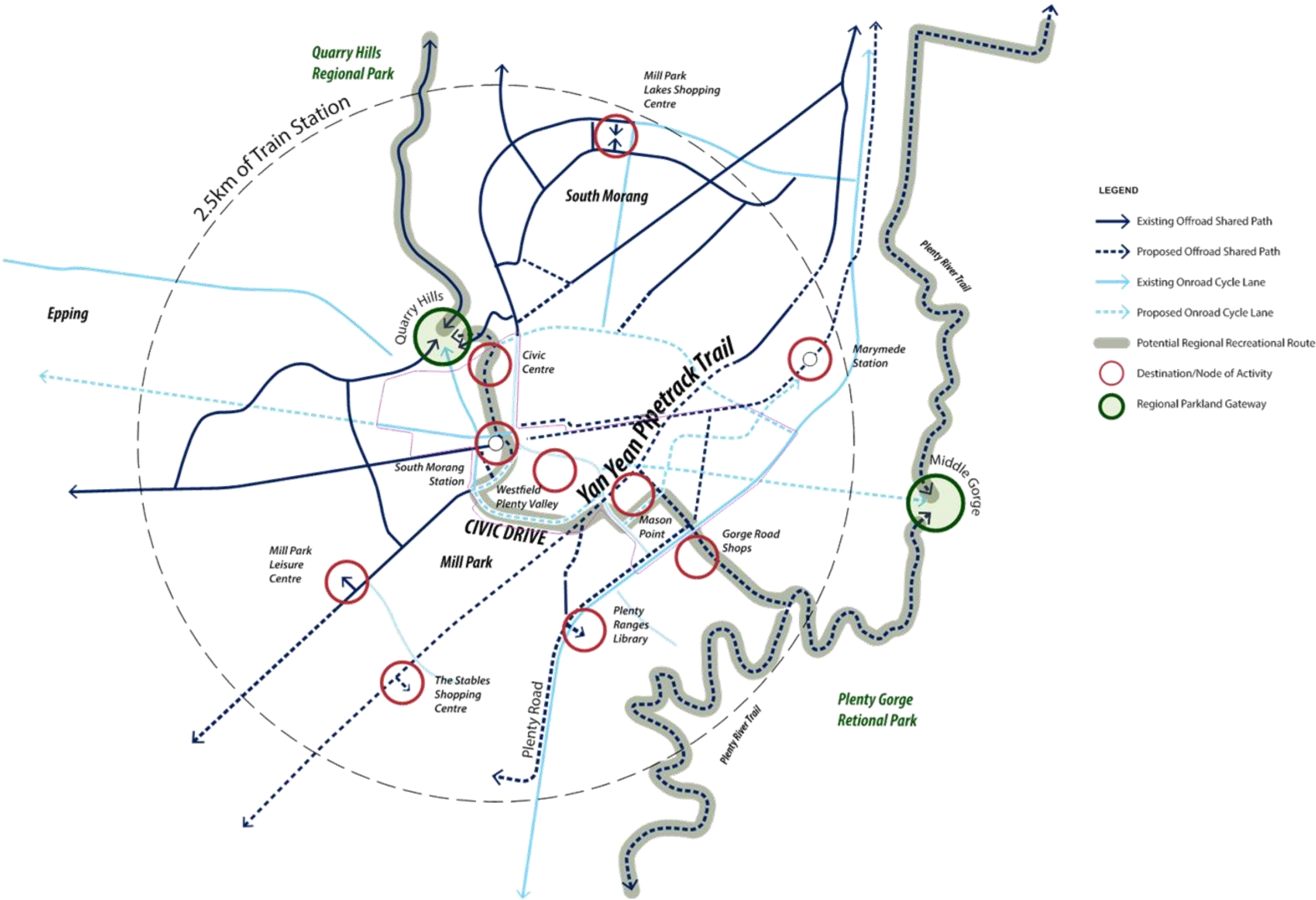


Figure 18 : Wider Cycling Infrastructure



## Public Transport Principle and Vision

### 5 Deliver an Integrated and Accessible Public Transport System

Plenty Valley Town Centre will eventually become one of the most accessible locations by Public Transport within the municipality. The extension of the rail line to Mernda Town Centre will make it possible for communities living near three additional stations to access the Town Centre directly by rail.

A tram will also eventually provide a direct and regular connection to University Hill/RMIT University. The Structure Plan identifies the proposed route for the tram extension and also an alternate route which could be considered as part of a feasibility study.

The rail and tram routes will be complimented by regular bus services to the wider Plenty Valley region. Buses will provide an important role transporting people to and from the Town Centre and transport interchanges.

Plenty Valley will form a key node within a comprehensive network of high capacity transport links connecting many of Whittlesea's Activity Centres (Figure 19). This means that residents will have better access to jobs, shops and services across each of the centres. Ultimately this network will support the municipality's self-sufficiency, retaining investment within the area and reducing the negative impact of long car-based commuting.

More locally, investment in public transport infrastructure will provide opportunities to adapt existing road cross-sections to facilitate a shift in mode share and create more active and vibrant streets.

## Public Transport Objectives and Strategies

### PT1: Capitalise on the activity and investment created by public transport stations and stops

**PT1.1** Ensure that publicly owned land and air rights in an around stations/stops is utilised for the development of multistorey, mixed-use buildings.

**PT1.2** Ensure that the pedestrian traffic associated with stations and stops is captured through the integration of high quality public spaces to encourage people to linger and undertake other activities.

**PT1.3** Ensure investments in public transport infrastructure contribute to the built form and identity of the Town Centre through good design.

### PT2: Interchange public transport modes to provide a flexible, cohesive network

**PT2.1** Ensure that a future tram extension maximises opportunities for integrated multi-modal connections, particularly adjacent to South Morang Train Station (Figure 20).

**PT2.2** Ensure that the design of public transport stations and stops considers the surrounding context and seeks to maximise movement efficiencies between modes of transport and, to a lesser extent, parking facilities.

### PT3: Ensure public transport is highly accessible to pedestrians and cyclists

**PT3.1** Ensure transport stops and pedestrian connections are well lit, clearly visible and are overlooked by buildings.

**PT3.2** Direct public transport routes and stops towards locations where they have the greatest potential catchment or are able to service the most active areas.

**PT3.3** Prioritise the completion of pedestrian and cycling links to and from train stations and tram stops.

**PT3.4** Ensure that all bus stop facilities are designed to integrate with the other transport modes, the surrounding area and where possible other activity generating land uses.

## Public Transport Actions

### Action

**PTA1:** Advocate for the extension of the tram connection from University Hill to Plenty Valley Town Centre through a clear advocacy program, supported by current evidence.

**PTA2:** Work with agencies to determine the appropriate alignment for the tram extension and investigate options for preserving tram reservation into the future.

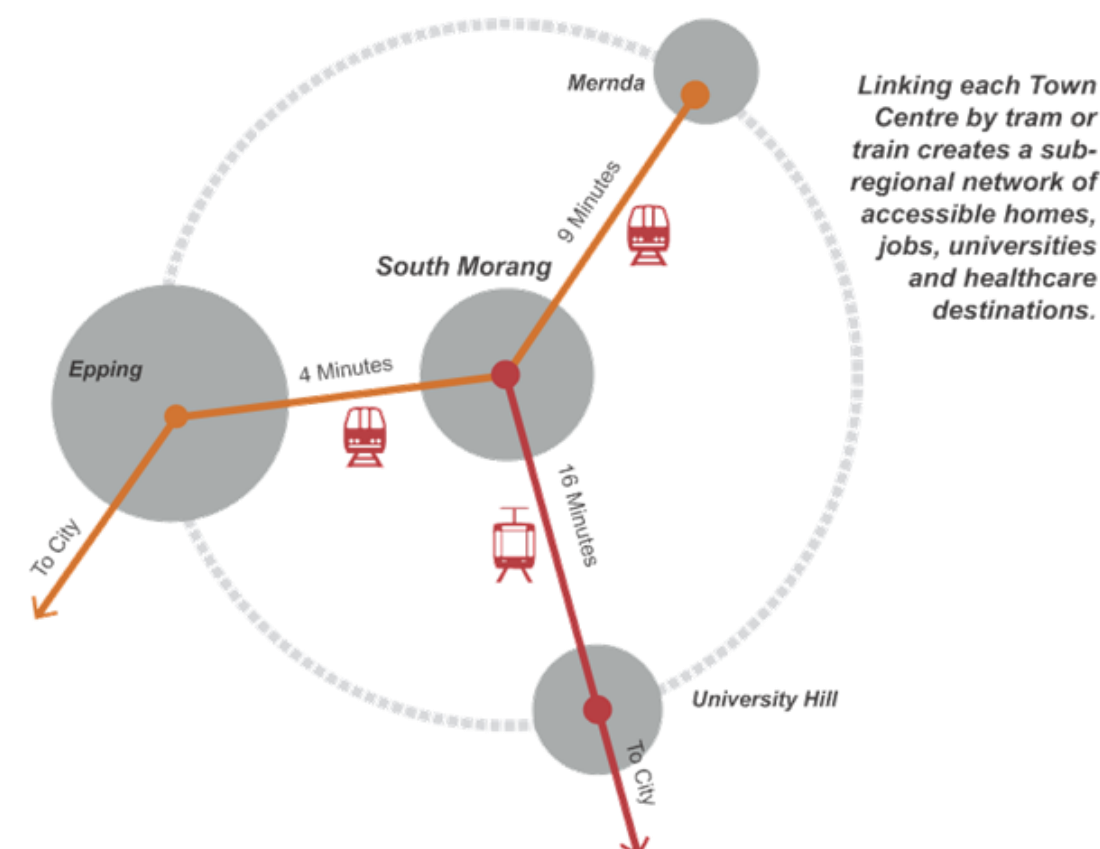


Figure 19 : The 20 Minute Network



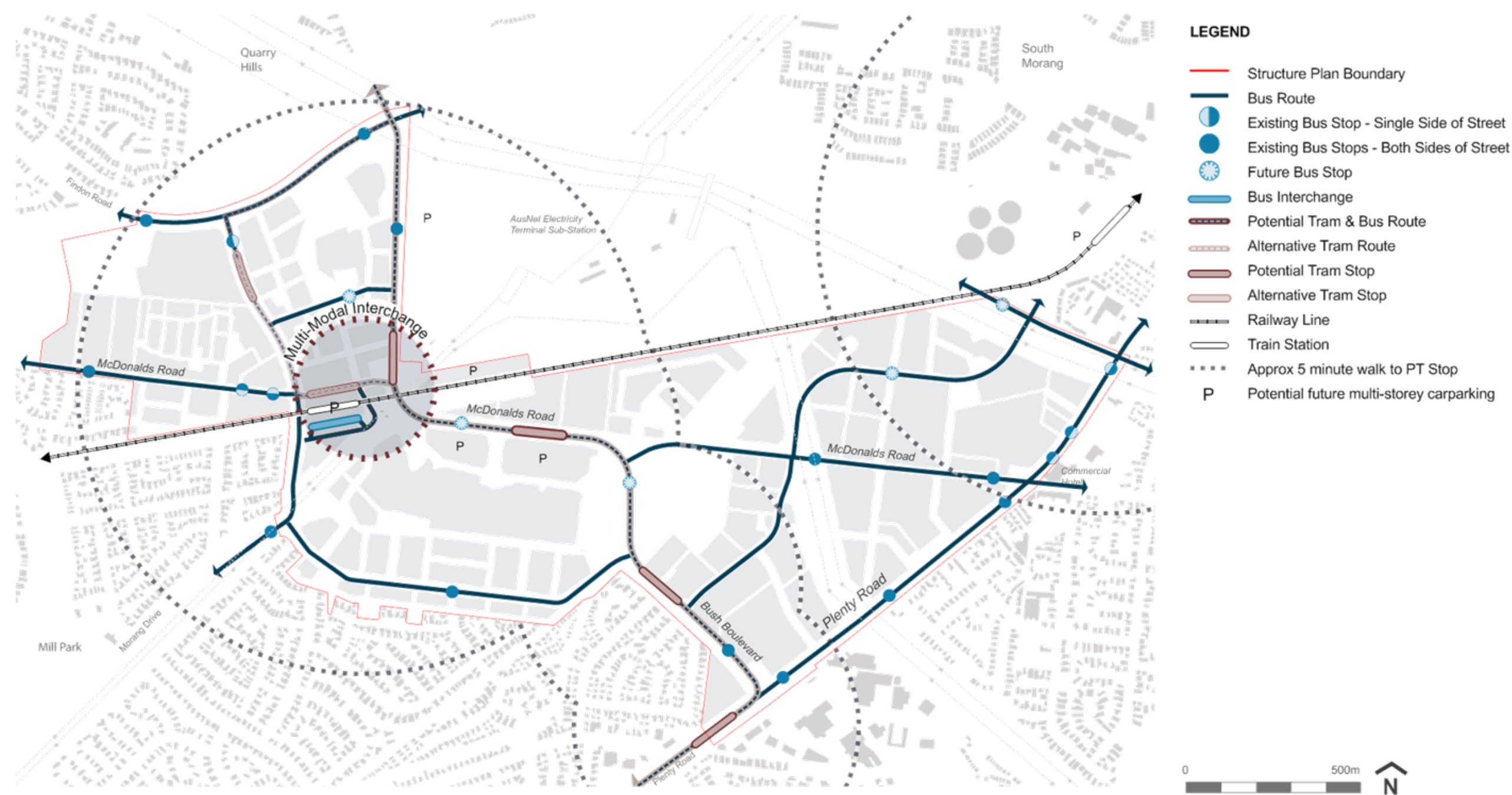


Figure 20 : Public Transport Plan

## Street Network Principle and Vision

### 6 Provide a Permeable Street Network and Encourage a Modal Shift Towards non-Vehicular Forms of Transport

Further investment in the road network will have a significant impact on the overall hierarchy of movement throughout the Town Centre, helping to alleviate its present over-reliance on a small number of arterial roads. While the extension of Findon Road will act as a bypass for the Town Centre, taking through-traffic away, the completion of local and connector streets will support a more distributed and permeable network supporting better functioning of all transport modes.

As the Town Centre develops additional intersections and signalised crossings will be added to the existing arterial network, creating a safer environment for vehicles, pedestrians and cyclists.

Improvements to walking, cycling and public transport links will ultimately also help reduce the volume of vehicular traffic accessing the Town Centre. High quality, calmed streets with continuous active frontages will provide a safe and vibrant pedestrian environment.

The amount of land dedicated to at-grade carparking will reduce significantly as land values within the Town Centre create opportunities for the intensification of development. This will encourage parking at multi-level within undercroft and 'cut and fill' arrangements utilising the space above for more productive land uses.

*Accommodating car journeys will remain a priority in the Plenty Valley Town Centre. However, walking, cycling and public transport movements will be prioritised first.*

## Street Network Objectives and Strategies

### SN1: Complete Missing Road Links

**SN1.1** Facilitate the construction of key missing local and connector roads in order to promote the emergence of a more intricate and permeable network.

**SN1.2** Avoid the use of Cul-de-sac and ensure new streets are well connected to the overall network to allow for permeability for all road-users through the centre.

**SN1.3** Promote the development of the Civic Drive extension through developer works as a local connector street with dedicated cycling infrastructure (see Appendix 5.2 for indicative cross-section).

**SN1.4** Ensure all arterial and connector streets are delivered as public roads. Encourage local roads to be developed as public roads.

### SN2: Adapt Existing Streets to Become Less Car Focussed

**SN2.1** Support the downgrading of the arterial road status of McDonalds Road to become a municipal road following the Findon Road extension.

**SN2.2** Utilise the introduction of public transport infrastructure to adapt the cross-section of roads to better facilitate pedestrian and cycle movement.

**SN2.3** Support the implementation of appropriate traffic calming measures on pedestrian priority streets such as landscape outstands, on-street parking and reductions in lane width.

**SN2.4** Reduce the amount of space dedicated to private vehicular movements within the Town Centre with a focus on improving pedestrian and cycle movement.

### SN3: Sensitively Integrate Parking into the Town Centre

**SN3.1** Locate car parking in the building footprint or to the rear or side of buildings.

**SN3.2** Ensure that where surface level parking is required, sufficient landscaping is introduced to maintain a good standard of visual amenity. At least one tree per every eight spaces should be provided.

**SN3.3** Accommodate parking within easements and along pipetracks where possible to mitigate against impacts on visual amenity.

**SN3.4** Encourage integrated on-street parking and landscaping solutions to provide accessible spaces, slow traffic and maintain a barrier between pedestrians and vehicles.

**SN3.5** Incorporate high quality landscaping into parking areas to improve visual amenity.

**SN3.6** Encourage consideration of flexible design initiatives that enable parking areas to be used for alternative uses such as community gatherings or markets or redeveloped for alternative uses in the future.

**SN3.7** Support reduction in car parking in the Town Centre for accommodation where residents have alternative transport options and other uses that do not generate additional demand for car parking.

## Street Network Transport Actions

### Action

**SNA1:** Advocate for the extension and duplication of Findon Road as a major east-west route.

**SNA2:** Work with VicRoads to improve the pedestrian permeability across McDonalds Road and investigate a potential down grade of its status particularly between Ferres Boulevard and Civic Drive.

**SNA3:** Cost the infrastructure items to be included in Capital Works or an InfraStructure Plan.

**SNA4:** Develop a detailed street network plan for the Town Centre to direct implementation of targeted traffic interventions.

**SNA5:** Prepare a parking precinct plan and investigate the use of the Parking Overlay in the Town Centre.

**SNA6:** Develop a streetscape/cross section design for Bush Boulevard and McDonalds Road to inform its adaptation for multi-modal transport and which accounts for changes in character and context along stages of its length.



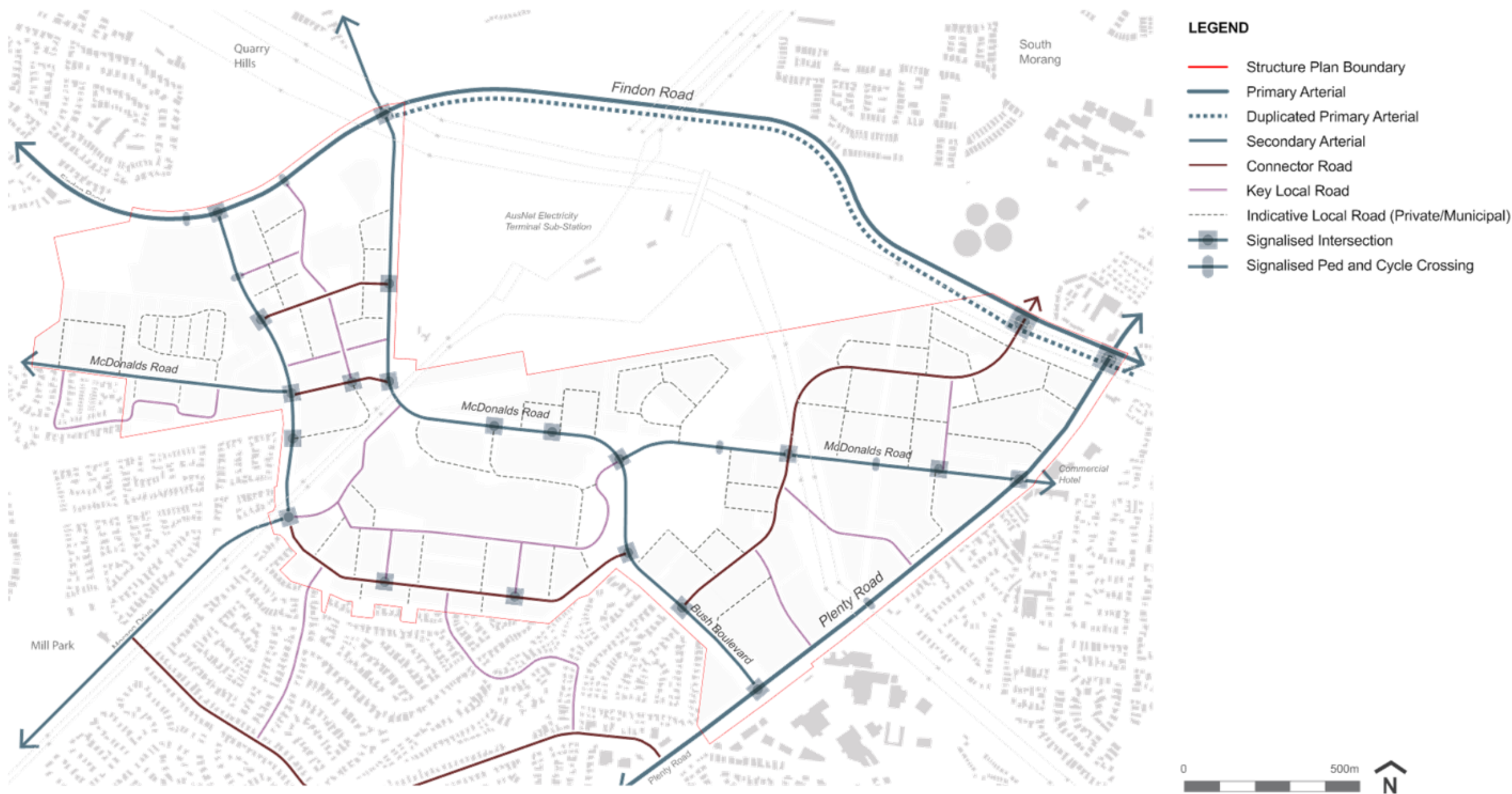


Figure 21 : Road Network and Intersection Plan



## 2.3 A Vibrant Town Centre

### Housing Principle and Vision

#### 7 Provide Additional Housing Close to Shops, Services and Public Transport

As new areas of housing are developed the population of Plenty Valley Town Centre will increase to a level where it is possible to support a broader range of local facilities and services. An established community will bring an additional level of vibrancy and activity presently lacking.

The Activity Centre location will attract further interest in the development of apartments, town houses and shop-top housing, providing a greater variety of housing options for the area.

It is proposed that housing will be of a medium scale ranging from 2 to 8 stories but generally 4 to 6 stories, which is reflective to the of the desire to increase residential densities in the Town Centre consistent with its context and setting.

It is anticipated a diverse household makeup will encompass a broad range of demographics, including families, students, first time buyers and retirees attracted to the well connected and serviced location.

### Housing Objectives and Strategies

#### HO1: Increase the Residential Population of the Town Centre

**HO1.1** Ensure that housing densities generally are sufficient to enable a sizeable local community to become established within the Town Centre.

**HO1.2** Encourage high densities within walking distance of key nodes such as the Civic Centre, South Morang Train Station, Mason Point, and Westfield Shopping Centre.

**HO1.3** Mix housing with other uses in the hatched areas set out in Figure 23 in order to support vibrant and walkable neighbourhoods.

**HO1.4** Encourage the development of permeable neighbourhoods, well integrated into the established and future movement context.

**HO1.5** Restrict the fragmentation or underdevelopment of sites which would prevent higher density, integrated development to be delivered in accordance with this plan.

**HO1.6** Support and promote consolidation of smaller lots into larger parcels that will enable higher density integrated development



*Mason Point provides evidence that market demand exists for higher density residential apartments and townhouses situated alongside other uses.*

#### HO2: Provide a Variety of Housing

**HO2.1** Support the delivery of a diversity of housing types to accommodate a range of households with varied needs.

**HO2.2** Promote a shift in the provision of dwelling types towards higher density townhouses and apartments.

**HO2.3** Utilise public land to deliver social and affordable housing to be delivered as part of mixed-tenure residential developments.

**HO2.4** Encourage the collocation of social and affordable housing alongside standard market orientated development and in highly accessible locations.

**HO2.5** Take a partnership approach to the delivery of social and affordable housing to ensure the involvement of specialist providers and government agencies.

Density Type	Density
High	More than 80 dwellings per net residential hectare.
Medium	Generally, between 30 - 80 dwellings per net residential hectare.
Low / Townhouse	More than 20 dwellings and generally less than 30 dwelling per net residential hectare .
Mixed	In mixed use developments the inclusion of other uses may result in variations to the above.

*Figure 22 : Densities for Residential Development*

### Housing Actions

#### Action

**HOA1:** Investigate and identify key opportunities for the provision of social and affordable housing as part of development.

**HOA2:** Identify and introduce mechanisms and incentives to attract the private sector to provide a minimum 5% social housing and 10% affordable housing target as part of development within the Town Centre.

**HOA3:** Provide and circulate educational resources about Federal and State Government initiatives to developers regarding social and affordable housing

**HOA4:** Ensure that integrated social and affordable housing is provided within the Civic Precinct as part of the masterplan finalisation.

**HOA5:** Advocate to the Department of Health and Human Services (DHHS) and other relevant State Government Departments to deliver new social and affordable housing projects in the Town Centre.

**HOA6:** Implement changes to the Planning Scheme to facilitate housing in accordance with the desired built form, in precincts in the Town Centre where it is encouraged.



Figure 23 : Residential Development Density



## Employment Principle and Vision

### 8 Support Local Trade and Employment and Economic Diversity

As Westfield further develops, the Town Centre's economic activity will still retain a key retail focus at its core. Nevertheless, the amount and diversity of other commercial areas will gradually increase to help balance the local economy and provide further opportunities for non-retail work. The employment precinct in particular will provide a greater diversity of commercial office spaces to support the formation of local business, while new mixed-use development blocks across the remainder of the Town Centre will incorporate a range of services.

## Employment Objectives and Strategies

### EMP1: Ensure Opportunities for High Quality Local Work Continue to Expand

**EMP1.1** Support the development of small-scale office spaces as part of mixed use sites in order to accommodate private professional services.

**EMP1.2** Encourage the development of work-from-home units in areas highly accessible by public and activate transport.

**EMP1.3** Ensure that spaces are available for a range of mid and large-scale office sizes and scales.

**EMP1.4** Promote the development of office development near to high quality public transport links.

### EMP2: Help The Local Economy To Diversify And Support A Greater Range Of Jobs.

**EMP2.1** Encourage the collocation of non-retail employment generating uses amongst predominantly retail areas.

**EMP2.2** Encourage higher order health, education and employment uses to establish in the Town Centre.

**EMP2.3** Promote the location of difficult to integrate large-scale retail uses away from high-value central locations.

**EMP2.4** Encourage an increased range of commercial office uses and spaces for small-scale manufacturing.

**EMP2.5** Provide for a range of ancillary and support services to complement civic and sub-regional uses.

**EMP2.6** Encourage longer trading hours for retail and entertainment premises in the Town Centre to improve the safety and vibrancy of the Town Centre during evening hours.

### EMP3: Accommodate Markets and Smaller Retail Uses

**EMP3.1** Ensure that space is available for accommodating itinerant markets such as direct-to-buyer farmers markets in order to complement the permanent retail offer.

**EMP3.2** Allow for the delivery of small-scale retail outside of the Central Shopping Precinct to support local needs.

**EMP3.3** Enable the introduction of community services, entertainment and dining options within the Central Shopping District.

**EMP3.4** Encourage space for temporary pop-up foodtrucks and coffee vans, particularly in underserved locations.

## Employment Actions

### Action

**EMPA1:** Work with established stakeholders to explore and support complementary employment generating uses to dominant market sectors, such as retail.

**EMPA2:** Monitor trends in business land use and employment generation throughout the Centre using annual surveys such as CLUE (Census of Land Use and Employment).

**EMPA3:** Develop clear and consistent planning controls for greater developer/investor certainty to more effectively support and manage economic growth

**EMPA4:** Develop a prospectus with a recognisable brand identity in order to highlight the benefits of investment in the Town Centre.

**EMPA5:** Investigate the establishment of a traders associations or advisory group for retailers and traders in the Town Centre.

**EMPA6:** Implement the 'Growing our Economy' strategic to provide the infrastructure and policy support required to foster increased economic investment.

**EMPA7:** Investigate the potential for a dedicated space for markets.



Civic and Community Services  
Principle and Vision

9 Support Plenty Valley’s  
Emergence as a Regional  
Centre for Civic and  
Community Services

The area surrounding the Civic Centre will be the focus on clustering civic and regional community services. The co-location of services will limit the need to make multi-destination trips by combining other activities such as shopping or onward-commuting in the precinct and provide synergistic opportunities. The emergence of the Town Centre as a multi-modal centre will provide greater equity of access to the centre and make ideal for regional/ local community services and facilities.

The focus on Plenty Valley Town Centre as a Civic hub for the municipality will also help to build a distinctive identity and distinguish it from other Activity Centres. Opportunities for local employment will increase as services are established.

Civic and Community Services Objectives and Strategies

CIV1: Promote the Centre as a Location for  
Sub-Regional and Municipal Services

- CIV1.1 Encourage sub-regional civic infrastructure such as a Justice Precinct, to locate in the Town Centre.
- CIV1.2 Pro-actively support and encourage the location of private or public healthcare institutions and ancillary services within the Town Centre.
- CIV1.3 Promote the Town Centre as a location for higher and further education such as TAFE or Melbourne Polytechnic.

CIV2: Cluster Civic and Community  
Services

- CIV2.1 Further consolidate Civic and Community Services within the Town Centre.
- CIV2.2 Enable visitors to efficiently utilise community-facing services by locating them within close proximity to one another.
- CIV2.3 Provide car parking efficiencies through the use of shared, consolidated car parking areas.

CIV3: Provide for Future Demand in  
Community Infrastructure

- CIV3.1 Support the provision of community services and infrastructure in accordance with community needs.
- CIV3.2 Utilise opportunities to use space within existing or new developments for the delivery of community services as they arise.
- CIV3.3 Provide a community hub facility in the Town Centre which integrates a range of facilities such as: early years facilities, community meeting spaces, neighbourhood house, consulting rooms and office space.
- CIV3.4 Support and work with Plenty Valley Community Health to ensure the community has access to a range of important health services.

Civic and Community Services Actions

Action
CIVA1: Finalise the Masterplan for the Civic Precinct.
CIVA2: Advocate for a Justice and Integrated Human Services Precinct within the Town Centre.
CIVA3: Advocate to Department of Health and Human Services (DHHS) for provision in the Town Centre of human services not currently accessible by residents within its catchment.
CIVA4: Develop a formal mechanism for the innovative use of vacant retail units in partnership with landowners.
CIVA5: Investigate the potential for remote study partnerships with universities or TAFE providers within the Town Centre.
CIVA6: Investigate the potential for partnerships to deliver health services such as Maternal Child Health in the Town Centre.
CIVA7: Plan for and deliver community meeting spaces in the Town Centre.

Draft Civic Precinct  
Masterplan



## Mixed Uses Principle and Vision

# 10 Support the Integration of Mixed Uses and Activities Across Plenty Valley.

As Plenty Valley Town Centre becomes established as a place it will become the principal focus for social and economic activity for a broad catchment within the Plenty Valley Growth Corridor. The provision of higher quality streets, squares and spaces will underpin a healthier and more vibrant public life. A flexible approach will be taken towards accommodating activities within the public realm which will encourage strong positive outcomes for social interaction and passing trade.

With a greater variety and number of public and private open spaces available across the Town Centre the opportunity will emerge for Council to partner with others to deliver a Place Management strategy to ensure an ongoing programme of events and markets. This will enable activity to stretch across evenings and weekends.

## Mixed Uses Objectives and Strategies

### MIX1: Mix Housing, Community and Retail Uses

**MIX1.1** Encourage mixed-use developments with residential uses located above active groundfloor uses in order to capitalise on proximity to services and provide passive surveillance of public streets and spaces.

**MIX1.2** Encourage the integration of residential with other uses horizontally and vertically within common development parcels.

**MIX1.3** Promote the provision of residential uses alongside community uses with a particular focus on supported accommodation for people with more complex needs.

**MIX1.4** Encourage the provision of small-scale retail, cafes and restaurants in order to enhance community and civic uses.

**MIX1.5** Ensure Council design guidelines and precinct plans provide direction for the integration and mixing of residential with other uses.

**MIX1.6** Promote small-scale ancillary offices in order to accommodate private professional services.

### MIX2: Complement Community and Civic Uses

**MIX2.1** Promote small-scale ancillary offices in order to accommodate private professional services

**MIX2.2** Promote the provision residential uses alongside community uses with a particular focus on supported accommodation for people with more complex needs.

**MIX2.3** Encourage the provision of small-scale retail, cafes and restaurants in order to enhance community and civic uses.

### MIX3: Promote the Centre as a Place for Events

**MIX3.1** Provide a high degree of activation within public streets and squares by regularly programming community, civic and market events.

**MIX3.2** Continue to hold public celebrations and ceremonies in suitable locations such as adjacent to the Civic Centre.

**MIX3.3** Engage with key stakeholders and local providers to negotiate and promote the use of open spaces for temporary food or craft markets.

**MIX3.4** Dedicate areas of parking or open space for accommodating large scale temporary events such as fairgrounds and circuses.

**MIX3.5** Ensure that entertainment and leisure based uses are compatible with the amenity of residential uses in the Town Centre.

### MIX4: Programme the Public Realm

**MIX4.1** Allow space within the streets, squares and spaces for cafes and restaurants to accommodate outdoor seating.

**MIX4.2** Promote public squares as locations for informal street entertainment.

**MIX4.3** Opportunities for informal places where people can gather and interact should be explored.

**MIX4.4** Develop a diversity of community spaces to provide flexibility for different types of events to be held at different times of day and year

## Mixed Uses Precinct Actions

### Action

**MIXA1:** Review planning scheme land use controls and amend as necessary to support the range of land uses supported by the Structure Plan

**MIXA3:** Develop a placemaking strategy for key strategic locations that focuses on activation of these areas.

**MIXA4:** Secure an area of surface parking or open space to host fairgrounds, circuses and other events that will bring people to the area.





Figure 24 : Mixed Use Plan - Flexible Zoning Required to Encourage Mixing







# Precinct Strategies and Policies

## 3.1 Precinct Overview

### Introduction

A number of distinctive precincts have been identified in order to focus development and design outcomes across the Structure Plan Area.

The precinct objectives and directions set out in this part of the document are intended to directly inform a statutory development framework for the Structure Plan. The ultimate arrangement of Zones and Overlays will be based on the direction contained in this Section.

### Purpose

- Identify where growth and change will occur across the Town Centre area.
- To provide built form guidance.
- Identify key opportunity sites and provide guidance for their development.
- Identify opportunities for a range of housing types and increased residential densities, and the implications for social and physical infrastructure, commercial and community services and facilities.
- Show suitable locations for passive and active open spaces.
- Identify opportunities to increase accessibility and permeability of the area.

### The Precincts

Each of the precinct areas have been identified as having shared characteristics and land uses particular to each part of the Structure Plan. The Precincts are as follows:

- A** Civic Precinct - Includes the Civic Centre and Mill Park Secondary College.
- B** Transport Hub Precinct - Includes South Morang Train Station and Bus Terminal.
- C** Central Shopping Precinct - Includes Westfield Plenty Valley and South Morang Central.
- D** Live and Work Precinct - Includes Conservation Reserves and Mason Point.
- E** Employment Precinct - Includes the Homemaker Centre and Masters building.



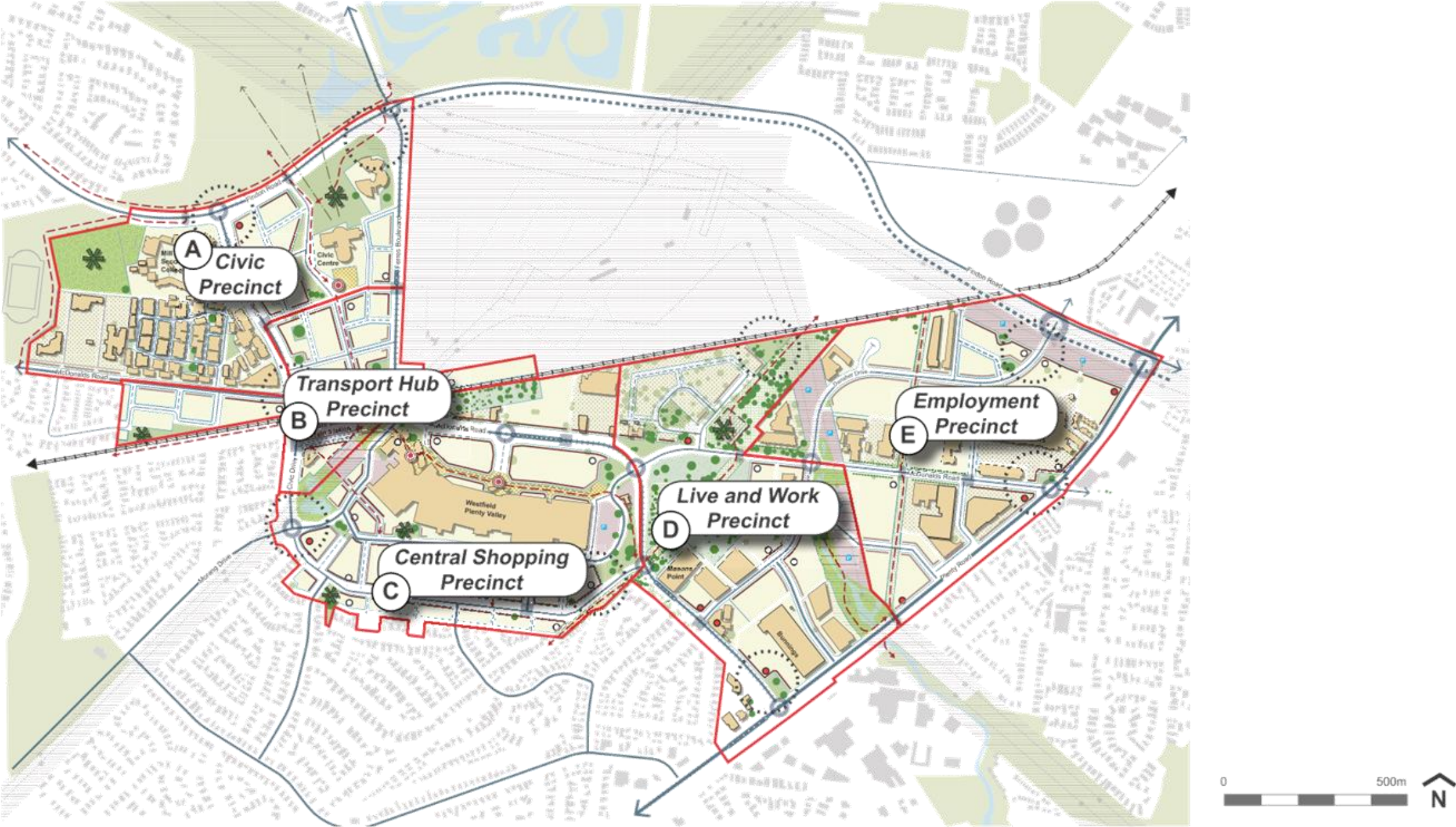


Figure 25 : Overall Precincts Plan



## 3.2 Civic Precinct

### Precinct Vision

The Civic Precinct will become the 'community heart' of the Town Centre by providing a focus for Civic and community uses, including sub-regional services such as a justice precinct.

The precinct will build upon its accessibility and proximity to South Morang Train Station and potential inter-modal interchange. By focussing development into higher than average densities, a mixed use community, office and residential precinct will be created. As one of the largest remaining development sites in the Town Centre, this area also has the greatest potential to deliver a residential development that will help to activate the Centre.

The Civic Precinct will have the opportunity to provide expansive public open space, along with green, pedestrian friendly streets that provide easy access to key services.

### Precinct Objectives

**CP01.1** Develop the Council owned land surrounding the Civic Centre by:

- » Co-locating municipal and subregional community and civic facilities.
- » Delivering high quality public streets and squares
- » Facilitate Social and Affordable Housing development opportunities
- » Integrating compatible Commercial Uses.

**CP01.2** Encourage development of higher density residential uses throughout the precinct. Ensure that residential uses are mixed with other uses vertically and horizontally throughout the district.

**CP01.3** Ensure that existing recreational reserves and sporting facilities are accessible and available for the use of the community.

**CP01.4** Protect and enhance visual and physical connections between existing council facilities at the Civic Centre and the Quarry Hills Regional Park.

**CP01.5** Further embellish existing public open spaces such as the Civic lawn and recreational grounds.

**CP01.6** Strengthen direct pedestrian and cycling connections between the transport hub and Civic centre.

### Precinct Directions

**CPD1.1** Encourage development of community and civic uses at the ground floor of multi-storey buildings along the central pedestrian priority green route.

**CPD1.2** Enhance the Civic Centre and PRACC public open space areas by providing facilities that encourage markets, passive recreation and general use (e.g. water fountains, shelter).

**CPD1.3** In partnership with housing providers/associations, developers and other stakeholders, facilitate social and affordable housing mixed use developments on Council owned land

**CPD1.4** Investigate further complementary commercial and employment forming uses within the Civic Precinct.



**CPD1.5** Provide a new pedestrian link connecting McDonalds Road to Findon Road via Meadowglen Reserve and Mill Park Secondary.

**CPD1.6** Advocate for the attraction of sub-regional uses including the establishment of a Justice Precinct.

**CPD1.7** Provide for a highly permeable, comprehensive street network that allows for direct and legible pedestrian movements.

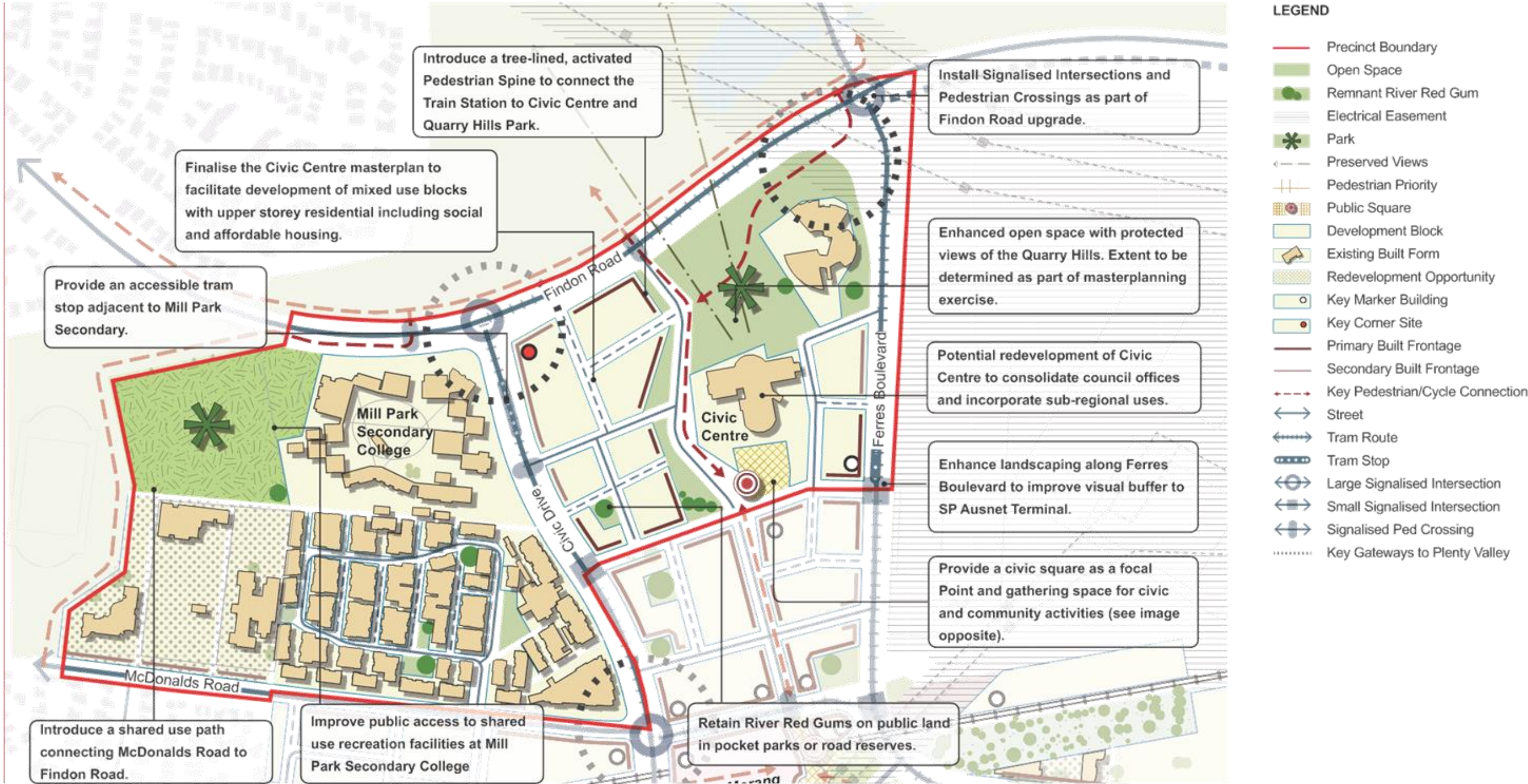
**CPD1.8** Ensure buildings provide a high level of activation and passive surveillance to key streets and public spaces to increase street level activity and perceptions of safety.

**CPD1.9** Advocate for the route 86 tram extension to service the precinct.



*A new Civic Square, delivered as part of a future Civic Precinct Masterplan, could provide a focal public space for the local community.*







## 3.3 Transport Hub Precinct

### Precinct Vision

This precinct will be the movement hub for the Town Centre. The Transport hub Precinct will enhance the relationship between existing movement networks. The precinct also proposes a future tram route to create an integrated and efficient transport interchange.

The consolidation of transport infrastructure in this precinct will provide the opportunity to enhance and expand the existing public realm, changing the nature of existing roads to better facilitate active transport options and gathering.

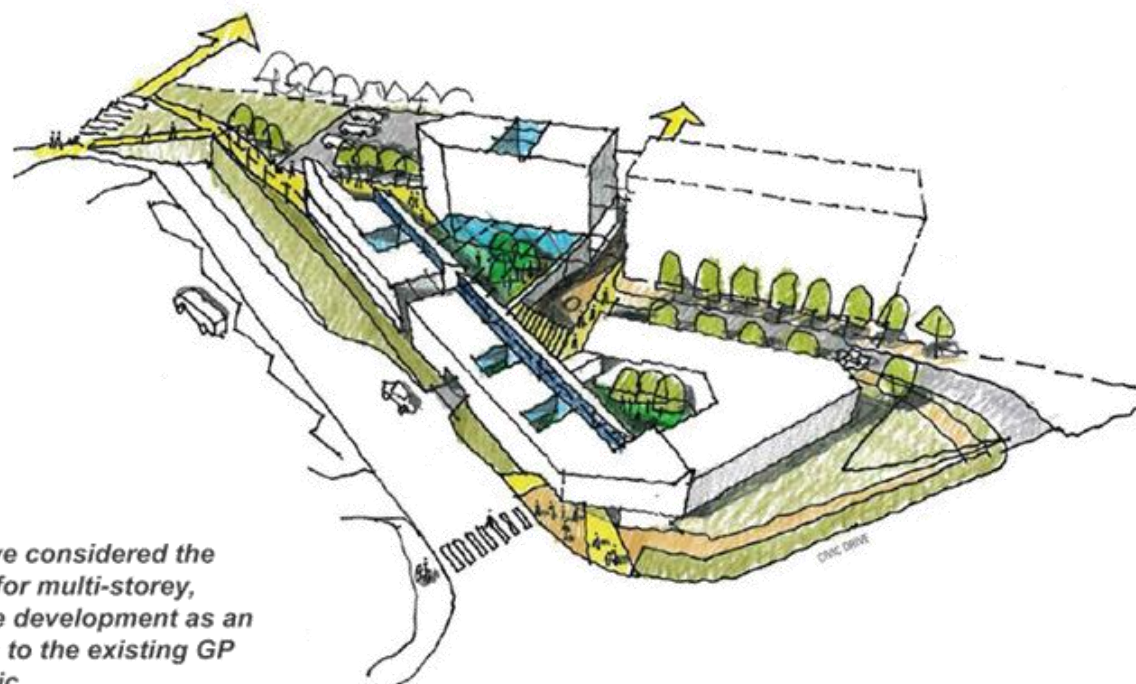
High density mixed use development will be encouraged within this precinct to capitalise on its excellent connections and services.

### Precinct Objectives

- TO1.1** Advocate for a complete transport interchange which incorporates bus, train and tram modes of travel.
- TO1.2** Provide a pedestrian oriented urban realm through the implementation of public spaces, landscaping, legible pathway connections and appropriate traffic management.
- TO1.3** Ensure that any investment in public infrastructure contributes positively to pedestrian and cycle connections through the precinct.
- TO1.4** Protect and develop key pedestrian and cycle routes in order to ensure the electrical easement and railway line are able to be traversed in multiple locations.
- TO1.5** Cluster high density development in proximity to the transport interchange.

### Precinct Directions

- TD1.1** Implement initiatives to facilitate the staged reduction of traffic flows on McDonalds Road between Civic Drive and Ferres Boulevard to create a priority pedestrian connection between the Transport Interchange and Civic Precinct. (as per the Eastland example below)
- TD1.2** Ensure that parking needs are considered strategically across different uses to ensure the most efficient use of space.
- TD1.3** Develop a car parking strategy in order to establish varying needs over time for the train station and community uses such as Plenty Valley Community Health. Ensure that as far as possible parking is provided in multi-deck facilities with active ground floor uses.
- TD1.4** Encourage high quality, dense residential development above ground floor level commercial, community and civic uses.
- TD1.5** Encourage high intensity mixed-use development of the remaining pad sites.
- TD1.6** Ensure the visual impact of the transmission pylons is effectively mitigated through the placement of buildings and landscaping.
- TD1.7** Enhance the visual amenity, facilities (benches, lighting etc.) and access to the existing wetlands to create a high quality public open space.
- TD1.8** Retain and enhance the presence of the significant River Red Gum on the north-west corner of the McDonalds Road and Ferres Boulevard intersection.
- TD1.9** Support the aims and outcomes of the Plenty Valley Community Health (PVCH) masterplan in order to assist in the delivery of a mixed use, multi-storey community, retail and healthcare hub. Work with PVCH to advocate for social and/or affordable housing opportunities as part of the masterplan process.
- TD1.10** Encourage development of retail and commercial uses at the ground floor of multi-storey development along the pedestrian spine between South Morang Train Station and the Civic Precinct.
- TD1.11** Support the future development of State Government land to include a minimum of 10% affordable and 5% social housing.



PVCH have considered the potential for multi-storey, mixed use development as an extension to the existing GP Superclinic.



The Maroondah Highway has been reduced from a 22m of carriageway to 11m in order to provide a direct pedestrian link from Ringwood Station to the town square at Eastland.



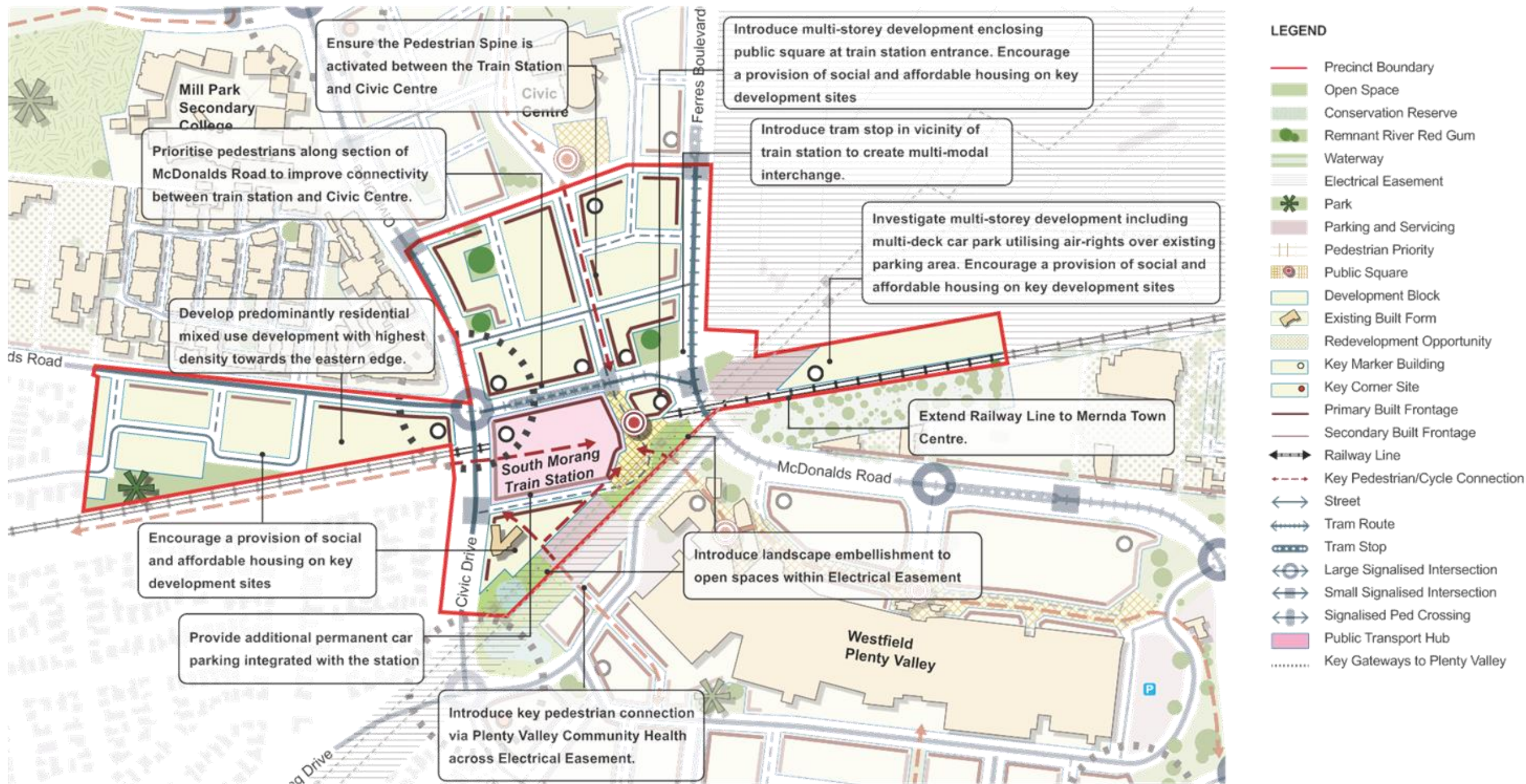


Figure 27 : Transport Hub Illustrative Precinct Plan





## 3.4 Central Shopping Precinct

### Precinct Vision

Future development of the existing shopping centre will begin to diversify away from the present internal, retail-dominant model to become a finer grain retail, leisure, commercial and residential precinct.

As Westfield Plenty Valley develops it will provide range of specialty retail, food and drink, entertainment and leisure uses as part of its core offer. This dense and varied mix of activities and destinations will be centred on pedestrian-orientated external streets and squares which will encourage activity to extend through the day and into the evening. The co-location here of essential community and commercial facilities will promote multi-purpose trips and reduce the number of vehicle trips within the Town Centre generally.

The extension of Civic Drive as a high amenity connector street will facilitate further east-west connections across the precinct, particularly for cyclists. The introduction of residential uses either side of this new street will provide a vibrant neighbourhood character.

### Precinct Objectives

**CSO1.1** Promote the diversification of the retail centre to provide a greater range of facilities, services and commercial opportunities.

**CSO1.2** Encourage the extension of activities and uses into the evening and late-night to improve public safety through passive surveillance.

**CSO1.3** Ensure that future development to the north of the existing shopping centre delivers an east-west street that provides a critical stage of the pedestrian spine connecting Mason Point to the Train Station and Civic Centre beyond.

**CSO1.4** Locate new public squares adjacent to the precincts most active uses, such as outdoor leisure uses or access points to the shopping centre.

**CSO1.5** Enhance existing pedestrian and cycle linkages from Westfield through to the Transport Hub precinct.

**CSO1.6** Promote the delivery of Civic Drive as a local connector road facilitating east-west cycling linkages and accessing a higher density residential and mixed use neighbourhood (see Appendix 5.2 for indicative cross section).



Westfield - Stratford, London is designed around outdoor streets which promote walkability + activity.

### Precinct Directions

**CSD1.1** Enhance parks and open space through increased landscaping, embellishment and the provision of facilities such as play equipment

**CSD1.2** Ensure that the design of public streets and squares encourage their use as meeting and event places.

**CSD1.3** Develop a town square, piazza or other public squares that will support a variety of activities as well as food and drink, entertainment and leisure uses.

**CSD1.4** Encourage more restaurants, cafes and entertainment options in highly pedestrianised areas such as public squares and along the pedestrian spine.

**CSD1.5** Facilitate a pedestrian/cycle green link along Civic Drive between the Live & Work precinct and the Transport Hub.

**CSD1.6** Ensure tram alignments and potential stops integrate appropriately with existing retail uses.

**CSD1.7** Develop a fine grain, open retail street along the northern edge of the Westfield Shopping Centre with a pedestrian oriented design.



New external public spaces could be activated by leisure and restaurant uses.

**CSD1.8** Prepare and implement a revised design of McDonalds Road to better support a safe and attractive pedestrian and cycle environment; potential design treatments may include dedicated bike lanes, on-street car parking and minimal building setbacks.

**CSD1.9** Support increased scale mixed-use development that provides an appropriate interface to existing low-density residential areas to the south.

**CSD1.10** Gradually reduce the provision of at-grade car parking, instead promoting multi-deck or undercroft parking as part of any future development.

**CSD1.11** Ensure the built form transitions to existing residential areas and protects the solar access to private open spaces of existing residences.

**CSD1.12** Support the development of Civic Drive on the alignment indicated in this plan in accordance with the indicative cross section in Appendix 5.2. As a connector street the funding of land and works are the responsibility of the developers as indicated in Section 4.3.

**CSD1.13** Ensure that connection of Stillman Drive and Peyton Drive to Civic Drive are designed to provide local access for residents and include traffic calming measures to restrict and discourage undesired traffic movement.



Civic Drive will feature safe off-street cycle provision.





Figure 28 : Central Shopping Illustrative Precinct Plan





## 3.5 Live and Work Precinct

### Precinct Vision

The Live and Work precinct balances areas of higher density, mixed use development alongside areas of protected landscape amenity.

The precinct will continue to develop as a mixed-use commercial and office precinct with a sizeable residential component provided above ground floor and away from principal streets.

The two substantial residential-focussed mixed use areas which comprise this precinct will be connected by high-quality landscape and open space connections utilising existing conservation and pipetracks. These areas of high environmental value will continue to make a strong contribution toward the character and amenity of this precinct.

Activity will be focussed along Oleander Drive with strong pedestrian and cycling links into the Central Shopping precinct and other surrounds.

### Precinct Objectives

**LO1.1** Continue to promote the area as a high-amenity residential community with excellent connections to the wider metropolitan area.

**LO1.2** Advocate for the use of existing reserves to provide parks and open spaces that take advantage of existing native trees and waterways for the growing resident and worker user base.

**LO1.3** Ensure buildings have frontage to streets, parks, waterways and easements to provide overlooking.

**LO1.4** Enhance the amenity and use of the waterway and conservation reserves by providing additional landscaping, benches, facilities and shared paths.

**LO1.5** Enhance features of the Yan Yean Water Supply System and provide interpretation to raise the profile of its State significant heritage values.

**LO1.6** Utilise existing infrastructure easements to facilitate shared path connections across the Town Centre and wider area.

**LO1.7** Provide a permeable movement network appropriate to the residential/mixed use focus of the precinct with a priority on pedestrian and public transport integration.

**LO1.8** Encourage a mix of compatible land uses including housing, health, office and some retail.

### Precinct Directions

**LD1.1** Establish a heritage interpretation space at the Pipehead Reservoir.

**LD1.2** Advocate for a tram stop on Bush Boulevard to service the growing residential and working population in this precinct.

**LD1.3** Investigate the drainage requirements of the Heaths Court Drain to inform potential design treatments and embellishment.

**LD1.4** Revise the cross section of Oleander Drive to better support a pedestrian based retail thoroughfare.

**LD1.5** Complete missing sections in the pedestrian/cycle network including footpaths, shared paths and pedestrian crossings.



Example of Integrated Street Design on Bush Boulevard



A public park featuring a heritage trail and shared bicycle paths could be provided as part of the Yan Yean pipetrack.





Figure 29 : Live and Work Illustrative Precinct Plan





### 3.6 Employment Precinct

Precinct Vision

This eastern part of the Town Centre is further from the existing South Morang train station than the other precincts and is less suitable for high density residential housing or accessible fine grain retail. Nevertheless, the provision of an additional railway station within a short walking distance will open up the potential for higher density employment generating uses along the precinct's northern edge.

Its excellent road connections have shaped the established character of the employment precinct as a location for commercial business and bulky goods retail. The extension of Danaher Drive and Findon Road will serve to further consolidate this pattern of land use as the precinct further develops. The employment precinct has the greatest potential for providing accommodation for a greater variety of employment opportunities - a range of business and larger format retail premises sizes and floorplates will enable start-up enterprises to locate within the Town Centre.

The identity of this precinct will be based on the landscape quality of McDonalds Road which acts as a gateway boulevard into the heart of the Town Centre from Plenty Road. As the centre develops and McDonalds Road becomes less significant as an east-west arterial road then opportunities are likely to arise for the redevelopment of existing larger format uses into higher density commercial/ mixed use developments.

Precinct Objectives

- EO1.1** Promote the intensification of commercial land uses to encourage an increase in the quality and quantity of jobs across the precinct.
- EO1.2** Support a range of employment-generating land uses such as office, small scale manufacturing, servicing and ancillary retail.
- EO1.3** Utilise buildings and landscaping to shield transmission pylons and minimise their visual prominence.
- EO1.4** Provide a high quality streetscape along McDonalds Road utilising extensive natural landscaping to soften the built form and compensate for excessive building setbacks through the introduction of street trees.
- EO1.5** Ensure development provides activation of public open spaces and the future shared path along the water pipetrack easement.
- EO1.6** Provide a direct pedestrian and cycle connection across Findon Road to the north to access the future railway station at Marymede, including a potential signalised intersection.
- EO1.7** Encourage development to respond to the proximity and increased potential of the future railway station.
- EO1.8** Promote the development of finer grain road network to encourage permeability through the precinct.

Precinct Directions

- ED1.1** Encourage development of offices and limited other local services or retail within 800m of the future railway station.
- ED1.2** Develop a shared path connection along the Melbourne Water pipetrack easement, nominated by VicRoads as a priority bicycle route.



Figure 30 : Employment Illustrative Precinct Plan







# Implementation



## 4.1 Implementation Overview

### Introduction

The Plenty Valley Town Centre Structure Plan will be implemented via a number of initiatives outlined in this section. The implementation process consists of the following:

- Planning Scheme Amendment
- Specific Actions
- Infrastructure
- Infrastructure Contributions Plan
- Monitoring and Review

Delivery of the implementation actions will be the responsibility primarily of Council with support from the State Government, other agencies and the private sector.

## 4.2 Specific Actions

### Landscape Actions

Action	Timing
<b>LAA1</b> : Develop a strategic landscaping planting plan to determine an approach to tree planting for streets and spaces across the Centre.	Short
<b>LAA2</b> : Amend the Conservation Management Plan for the Central Conservation Reserve in order to incorporate a pedestrian connection to Mason Point.	
<b>LAA3</b> : Apply for Living Rivers funding to support the further embellishment of Hendersons Creek and other creeks and wetlands in the Town Centre..	
<b>LAA4</b> : Undertake an assessment of Heath Courts Drain to establish its biodiversity values and the potential for incorporating Water Sensitive Urban Design into a publicly accessible open space.	Medium
<b>LAA5</b> : Develop guidelines for the incorporation of River Red Gums into useable public spaces as part of a review of River Red Gum policy.	

### Public Space Actions

Action	Timing
<b>PSA1</b> : Work with key agencies and developers to introduce active recreation and playspaces into the Yan Yean pipetrack.	Short
<b>PSA2</b> : Develop public art strategies for the Town Centre.	
<b>PSA3</b> : Negotiate a revised shared user agreement with the Department of Education to remove fencing and improve community access to the Mill Park Sports Pitches.	Medium
<b>PSA4</b> : Prepare a streetscape design framework for arterial, sub-arterial and collector streets which emphasise the public realm function of the street.	
<b>PSA5</b> : Undertake a competitive design process for the provision of a civic space to be delivered as part of the Civic Precinct expansion.	



### Design Actions

Action	Timing
<b>DESA1</b> : Implement new built form controls in a schedule to the Planning Scheme in accordance with the design principles and guidelines identified in the Structure Plan.	<b>Short</b>
<b>DESA2</b> : Introduce a Planning Application requirement for Sustainable Design Assessment (SDA) or Sustainability Management Plans (SMP) to accompany significant planning applications.	
<b>DESA3</b> : Investigate the use of annual design awards for excellence for developments within the Town Centre.	<b>Medium</b>

### Walking and Cycling Actions

Action	Timing
<b>WCA1</b> : Utilise a variety of funding sources to complete key missing pedestrian infrastructure aligned with 'Missing Links' strategy, across the Town Centre.	<b>Short</b>
<b>WCA2</b> : Install safe crossing points on major roads to link key activity generating uses and provide access from the Town Centre to the Quarry Hills and Plenty Gorge regional parklands.	<b>Medium</b>
<b>WCA3</b> : Implement street upgrades including the introduction of additional street trees, furniture, wayfinding and public art along key routes.	
<b>WCA4</b> : Produce initial design and development briefs for Shared User Paths to be developed within electrical and pipetrack easements in order to establish in-principle agreements with landowners and key referral agencies.	<b>Short</b>
<b>WCA5</b> : Produce a wayfinding and integrated cycling infrastructure strategy for the connection of Quarry Hills to Plenty Gorge Regional Parks via the Town Centre.	<b>Medium</b>
<b>WCA6</b> : Publish a cycling network plan as a guide to assist cyclists in locating routes into the Town Centre.	

### Public Transport Actions

Action	Timing
<b>PTA1</b> : Advocate for the extension of the tram connection from University Hill to Plenty Valley Town Centre through a clear advocacy program, supported by current evidence.	<b>Short</b>
<b>PTA2</b> : Work with agencies to determine the appropriate alignment for the tram extension and investigate options for preserving tram reservation into the future.	

### Street Network Actions

Action	Timing
<b>SNA1</b> : Advocate for the extension and duplication of Findon Road as a major east-west route.	<b>Short</b>
<b>SNA2</b> : Work with VicRoads to improve the pedestrian permeability across McDonalds Road and investigate a potential down grade of its status particularly between Ferres Boulevard and Civic Drive.	
<b>SNA3</b> : Cost the infrastructure items to be included in Capital Works or an InfraStructure Plan.	
<b>SNA4</b> : Develop a detailed street network plan for the Town Centre to direct implementation of targeted traffic interventions.	
<b>SNA5</b> : Prepare a parking precinct plan and investigate the use of the Parking Overlay in the Town Centre.	<b>Medium</b>
<b>SNA6</b> : Develop a streetscape/cross section design for Bush Boulevard and McDonalds Road to inform its adaptation for multi-modal transport and which accounts for changes in character and context along stages of its length.	



**IMPLEMENTATION** Henty Valley Town Centre Draft Structure Plan

**Housing Actions**

Action	Timing
<b>HOA1:</b> Identify incentives to attract and engage the private sector to provide social and affordable housing as part of new housing developments.	<b>Short</b>
<b>HOA2:</b> Identify and introduce mechanisms and incentives to attract the private sector to provide a minimum 5% social housing and 10% affordable housing target as part of development within the Town Centre.	
<b>HOA3:</b> Provide and circulate educational resources about Federal and State Government initiatives to developers regarding social and affordable housing	
<b>HOA4:</b> Ensure that integrated social and affordable housing is provided within the Civic Precinct as part of the masterplan finalisation.	
<b>HOA5:</b> Advocate to the Department of Health and Human Services (DHHS) and other relevant State Government Departments to deliver new social and affordable housing projects in the Town Centre.	
<b>HOA6:</b> Implement changes to the Planning Scheme to facilitate housing in accordance with the desired built form, in precincts in the Town Centre where it is encouraged.	

**Employment Actions**

Action	Timing
<b>EMPA1:</b> Work with established stakeholders to explore and support complementary employment generating uses to dominant market sectors, helping to diversify away from retail.	<b>Short</b>
<b>EMPA2:</b> Monitor trends in business land use and employment generation throughout the Centre using annual surveys such as CLUE (Census of Land Use and Employment).	
<b>EMPA3:</b> Develop clear and consistent planning controls for greater developer/investor certainty to more effectively support and manage economic growth	
<b>EMPA4:</b> Develop a prospectus with a recognisable brand identity in order to highlight the benefits of investment in the Town Centre.	
<b>EMPA5:</b> Investigate the establishment of a traders associations or advisory group for retailers and traders in the Town Centre.	<b>Medium</b>
<b>EMPA6:</b> Implement the 'Growing our Economy' strategic to provide the infrastructure and policy support required to foster increased economic investment.	
<b>EMPA7:</b> Investigate the potential for a dedicated space for markets	

**Civic and Community Actions**

Action	Timing
<b>CIVA1:</b> Finalise the Masterplan for the Civic Precinct.	<b>Short</b>
<b>CIVA2:</b> Advocate for a Justice and Integrated Human Services Precinct within the Town Centre.	
<b>CIVA2:</b> Advocate to Department of Health and Human Services (DHHS) for provision in the Town Centre of human services not currently accessible by residents within its catchment.	
<b>CIVA4:</b> Develop a formal mechanism for the innovative use of vacant retail units in partnership with landowners.	<b>Medium</b>
<b>CIVA5:</b> Investigate the potential for remote study partnerships with universities or TAFE providers within the Town Centre.	
<b>CIVA6:</b> Investigate the potential for partnerships to deliver health services such as Maternal Child Health in the Town Centre.	
<b>CIVA7:</b> Plan for and deliver community meeting spaces in the Town Centre	

**Mixed Use Actions**

Action	Timing
<b>MIXA1:</b> Review planning scheme land use controls and amend as necessary to support the range of land uses supported by the Structure Plan	<b>Short</b>
<b>MIXA2:</b> Develop a placemaking strategy for key strategic locations that focuses on activation of these areas.	<b>Medium</b>
<b>MIXA3:</b> Secure an area of surface parking or open space to host fairgrounds, circuses and other events that will bring people to the area.	

## 4.3 Infrastructure

### *Infrastructure*

The Plenty Valley Town Centre will require a range of infrastructure items to support its development, future residents and realise the vision outlined in this Structure Plan.

Figures 31, 33, 35, and 37 list the infrastructure projects. The location of the projects are shown in Figures 32, 34, 36, and 38.

The infrastructure projects include:

- roads,
- intersections,
- pedestrian and cycling,
- public transport,
- community facilities,
- open space and
- drainage.

The projects will be delivered via a variety of mechanisms. Tables 31, 33, 35 and 37 identify whether the works are 'developer works'.

**Developer Works** - Infrastructure and works which have a direct nexus to development and required to be fully funded by the developer as part of development of the land.

Works which are not developer works may be funded via a range of funding sources including:

**Council Funding Sources** - To be funded from Council's capital works budget or via another Council funding source such as land sale.

**State Government Funding Sources** - Infrastructure reliant on funding or grants from the State Government or agencies including VicRoads and Melbourne Water.

**Open Space Contributions** - To be funded by land or monetary contributions required in accordance with s18 of the Subdivisions Act 1988.

**Developer Contributions** - Contribution made to infrastructure either via levy in a Contributions Plan or agreement to contribute to infrastructure funding.

An Infrastructure Contribution Plan (ICP) will be prepared to provide a framework for development proponents to contribute a fair proportion towards the cost of works and services for the new development in the Town Centre. Details of the ICP are discussed in Section 4.4.



## IMPLEMENTATION

Plenty Valley Town Centre Draft Structure Plan

Plan Code	Category	Infrastructure Item	Description	Timing	Developer Works
ROADS					
RD-01	Road	Road duplication - Bush Boulevard	Construct second carriageway - Bush Boulevard	Long	No
RD-02	Road	Road construction	Construct 'Local Road from Ferres Blvd - Civic Drive	Short	Yes
RD-03	Road	Road construction - Findon Road	Findon Road Extension from The Great Eastern Way to Plenty Road	Short	Yes (part)
RD-04	Road	Road duplication - Findon Road	Construct second carriageway - Findon Road - between Civic Drive and Ferres Boulevard	Medium	No
RD-05	Road	Road duplication - Ferres Boulevard	Construct second carriageway - Ferres Boulevard - between Findon Road and McDonalds Road	Long	No
RD-06	Road	Land and road construction - Civic Drive extension	Civic Drive Extension from Morang Drive to Bush Boulevard	Medium	Yes
RD-07	Road	Road construction - Murdoch Road	Extension of Danaher Drive to Findon Road	Medium	Yes
RD-08	Culvert	Culvert construction - Heath Court Drain	Construct a road and culvert over Heath Court Drain - Oleander Drive to Plenty Road	Long	Yes
RD-09	Road and Public Realm	Streetscape improvement	Traffic calming measures and streetscape improvement to Peyton Drive and Stillman Drive	Medium	Yes
RD-10	Road and Public Realm	Streetscape improvement	Streetscape improvement works on McDonalds Road between Civic Drive and Ferres Blvd to prioritise pedestrian movements.	Medium	No
RD-11	Road	Road Upgrade - Civic Drive	Upgrade and widening of Civic Drive	Medium	No

Figure 31 : Infrastructure Table - Roads

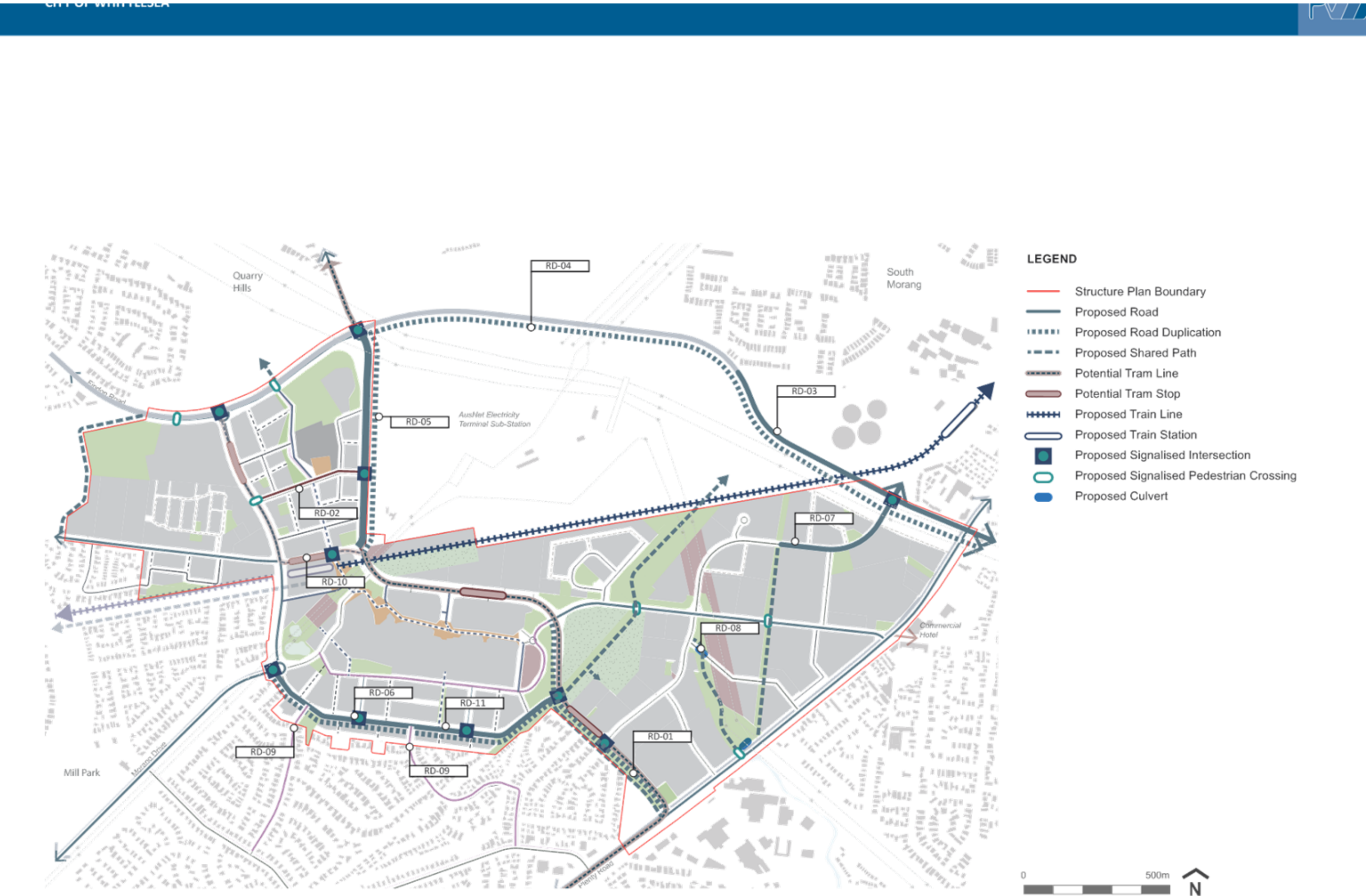


Figure 32 : InfraStructure Plan - Roads



## IMPLEMENTATION

Plenty Valley Town Centre Draft Structure Plan

Plan Code	Category	Infrastructure Item	Description	Timing	Developer Works
<b>INTERSECTIONS</b>					
IN-01	Road	Signalised intersection	Signalisation - Ferres Boulevard and Main Road (Council access road)	Medium	Yes
IN-02	Road	Signalised intersection	Signalise intersection - Ferres Boulevard and Findon Road	Short	No
IN-03	Road	Signalised intersection	Signalise intersection - Findon Road and Civic Drive	Long	No
IN-04	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing - Findon Road - east of Civic Drive	Short	No
IN-05	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing on Civic Drive between Mill Park Secondary College and Civic Precinct	Long	No
IN-06	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing at Findon Road between Mill Park Secondary College sports fields and Quarry Hills Park	Short	No
IN-07	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing to facilitate Yan Yean shared path - Conservation Reserve to proposed Heritage Park	Medium	No
IN-08	Road	Signalised intersection	Signalise intersection - Morang Drive and Civic Drive	Medium	No
IN-09	Road	Signalised pedestrian crossing	Signalise intersection - Civic Drive and Bush Boulevard. Connection to Civic Drive extension.	Long	No
IN-10	Road	Signalised intersection	Signalise intersection - Oleander Drive and Bush Boulevard - potential to incorporate tram stop	Long	No
IN-11	Road	Signalised intersection	Signalise intersection - Findon Road and Danaher Drive	Long	No
IN-12	Road	Signalised intersection	Signalise intersection - Local Road (from Civic Precinct) and McDonalds Road	Medium	Yes
IN-13	Road	Signalised intersection	Signalise intersection - Local Road (from Westfield Plenty Valley) and Civic Drive	Long	Yes
IN-14	Road	Signalised intersection	Signalise intersection - Local Road (from Westfield Plenty Valley) and Civic Drive	Long	Yes
IN-15	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing of McDonalds Road for shared path on pipetrack	Medium	No
IN-16	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing of Plenty Road for shared path on pipetrack	Medium	No

Figure 33 : Infrastructure Table - Intersections

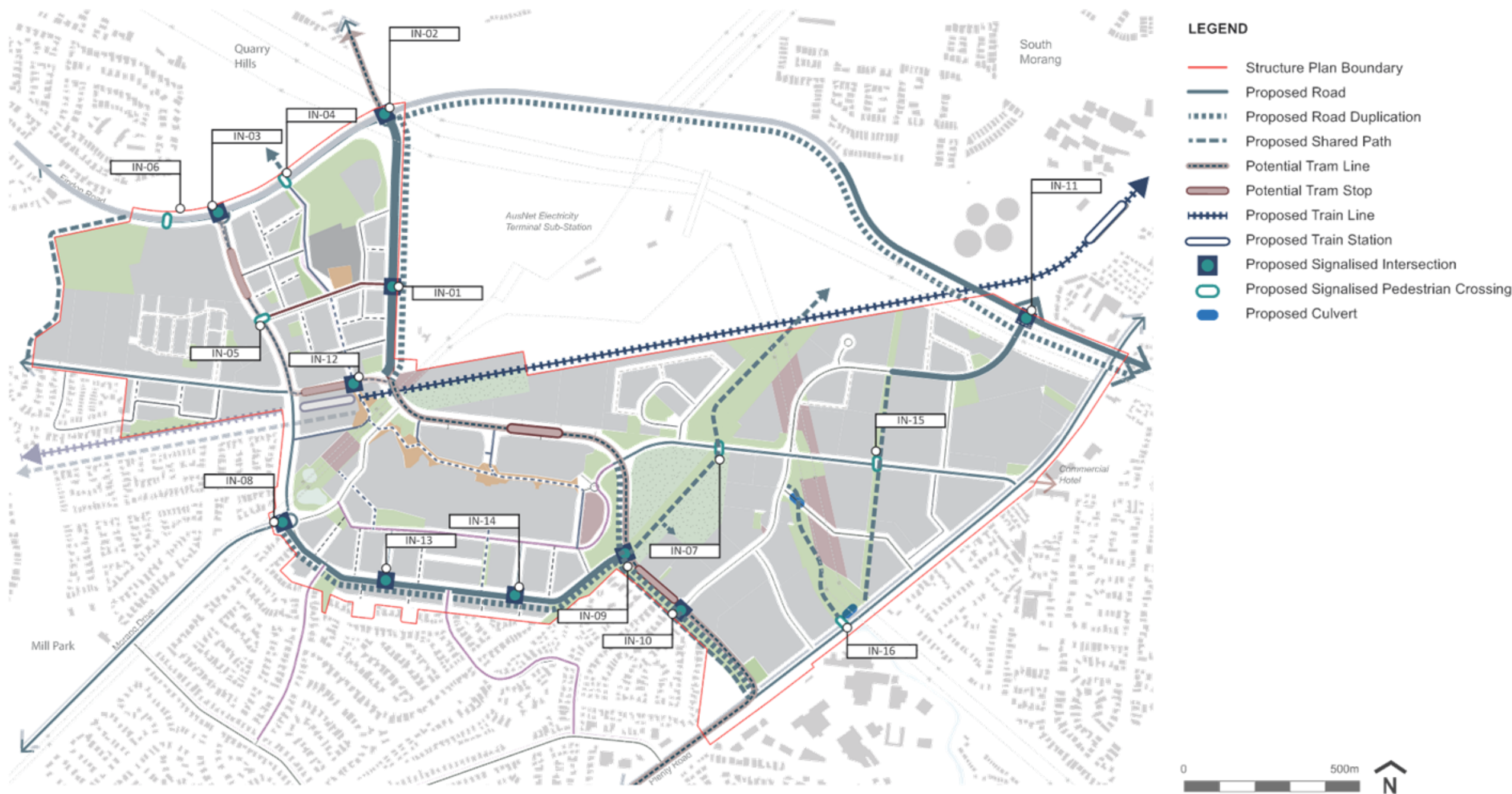


Figure 34 : InfraStructure Plan - Intersections



**IMPLEMENTATION** Plenty Valley Town Centre Draft Structure Plan

Plan Code	Category	Infrastructure Item	Description	Timing	Developer Works
<b>PEDESTRIAN AND CYCLING</b>					
PED-01	Shared Path	Shared path construction - Bush Boulevard	Construct shared path - Bush Boulevard - Plenty Road to Westfield Shopping Centre - Bush Bvd	Short	No
PED-02	Shared Path	Shared path construction - Yan Yean Heritage Pipe Track	Construct shared path - Yan Yean Pipetrack from Bush Boulevard to Williamsons Road	Short	No
PED-03	Shared Path	Shared path construction - Meadowglen athletics centre	Construct shared path - Meadowglen athletics fields - McDonalds Road to Findon Road	Medium	No
PED-04	Shared Path	Shared path construction - Heath Court Drain	Construct shared path - Heath Court Drain - Plenty Road to Oleander Drive	Medium	Yes (part- land in private ownership)
PED-05	Shared Path	Shared path construction - Pipe Track	Construct shared path - Pipetrack from Plenty Road to Danaher Drive	Long	No
PED-06	Shared path / shared footbridge	Shared path / shared footbridge construction - Plenty Road	Construct shared path and shared footbridge - Plenty Road - Wealthiland Drive to existing shared path (south of Axis Centre) - Plenty Road	Short	No
<b>PUBLIC TRANSPORT</b>					
PUB-01	Public Transport	Tram line extension	Extension of the Route 86 tram from Bundoora to South Morang	Medium - Long	No
PUB-02	Public Transport	Mernda rail extension	Extension of the railway line from South Morang to Mernda	Short	No

Figure 35 : Infrastructure Table - Pedestrian, Cycling, and Public Transport

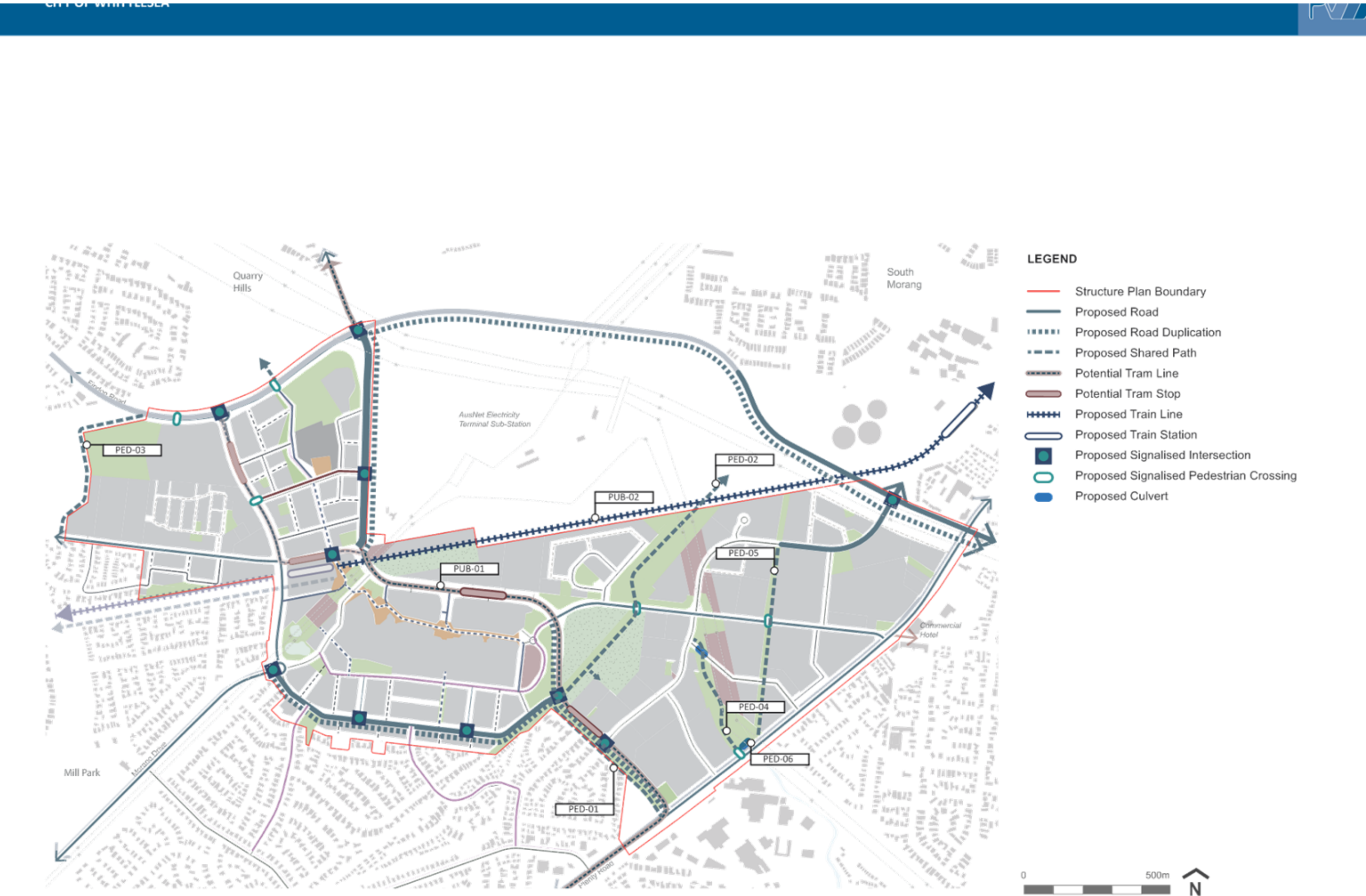


Figure 36 : InfraStructure Plan - Pedestrian, Cycling, and Public Transport



## IMPLEMENTATION

Penty Valley Town Centre Draft Structure Plan

Plan Code	Category	Infrastructure Item	Description	Timing	Developer Works
<b>COMMUNITY FACILITIES AND BUILDINGS</b>					
CI-01	Building	Civic centre redevelopment	Civic centre redevelopment	Medium	No
CI-02	Community Facility	Community space	Community meeting and activity spaces	Medium	No
<b>PUBLIC OPEN SPACE AND PUBLIC REALM</b>					
POS-01	Open Space	Heritage park	Embellish land owned by Melbourne Water at McDonalds Road to develop an area of open space - incorporate buried bluestone reservoir for heritage interpretation	Short	No
POS-02	Open Space	Tree Reserve / Small Park	Creation of a small public open space including a retained River Red Gum as part of development of site - Corner of Civic Drive and Peyton Drive	Medium	Yes
POS-03	Public Realm	Civic square	Construction of urban civic square as part of Civic centre redevelopment	Medium	No
POS-04	Open Space	Large Park	Embellish land in the Civic Precinct to create a common green.	Medium	No
POS-05	Open Space	Small park	Creation of small public open space including a retained River Red Gum as part of development of site - South-east corner of Danaher Drive and Murdoch Road	Long	No
POS-06	Public Realm	Urban Squares	Development of urban squares for public use as part of development of shopping centre.	Short	Yes
POS-07	Open Space	Tree Reserve	Creation of tree reserve between Westfield open-air car park and Civic Drive extension in order to retain several significant River Red Gums	Medium	Yes
POS-08	Open Space	Small park	Creation of small public open space on land between McDonalds Road and railway line.	Medium	No
POS-09	Public Realm	Streetscape landscaping	Landscaping streetscape works along Ferres Boulevard.	Medium	No
<b>DRAINAGE</b>					
DRA-01	Drainage	Embellish existing waterways	Embellish existing open drainage channel for improved function and amenity including potential Water Sensitive Urban Design	Medium	No
DRA-02	Drainage	Embellish existing waterways	Embellish existing sediment pond, rain garden and outfall trench for improved function and amenity including potential Water Sensitive Urban Design	Medium	Yes

Figure 37 : Infrastructure Table - Community Facilities, Public Realm and Open Space, and Drainage

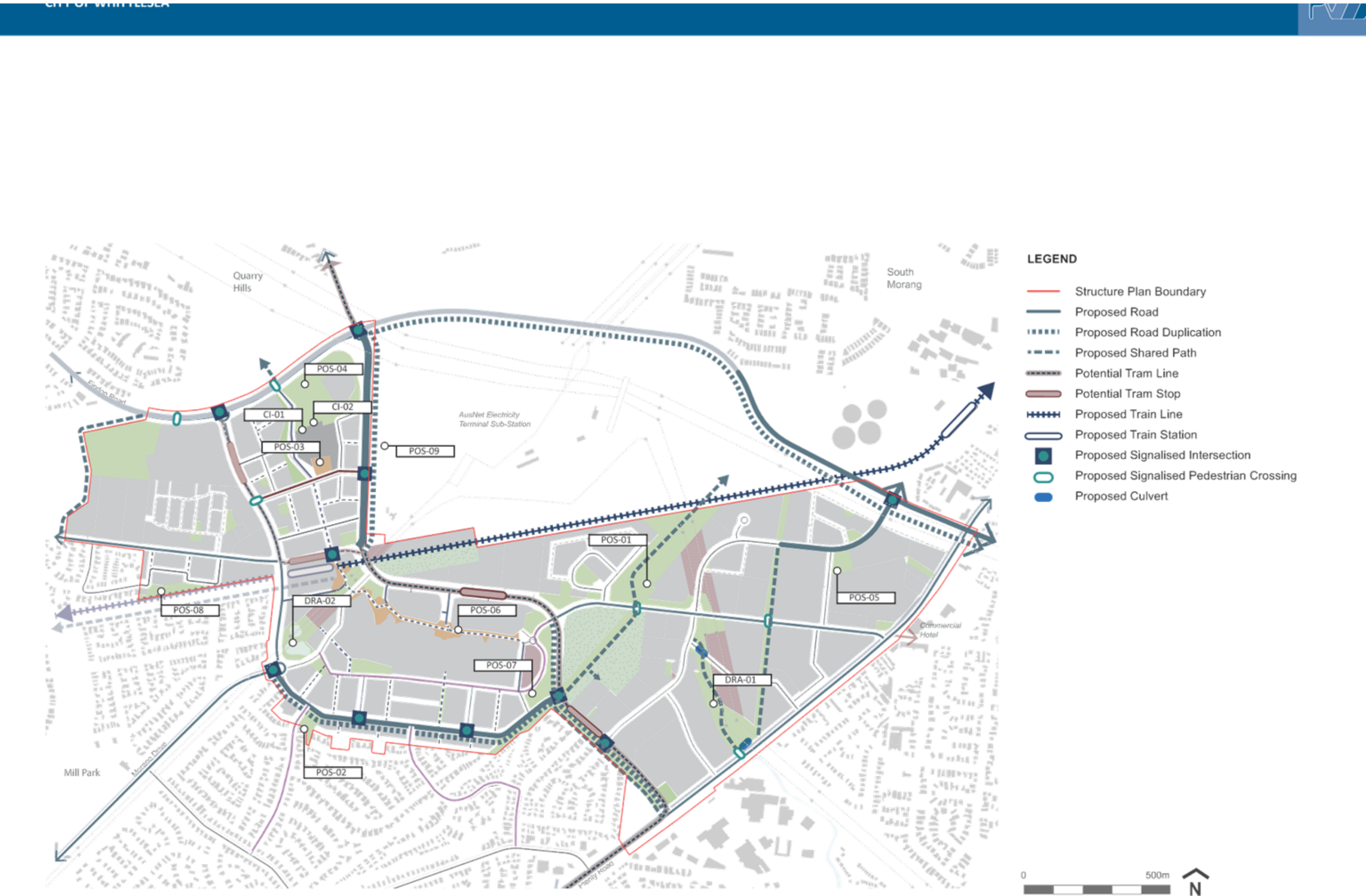


Figure 38 : InfraStructure Plan - Community Facilities, Public Realm and Open Space, and Drainage



## 4.4 Infrastructure Contributions Plan (ICP)

Development proponents within the Plenty Valley Town Centre Structure Plan will be bound by the Plenty Valley Town Centre Infrastructure Contributions Plan (the ICP).

The ICP will establish a framework for development proponents to contribute a fair proportion towards the cost of works and services for the new development in the Town Centre.

The ICP will set a standard rate which will be required to be contributed for new development which will vary according to the use of the land. The rate will be determined by the Minister for Planning and formalised through a Ministerial Direction.

The ICP will be incorporated into the Whittlesea Planning Scheme.

Development proponents wishing to develop prior to incorporation of this ICP can enter into agreements with the City of Whittlesea under Section 173 of the Planning and Environment Act 1987 to expedite contributions. Contributions will be required to be made consistent with the likely contributions required in accordance with a future ICP.

Figure 39 identifies the infrastructure items required to support the Plenty Valley Town Centre which may be included in a future ICP. The location of the items is shown in Figure 40.

A future Ministerial Direction will clarify which items/projects will be funded by a future ICP in urban renewal areas.

Figure 39 : Infrastructure Table - Potential ICP Items

Plan Code	Category	Infrastructure Item	Description
RD-01	Road	Road duplication - Bush Boulevard	Construct second carriageway - Bush Boulevard
RD-03	Road	Road construction - Findon Road	Findon Road Extension from The Great Eastern Way to Plenty Road
RD-05	Road	Road duplication - Ferres Boulevard	Construct second carriageway - Ferres Boulevard - between Findon Road and McDonalds Road
RD-09	Public Realm	Streetscape improvement	Traffic calming measures and streetscape improvement to Peyton Drive and Stillman Drive
RD-10	Public Realm	Streetscape improvement	Streetscape improvement works on McDonalds Road between Civic Drive and Ferres Blvd to prioritise pedestrian movements.
RD-11	Road	Road Upgrade - Civic Drive	Upgrade and widening of Civic Drive
IN-01	Road	Signalised intersection	Signalisation - Ferres Boulevard and Main Road (Council access road)
IN-02	Road	Signalised intersection	Signalise intersection - Ferres Boulevard and Findon Road
IN-03	Road	Signalised intersection	Signalise intersection - Findon Road and Civic Drive
IN-04	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing - Findon Road - east of Civic Drive
IN-05	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing on Civic Drive between Mill Park Secondary College and Civic Precinct
IN-06	Road	Signalised pedestrian crossing	Construct signalised pedestrian crossing at Findon Road between Mill Park Secondary College sports fields and Quarry Hills Park
IN-08	Road	Signalised intersection	Signalise intersection - Morang Drive and Civic Drive
IN-09	Road	Signalised intersection and connection	Signalise intersection - Civic Drive and Bush Boulevard. Connection to Civic Drive extension
IN-10	Road	Signalised intersection	Signalise intersection - Oleander Drive and Bush Boulevard - potential to incorporate tram stop
PED-01	Shared Path	Shared path construction - Bush Boulevard	Construct shared path - Bush Boulevard - Plenty Road to Westfield Shopping Centre - Bush Blvd
PED-03	Shared Path	Shared path construction - Meadowglen athletics centre	Construct shared path - Meadowglen athletics fields - McDonalds Road to Findon Road
PED-04	Shared Path	Shared path construction - Heath Court Drain	Construct shared path - Heath Court Drain - Plenty Road to Oleander Drive
PED-06	Shared path / culvert	Shared path / culvert construction - Plenty Road	Construct shared path and culvert - Plenty Road - Wealthland Drive to existing shared path (south of Axis Centre) - Plenty Road
CI-02	Community Facility	Community space	Community meeting and activity spaces
POS-03	Public Realm	Civic square	Construction of urban civic square as part of Civic centre redevelopment
POS-09	Public Realm	Streetscape landscaping	Landscaping streetscape works along Ferres Boulevard.
DRA-02	Drainage	Embellish existing waterways	Embellish existing sediment pond, rain garden and outfall trench for improved function and amenity including potential Water Sensitive Urban Design
ADMIN-01	Plan Costs	Plan Preparation Costs	Preparation of documents and plans to support Infrastructure Contributions Plan.

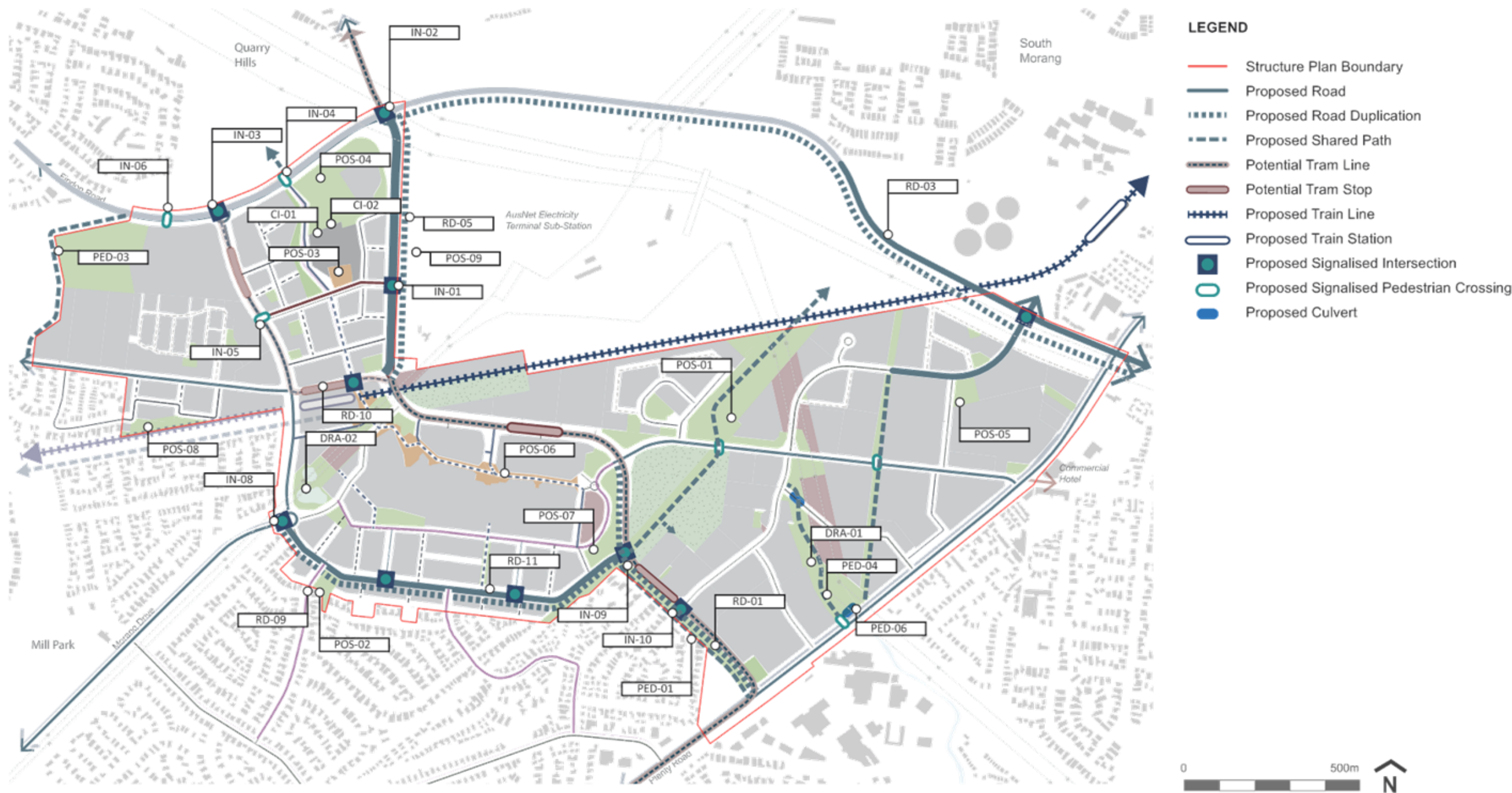


Figure 40 : InfraStructure Plan - Potential ICP Items





## 4.5 Monitoring and Review

The City of Whittlesea will monitor the implementation of the Structure Plan and the deliverance of actions. A report should be prepared on an annual basis to measure progress and ensure adequate resources and attention is allocated to delivering key actions.

The Structure Plan should be reviewed every five years to ensure that it remains relevant and consistent with Council's plans and policies, and to identify any changes required to respond to new trends, policies or changing circumstances.









# Appendix



## 5.1 Glossary of Key Terms

**Activation:** building frontages that provide for some level of interaction with the street through the use of windows, or doors opening on to or facing the street.

**Active Transport:** transport requiring physical activity, typically walking and cycling.

**Activity Centre:** suburban centres that provide a focus for services, employment, housing, transport and social interaction.

**Affordable Housing:** housing where the cost (whether of mortgage repayments or rent) is no more than 30 per cent of the household's income.

**Arterial Road:** a higher order road providing for moderate to high volumes at relatively high speeds typically used for inter-suburban journeys and linking to freeways.

**Boulevard:** a street which contains a central median with tree planting.

**Bulk:** the combined effect of the arrangement, volume and shape of a building or group of buildings.

**Capping:** distinct built form on top of a building, usually as an architectural statement. Can be a new development on top of an existing building to provide additional design value and/or function.

**Connector Boulevard Street:** a lower order street providing for low to moderate volumes and moderate speeds linking local streets to the arterial network, with a central median including street tree planting.

**Connector Street:** a lower order street providing for low to moderate volumes and moderate speeds linking local streets to the arterial network.

**Encumbered Open Space:** land for public open space that is constrained for development purposes by easements (electricity transmission lines, sewers or gas), drainage, or native vegetation for example.

**Environment:** our physical surrounds, including land, waters, atmosphere, climate, sounds, odours, animals and plants, as well as aesthetics.

**Environmentally Sustainable Design (ESD):** the use of design principles and strategies to help reduce the environmental impact of buildings.

**Façade:** the principal face of a building.

**Frontage:** the road alignment at the front of a lot. If a lot abuts two or more roads, the one to which the building, or proposed building faces.

**Gateway Location:** locations where a significant number of people enter and exit a precinct or neighbourhood. They occur at a variety of scales, including to an area as a whole, to precincts, or to specific developments.

**Heat Island Effect:** The heat island effect describes the pattern of higher temperatures in urban areas in comparison to surrounding areas, due to the increased thermal storage capacity of concrete, asphalt and other materials in these areas.

**High Density Housing:** more than 80 dwellings per net residential hectare.

**Housing Density:** one of several measures that describe how intensively an urban area is developed.

**Incorporated Document:** are documents which are listed in the Table to Clause 81.01 in the planning scheme and support the planning scheme. They must be taken into account as part of decision making.

**Infrastructure:** basic urban facilities and networks needed for the functioning of a local community or broader society.

**Justice Precinct:** a precinct for justice related land uses such as court house, police station and ancillary uses.

**Key Corner Site:** a key site on the corner of a major intersection identified to accommodate a larger and prominent building.

**Key Marker Building:** a prominent or iconic building located at an important node or intersection.

**Landscape Priority:** streets which contain and prioritise significant tree planting and landscaping.

**Local Road:** a lower order street providing for low volumes and low vehicle speeds providing access to individual properties.

**Low Density Housing:** more than 20 dwellings and generally less than 30 dwelling per net residential hectare.

**Medium Density Housing:** generally, between 30 - 80 dwellings per net residential hectare.

**Native Vegetation:** plants that are indigenous to Victoria, including trees, shrubs, herbs, and grasses.

**Node:** a place where activity and routes are concentrated.

**Open Space:** includes land reserved for natural landscape, parklands, recreation and active sports, as well as waterways

**Pedestrian Priority:** streets where pedestrian movements are prioritised over vehicle movements.

**Public Benefit:** features of a site or building which are of benefit to the wider community which can be:

- publicly accessible and embellished open space areas on site (additional to any public open space contribution under Clause 52.01 of the Planning Scheme); and
- provision of social housing in the building.

**Scale:** the size of a building in relation to its surroundings

**Sensitive Interface:** where lower-scale residential development transitions to higher, denser and more active types of development.

**Setback:** the minimum distance from any allotment boundary to a building.

**Social Housing:** a dwelling unit or units whose title is transferred to a non-for-profit registered housing association or provider for the purposes of low cost rental housing. Social housing is an overarching term that covers both public housing and community housing.

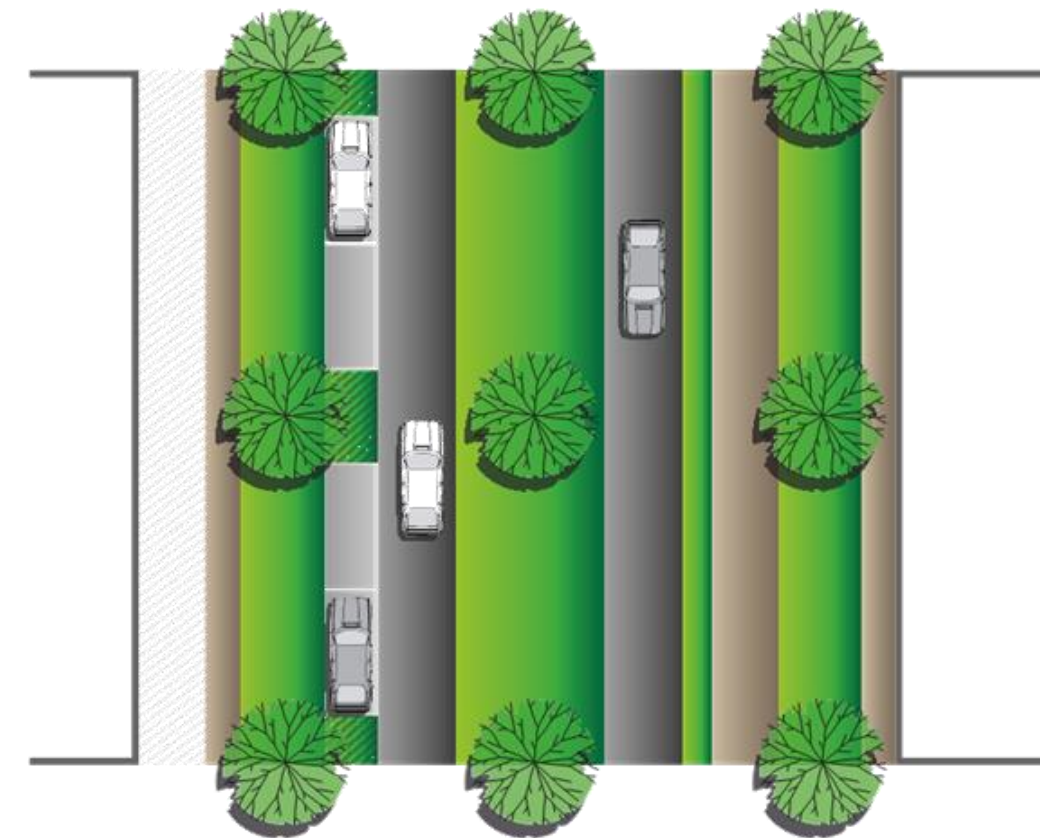
**Street Wall:** means any part of the building constructed on a lot boundary fronting the street.

**Walkable:** a condition of a system of routes which are barrier free, interesting, safe, well-lit, comfortable and inviting to pedestrian travel.

**Wrapping:** parts of a proposed building that cover an otherwise blank wall, generally to provide greater engagement with the street and an improved appearance. This also applies to new development abutting an existing building to cover a blank wall.



Civic Drive - Connector Street [30.0m]



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## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

**Submission No. 1**

**Summary of Submission:** Submission on behalf of significant in Shopping Centre Precinct (south of Civic Drive) generally supporting the structure plan but also raising a range of technical matters.

Summary of Issues Raised	Comment	Structure Plan Updates
<ul style="list-style-type: none"> <li>Support mixed use nomination to 6 storeys and proposed setback to Civic Drive and traffic connections from Civic Drive to both Stillman Drive and Peyton Drive.</li> <li>Seek inclusion of part of site fronting central road within development for 6 storey mixed use.</li> <li>Requests increase 2 storey min height to &lt;8m as per NRZ, and limit it to 30% of site depth.</li> </ul>	A review of the built form controls and street network in this precinct has been undertaken. The preferred building heights has been reduced from 6 storeys to 4 storeys to provide consistency with existing Development Plan and provide a transition to existing residential areas. The height of 2 storey development has been retained at 7m. Local connections to Civic Drive to both Stillman Drive and Peyton Drive have been retained as 'Left In Left Out' arrangements.	<ul style="list-style-type: none"> <li>Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been reviewed and updated.</li> <li>Figure 21: Road Network and Intersection Plan (page 45) has been amended to stagger the intersection of local roads with Civic Drive and relocate the proposed traffic signals.</li> </ul>
Does not support designation of western end as max 2 storeys or the designated local street-pedestrian priority street with a range of reasons provided.	Two storey development is appropriate given the interface with existing low scale residential on two sides. Agree that the pedestrian priority local street is not a critical connection or integral to the street network and may be problematic in developing a good design.	<ul style="list-style-type: none"> <li>Figure 21: Road Network and Intersection Plan (page 45) has been amended to remove the pedestrian priority local street on this site.</li> </ul>
Object to DES1.3 requiring minimum private open space provision. Request standard Rescode provisions to apply to provision of private open	Agree. Standard Res Code should apply and DES1.3 to be reviewed	<ul style="list-style-type: none"> <li>DES1.3 (page 32) has been amended to remove private open space area specification.</li> </ul>





## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

space.		
Do not support pedestrian priority streets—pedestrian priority access dissecting the site other than those which generally link to existing streets.	These local streets/access ways provide for important north south connectivity for pedestrians and therefore it is important to show them so that they are provided for as part of the future development of the site. Development proposals need to be generally in accordance with the Structure Plan and the actual alignment of the streets will be subject of further design. These streets have been removed from other plans such as the Building Heights Plan which do not show need to show a permeable street network.	<ul style="list-style-type: none"> <li>Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been updated to remove the potential local streets. These have been retained on Figure 21: Road Network and Intersection Plan (page 45) to show a permeable street network.</li> </ul>
Do not support pocket park unless an agreement is made to offset/compensate for the additional open space contribution. Agreement and/or compensation is required for the additional open space contribution/ otherwise tree can be retained within development layout.	The pocket park/ tree reserve provides for the protection of a significant River Red Gum and will provide for the amenity of residents within a high density environment. Therefore, it is considered appropriate to retain it in the Structure Plan.	<ul style="list-style-type: none"> <li>Figure 37 Infrastructure Table (page 78) includes a reference to this open space area as 'POS-07 Tree Reserve'.</li> </ul>

**Submission No. 2**

**Summary of Submission:** Submission on behalf of significant landowner in Live and Work Precinct raising a range of technical matters.

Summary of Issues Raised	Comment	Structure Plan Updates
Request a review of building heights to ensure greater consistency with approved Development Plans. Allow	The proposed building heights have been reviewed and simplified. They now better align with existing controls and development. A figure which caused ambiguity in the draft plan has been deleted and replaced by Figure 11 (page 34) and which presents the building heights in a clear table	<ul style="list-style-type: none"> <li>Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls</li> </ul>



## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

greater flexibility for site specific design responses and allow for increases in preferred building heights if it can be demonstrated that no significant impact will occur on neighbouring properties.	supported by a plan (Figure 13 on page 35). The building heights will not be mandatory and it is not considered necessary to provide any greater flexibility. However, there is the opportunity to support good outcomes such as the provision of social housing and additional public open space in exchange for greater density.	(page 36) have been reviewed and updated to improve the clarity of controls and ensure that higher density development occurs in appropriate locations in the Town Centre and is appropriately designed. New controls have been included to provide for greater density where social housing or additional public open space is provided.
Question the pedestrian spine connection to Masons Point including its physical access and intended purpose.	The pedestrian spine is important to provide pedestrian connectivity between the precincts including residents of Masons Point to the shopping centre and train station.	<ul style="list-style-type: none"> <li>• No change to Structure Plan</li> </ul>
Seek retention of 8 storey allowable building heights on Boulevard and Connector streets as per existing DP.	The proposed building heights have been reviewed and simplified. They now better align with existing controls and development. The precinct has been identified as having a maximum building height of 6 storeys, however three key sites including at the corner of Bush Blvd and Oleander Drive have been identified as Key Corner Buildings and permitted up to 8 storeys.	<ul style="list-style-type: none"> <li>• Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been reviewed and updated.</li> </ul>
Include additional text to explain rationale behind Indicative building Setbacks and Gateways Plan	It is appropriate to define gateways and setbacks in the plan to inform future controls. 'Indicative building Setbacks and Gateways Plan' requires supporting text in opening sentence and strategies. The plan (Figure 15) has been simplified and made easier to understand.	<ul style="list-style-type: none"> <li>• A Glossary of key terms (page 86) has been included to better explain key terms used in the document.</li> <li>• DES4.1 (page 33) has been revised to make clear the intention of the terms gateway' sites and 'key marker buildings'.</li> <li>• Figure 14 Built Form Controls (page 36) has been updated to</li> </ul>

Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

		<p>include setback provisions.</p> <ul style="list-style-type: none"> <li>• Figure 15 Indicative Building Setbacks and Gateways Plan (page 37) has been simplified and made easier to understand.</li> </ul>
Show site at corner of intersection at Bush Blvd and Oleander Dve as high density.	Agree. This has been amended.	<ul style="list-style-type: none"> <li>• Figure 23: Residential Development Density has been updated to show land in Masons Point being suitable to be developed for high density (page 47).</li> </ul>
Include additional text to describe 'key marker building'	Provide additional description of key marker buildings in discussion/glossary and strategies.	<ul style="list-style-type: none"> <li>• A Glossary of key terms (page 86) has been included to better explain key terms used in the document such as 'key marker buildings'.</li> <li>• DES4.1 (page 33) has been revised to make clear the intention of the terms gateway' sites and 'key marker buildings'.</li> </ul>
Figure 28 - The note 'promote the landscaping of McDonalds Road...' points to the wrong section of the plan.	Agree.	<ul style="list-style-type: none"> <li>• Figure 30 (page 65) has been updated.</li> </ul>
Question the purpose of a key pedestrian/cycle connection along the pipetrack between Plenty Road and the railway line to the north, but rather suggest pathways along the road network will suffice.	The pipe track provides the opportunity for completely off road cycle/pedestrian path limiting conflict with vehicles. The extension to the railway line is subject to a shared path being provided along the rail corridor.	<ul style="list-style-type: none"> <li>• No change to Structure Plan</li> </ul>



## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

Supporting text is required to explain the identification for McDonalds Road as an 'alternative frontage'.	Explain 'alternative frontage' nomination in legend or glossary	<ul style="list-style-type: none"> <li>A Glossary of key terms (page 86) has been included to better explain key terms used in the document including 'frontage'.</li> </ul>
Support health services in Precinct 4 and 5.	Health services are an industry supported in the employment precinct. There is an opportunity to provide direction in respect to uses which are compatible with residential use (such as health) in the Live and Work precinct would be supported.	<ul style="list-style-type: none"> <li>LO1.8 (page 62) in the Live and Work precinct has been amended to included reference to 'health'.</li> </ul>

**Submission No. 3**

**Summary of Submission:** *Petition signed by 35 residents objecting to the connection of Stillman Drive to Civic Drive due to traffic and safety concerns.*

Summary of Issues Raised	Comment	Structure Plan Updates
Concerns with connection of Stillman Drive with Civic Drive due to increased traffic and safety impacts.	<p>The Civic Drive and local street connections are considered vital for a permeable and connected street network and have been foreshadowed since the earliest planning of the Mill Park North and Activity Centre area in the 1970s and 1980s. They were included in the <i>South Morang Activity Centre Overall Development Plan, 1998</i> and other more recent documents including the <i>South Morang Development Plan, 2009</i>. The connections will provide for improved pedestrian and vehicle connectivity for local residents to the Town Centre.</p> <p>It is important to note that the draft Structure Plan proposes to retain the Stillman Drive and Peyton Drive as 'Local Roads' only and will not upgrade their status to Collector Roads. This was occasionally misinterpreted during consultation so it is important that the final Structure Plan contains a clear street hierarchy.</p> <p>The designation of Stillman Drive and Peyton Drive as 'Local Roads' means that they are only intended to service the residents in the local area and are not designed to promote through traffic movement. There will be a</p>	<ul style="list-style-type: none"> <li>Figure 21: Road Network and Intersection Plan (page 45) has been amended to stagger the intersection of local roads with Civic Drive and relocate the proposed traffic signals.</li> <li>Include new direction CSD1.13 'ensure that connection of and Peyton Drive to Civic Drive are designed to provide local access for residents and include traffic calming measures to restrict and discourage undesired traffic movement (page 60).</li> <li>Include infrastructure item RD-09, traffic calming measures and streetscape improvements to</li> </ul>

## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

	<p>small increase in the traffic levels from local residents who will use the streets to access the Town Centre however it is envisaged it will be minimal and of a local nature. The purpose of the connection is to purely improve connectivity for local residents. Therefore, the narrowness of the street is considered appropriate and conducive to maintaining a local road function, reducing traffic speed and providing a safe environment for pedestrians.</p> <p>Traffic modelling indicates the significant majority of traffic will continue to utilise arterial and collector roads such as; Morang Drive and Bush Boulevard. It is recommended that the role and function of different streets be clearly explained in the final Structure Plan.</p> <p>The ultimate 'Road Network' proposed in the draft Structure Plan includes a number of key connections which are currently not provided such as the Findon Road and Civic Drive extensions. These road projects, along with the Mernda Rail extension (due in 2019), will improve the overall transport system in the Town Centre. This will assist in distributing traffic across the network and reduce pressure on local streets.</p> <p>There is opportunity to explore how the final Structure Plan can help to ensure that local streets reflect their local road function and restrict 'non-local movements'. In order to inform this, it is important to understand the current use and conditions of the street. It is recommended that further investigations be undertaken in respect to the current use of the streets.</p> <p>In terms of future options, there are a range of street design initiatives which could be explored to reinforce the local road status, reduce potential traffic flows, encourage safe driver behaviour (e.g. slower vehicle speeds) and provide a safe pedestrian environment. Examples include; narrow street entrances, traffic calming devices and alternative pavement. These measures provide a signal to drivers that they are in a residential street. The final Structure Plan identifies the need to consider these</p>	<p>Peyton Drive and Stillman Drive (page 72).</p> <ul style="list-style-type: none"> <li>• Indicative cross section for Civic Drive has been included in Appendix 5.2 (page 87) which identifies a boulevard treatment.</li> </ul>
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Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

	<p>measures as part of the design of future road connections.</p> <p>In summary the updated draft Structure Plan incorporates the following changes:</p> <ol style="list-style-type: none"> <li>1. Downgrade Peyton Drive and Stillman Drive intersections with Civic Drive to 'Left In Left Out' only to restrict access and discourage traffic;</li> <li>2. Relocation of signalised intersections away from Peyton Drive and Stillman Drive so they only serve the function of providing direct connection to Westfield Plenty Valley;</li> <li>3. Reconfiguration of Civic Drive extension into a 'boulevard' with a central median to minimise turn movements into Peyton Drive and Stillman Drive;</li> <li>4. Ensure the design of Peyton Drive and Civic Drive intersection maximises pedestrian safety and discourage undesired traffic movement to local streets;</li> <li>5. Identifying additional traffic calming measures for Peyton Drive and Stillman Drive, if required; and,</li> <li>6. Clearly identifying Peyton Drive and Stillman Drive as 'local streets' only.</li> </ol>	
Concerns with 3-4 storey apartments along Civic Drive.	<p>The nomination of high density residential across the Town Centre is supported by both State and Local policies and strategies. The Town Centre is well serviced with facilities and public transport and is well suited to supporting higher densities of development. Supporting higher densities in the Town Centre will reduce pressure for such development to be built in less appropriate locations. The building heights and residential density plans have been reviewed to ensure their appropriateness.</p> <p>As part of this process the built form controls for development abutting existing residential areas have been reviewed. The Structure Plan includes specific controls for 'sensitive interface areas' which abut existing residential areas. In these areas development must be setback 9m from the boundary and 16m for higher densities (above 2 storeys). This ensures that there is no overlooking or overshadowing impacts on adjoining properties and provides a transition of building heights. Essentially the controls replicate existing built form in standard residential areas such as</p>	<ul style="list-style-type: none"> <li>• Figure 13: Buildings Heights (page 35) has been amended to clearly show a maximum of 4 storeys for development along Civic Drive and identify sensitive interface areas where development is to be less than 2 storeys.</li> <li>• Figure 14: Built form controls (page 36) has been amended to include specific controls for sensitive interfaces including a 9m setback and 16m setback for development more than 2 storeys. The controls required that development transitions in density</li> </ul>

Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

	Buick Drive in the interface area. Development along Civic Drive will be a maximum of 4 storeys consistent with current controls.	and has minimal overshadowing and overlooking impacts.
Proposed route of tram 86 extension should be along Ferres Boulevard north of South Morang Train Station.	The alternative route was suggested as an option to improve the viability of the tram extension. It is recognised that the community preference for the tram route to be along Ferres Boulevard. It is proposed that this be shown as the preferred route and that the route along Civic Drive be shown as an alternative. Ultimately, there is further investigation and a feasibility study to be undertaken which will investigate all options.	<ul style="list-style-type: none"> <li>Figure 20: Public Transport Plan (page 43) has been amended to show the proposed route along Ferres Boulevard as the primary route and the route along Civic Drive as an alternative option. The figure has been updated to more clearly show the tram route extended north along the Lakes Boulevard.</li> </ul>

**Submission No: 4**

**Summary of Submission:** Submission from local community group (Friends of South Morang) recommending that the originally proposed extension to the No.86 Tram route be supported by the structure plan.

Summary of Issues Raised	Comment	Structure Plan Updates
Support overall development framework.	Noted.	No change to Structure Plan
Proposed route of tram 86 extension should be along Ferres Boulevard north of South Morang Train Station.	The alternative route was suggested as an option to improve the viability of the tram extension. It is recognised that the community preference for the tram route to be along Ferres Boulevard. It is proposed that this be shown as the preferred route and that the route along Civic Drive be shown as an alternative. Ultimately, a feasibility study will need to be undertaken which will investigate all options.	<ul style="list-style-type: none"> <li>Figure 20: Public Transport Plan (page 43) has been amended to show the proposed route along Ferres Boulevard as the primary route and the route along Civic Drive as an alternative option. The figure has been updated to more clearly show the tram route extended north along the Lakes Boulevard.</li> </ul>
Civic Drive extension will	This has been discussed in detail in Submission No.3.	As per Submission No. 3



Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

create unreasonable traffic impacts on local streets.	<p>In summary the updated draft Structure Plan incorporates the following changes:</p> <ol style="list-style-type: none"> <li>1. Downgrade Peyton Drive and Stillman Drive intersections with Civic Drive to 'Left In Left Out' only to restrict access and discourage traffic;</li> <li>2. Relocation of signalised intersections away from Peyton Drive and Stillman Drive so they only serve the function of providing direct connection to Westfield Plenty Valley;</li> <li>3. Reconfiguration of Civic Drive extension into a 'boulevard' with a central median to minimise turn movements into Peyton Drive and Stillman Drive;</li> <li>4. Ensure the design of Peyton Drive and Civic Drive intersection maximises pedestrian safety and discourage undesired traffic movement to local streets;</li> <li>5. Identifying additional traffic calming measures for Peyton Drive and Stillman Drive, if required; and,</li> <li>6. Clearly identifying Peyton Drive and Stillman Drive as 'local streets' only.</li> </ol>	<ul style="list-style-type: none"> <li>• Figure 21: Road Network and Intersection Plan (page 45) has been amended to stagger the intersection of local roads with Civic Drive and relocate the proposed traffic signals.</li> <li>• Include new direction CSD1.13 'ensure that connection of and Peyton Drive to Civic Drive are designed to provide local access for residents and include traffic calming measures to restrict and discourage undesired traffic movement (page 60).</li> <li>• Include infrastructure item RD-09, Traffic calming measures and streetscape improvements to Peyton Drive and Stillman Drive (page 72).</li> <li>• Indicative cross section for Civic Drive has been included in Appendix 5.2 (page 87). ) which identifies a boulevard treatment.</li> </ul>
High density residential is inappropriate due to traffic impacts	<p>The nomination of high density residential across the Town Centre is supported by both State and Local policies and strategies. The Town Centre is well serviced with facilities and public transport and is well suited to supporting higher densities of development. Supporting higher densities in the Town Centre will reduce pressure for such development to be built in less appropriate locations. The building heights and residential density plans have been reviewed to ensure their appropriateness. Car parking must be provided in accordance with the Clause 52.06 of the Planning</p>	<ul style="list-style-type: none"> <li>• Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been reviewed and updated to improve the clarity of controls and ensure that higher density development occurs in</li> </ul>

Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

	Scheme.	appropriate locations in the Town Centre and is appropriately designed. <ul style="list-style-type: none"> <li>New controls have been included for 'sensitive interfaces' where new development abuts existing residential areas to ensure that it does not detrimentally affect these properties (page 36).</li> </ul>
A Tram stop should be nominated on Plenty Road.	Supported. This is included but has not been shown consistently across all plans.	<ul style="list-style-type: none"> <li>Figure 20: Public Transport Plan (page 43) has been amended to show the potential tram stop on Plenty Road.</li> </ul>
A slip lane should be provided to turn left into Plenty Road from Bush Boulevard.	There is merit in investigating the option of a slip lane from Bush Boulevard into Plenty Road. However, this is detail that is too fine for a Structure Plan and will require further analysis and discussion with VicRoads.	<ul style="list-style-type: none"> <li>No change to Structure Plan.</li> </ul>

**Submissions 5-7**

**Summary of submissions:** All submissions raised concern/ objection to the connection of Stillman Drive to Civic Drive due to safety and traffic issues.

Summary of Issues Raised	Comment	Structure Plan Updates
<ul style="list-style-type: none"> <li>Connection of Stillman Drive and Civic Drive should be properly assessed.</li> <li>Safety measures should be implemented onto Stillman Drive.</li> </ul>	<p>This has been discussed in detail in Submission No.3.</p> <p>In summary the updated draft Structure Plan incorporates the following changes:</p> <ol style="list-style-type: none"> <li>Downgrade Peyton Drive and Stillman Drive intersections with Civic Drive to 'Left In Left Out' only to restrict access and discourage traffic;</li> </ol>	<p>As per Submission No. 3</p> <ul style="list-style-type: none"> <li>Figure 21: Road Network and Intersection Plan (page 45) has been amended to stagger the intersection of local roads with Civic Drive and relocate the proposed traffic signals.</li> </ul>



Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

<ul style="list-style-type: none"> <li>Do not support Stillman Drive connecting to Civic Drive due impacts of increase traffic and safety issues.</li> </ul>	<p>2. Relocation of signalised intersections away from Peyton Drive and Stillman Drive so they only serve the function of providing direct connection to Westfield Plenty Valley;</p> <p>3. Reconfiguration of Civic Drive extension into a 'boulevard' with a central median to minimise turn movements into Peyton Drive and Stillman Drive;</p> <p>4. Ensure the design of Peyton Drive and Civic Drive intersection maximises pedestrian safety and discourage undesired traffic movement to local streets;</p> <p>5. Identifying additional traffic calming measures for Peyton Drive and Stillman Drive, if required; and,</p> <p>6. Clearly identifying Peyton Drive and Stillman Drive as 'local streets' only.</p>	<ul style="list-style-type: none"> <li>Include new direction CSD1.13 'ensure that connection of and Peyton Drive to Civic Drive are designed to provide local access for residents and include traffic calming measures to restrict and discourage undesired traffic movement (page 60).</li> <li>Include infrastructure item RD-09, traffic calming measures and streetscape improvements to Peyton Drive and Stillman Drive (page 72).</li> <li>Indicative cross section for Civic Drive has been included in Appendix 5.2 (page 87) which identifies a boulevard treatment.</li> </ul>
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**Submission No. 8**

**Summary of Submission:** *Petition signed by 47 residents objecting to the connection of Peyton Drive to Civic Drive due to traffic and safety concerns. One also objects to the development of apartment buildings.*

Summary of Issues Raised	Comment	Structure Plan Updates
Do not support apartment buildings in the plan.	The nomination of high density residential across the Town Centre is supported by both State and Local policies and strategies. The Town Centre is well serviced with facilities and public transport and is well suited to supporting higher densities of development. Supporting higher densities in the Town Centre will reduce pressure for such development to be built in less appropriate locations. The building heights and residential density plans have been reviewed to ensure their appropriateness.	<ul style="list-style-type: none"> <li>Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been reviewed and updated to improve the clarity of controls and ensure that higher density development occurs in</li> </ul>

Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

Summary of Issues Raised	Comment	Structure Plan Updates
	As part of this process the built form controls for development abutting existing residential areas have been reviewed. The Structure Plan includes specific controls for 'sensitive interface areas' which abut existing residential areas. In these areas development must be setback 9m from the boundary and 16m for higher densities (above 2 storeys). This ensures that there is no overlooking or overshadowing impacts on adjoining properties and provides a transition of building heights. Essentially the controls replicate existing built form in standard residential areas such as Buick Drive in the interface area. Development along Civic Drive will be a maximum of 4 storeys consistent with current controls.	appropriate locations in the Town Centre and is appropriately designed. New controls have been included for 'sensitive interfaces' where new development abuts existing residential areas to ensure that it does not detrimentally affect these properties (page 36).
Do not support Peyton Drive and connecting it to Civic Drive/Westfield with a signalised intersection due to the following reasons:  - residents have not been consulted.  - will impact their livelihood.  - will increase traffic volumes.  - will encourage rat running.  - will increase noise.  - will affect safety.	This has been discussed in detail in Submission No.3.  In summary the updated draft Structure Plan incorporates the following changes:  1. Downgrade Peyton Drive and Stillman Drive intersections with Civic Drive to 'Left In Left Out' only to restrict access and discourage traffic;  2. Relocation of signalised intersections away from Peyton Drive and Stillman Drive so they only serve the function of providing direct connection to Westfield Plenty Valley;  3. Reconfiguration of Civic Drive extension into a 'boulevard' with a central median to minimise turn movements into Peyton Drive and Stillman Drive;  4. Ensure the design of Peyton Drive and Civic Drive intersection maximises pedestrian safety and discourage undesired traffic movement to local streets;  5. Identifying additional traffic calming measures for Peyton Drive and Stillman Drive, if required; and,	As per Submission No. 3 • Figure 21: Road Network and Intersection Plan (page 45) has been amended to stagger the intersection of local roads with Civic Drive and relocate the proposed traffic signals. • Include new direction CSD1.13 'ensure that connection of and Peyton Drive to Civic Drive are designed to provide local access for residents and include traffic calming measures to restrict and discourage undesired traffic movement (page 60). • Include infrastructure item RD-09, traffic calming measures and streetscape improvements to Peyton Drive and Stillman Drive (page 72).



## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

Summary of Issues Raised	Comment	Structure Plan Updates
	6. Clearly identifying Peyton Drive and Stillman Drive as 'local streets' only.	<ul style="list-style-type: none"> <li>Indicative cross section for Civic Drive has been included in Appendix 5.2 (page 87) which identifies a boulevard treatment.</li> </ul>

**Submission No. 9**

**Summary of Submission:** *Submission promoting a development at 25 Oleander Drive and suggesting building height controls be made flexible to support good development outcomes.*

Summary of Issues Raised	Comment	Structure Plan Updates
Request a review of building heights to ensure greater consistency with approved Development Plans. Allow greater flexibility for site specific design responses and allow for increases in preferred building heights if it can be demonstrated that no significant impact will occur on neighbouring properties.	<p>The proposed building heights have been reviewed and simplified. They not better align with existing controls and development. A figure which caused ambiguity in the draft plan has been deleted and replaced by Figure 11 (page 34) and which presents the building heights in a clear table supported by a plan (Figure 13 on page 35).</p> <p>The building heights will not be mandatory and it is not considered necessary to provide any greater flexibility. However, there is the opportunity to support good outcomes such as the provision of social housing and additional public open space in exchange for greater density.</p>	Figures 11 Building Height Controls (page 34), Figure 13 Building Heights Plan (page 35) and Figure 14 Built Form Controls (page 36) have been reviewed and updated to provide improve the clarity of controls and ensure that higher density development occurs in appropriate locations in the Town Centre and is appropriately designed. New controls have been included to provide for greater density where social housing or additional public open space is provided.

## Attachment 4- Plenty Valley Town Centre Structure Plan Stage 2 Consultation Submission Summary Table

**Submission No. 10**

**Summary of Submission:** Local resident concerned with the impacts of development including traffic, high density development, noise and light pollution.

Summary of Issues Raised	Comment	Structure Plan Updates
<p>Concerned with increased development and requests buffer between Civic Drive and property to deal with noise and light impacts from new road and development.</p> <p>Concerned that there is no provision for a transition between higher densities and low scale residential.</p>	<p>The longstanding and consistent planning for the area has shown an activity centre in this location. This includes the South Morang Development Plan 1997. As a result, the area is becoming increasingly urbanised with a greater range and number of activities at different times of the day. However, as noted, an appropriate transition between existing residential areas and economic uses needs to be maintained.</p> <p>Similarly, Civic Drive has long been planned to provide an additional east-west connection. The design of this road needs to consider residential areas to the south and how it will integrate with the Town Centre. The updated Structure Plan includes an updated indicative cross section for Civic Drive. The updated cross section is a boulevard style street with landscaping and street trees. This will assist in calming traffic and minimise any potential impacts.</p> <p>It is noted that both the pipetrack (20-30 metres wide) and future Civic Drive (extension) are situated between, providing a separation distance of about 50 metres to future development.</p>	<ul style="list-style-type: none"> <li>Indicative cross section for Civic Drive has been included in Appendix 5.2 (page 87).</li> <li>Figure 14 Built Form Controls (page 36) has been reviewed and updated to ensure new development is appropriately designed and minimises impact on existing residents.</li> </ul>
<p>Comments on the proposed embellishment of the Yan Yean Pipe Track and that this is still yet to occur.</p>	<p>A shared path is planned along the Yan Yean Pipetrack, to connect through Mill Park, the conservation reserve and up to South Morang. This will also be embellished in time, with an opportunity to incorporate a currently buried historic bluestone reservoir into a new public open space along the trail (next to McDonalds Road). This project has been identified in the updated Structure Plan.</p>	<ul style="list-style-type: none"> <li>Figure 35 Infrastructure Table (page 76) includes a reference to this open space area as 'PED-02 Shared Path Construction - Yan Yean Heritage Pipe Track'.</li> </ul>





# Plenty Valley Town Centre

## PARKING PRECINCT PLAN 2017



City of  
Whittlesea



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# 1. Introduction

Plenty Valley is an Activity Centre under the Plan Melbourne strategy.

Provision of car parking within the Plenty Valley Town Centre must be considered in conjunction with various other inter-related factors including existing and future land uses, public transport, road networks and employment in the centre.

The parking policies contained in this Parking Precinct Plan are supported by the findings of the Plenty Valley Town Centre Structure Plan – Sustainable Transport Modelling Background Report (GTA 2014) and the Plenty Valley Town Centre Structure Plan (2016)

The Plenty Valley Town Centre Structure Plan (the Structure Plan) includes a key objective to increase residential and employment densities close to public transport while also improving walking and cycling conditions on order to reduce dependencies on private vehicles. This whole approach includes strategies and actions relating to transport in the centre, including parking.

## 1.1 Parking outcome to be achieved

The aim of the Plenty Valley Precinct Plan is to provide on-site parking in accordance with demand whilst reducing the visual impact of parking and facilitating increased use of sustainable forms of transport.

To this this effect, the plan has the following objectives:

- To sufficiently and effectively provide for parking in the Plenty Valley Town Centre.
- Provide for sufficient car parking as part of new development.
- Encourage the use of active and sustainable travel modes rather than increased private vehicle travel.
- Improve the visual and pedestrian amenity within Plenty Valley Town Centre.
- Address the high the demand for commuter car parking in proximity to the South Morang Train Station.
- Identify further actions to manage parking in the Plenty Valley Town Centre.

## 1.2 Parking precinct area

This Parking Precinct Plan applies to the area within the Plenty Valley Town Centre Structure Plan boundary. This area is shown in Figure 1.

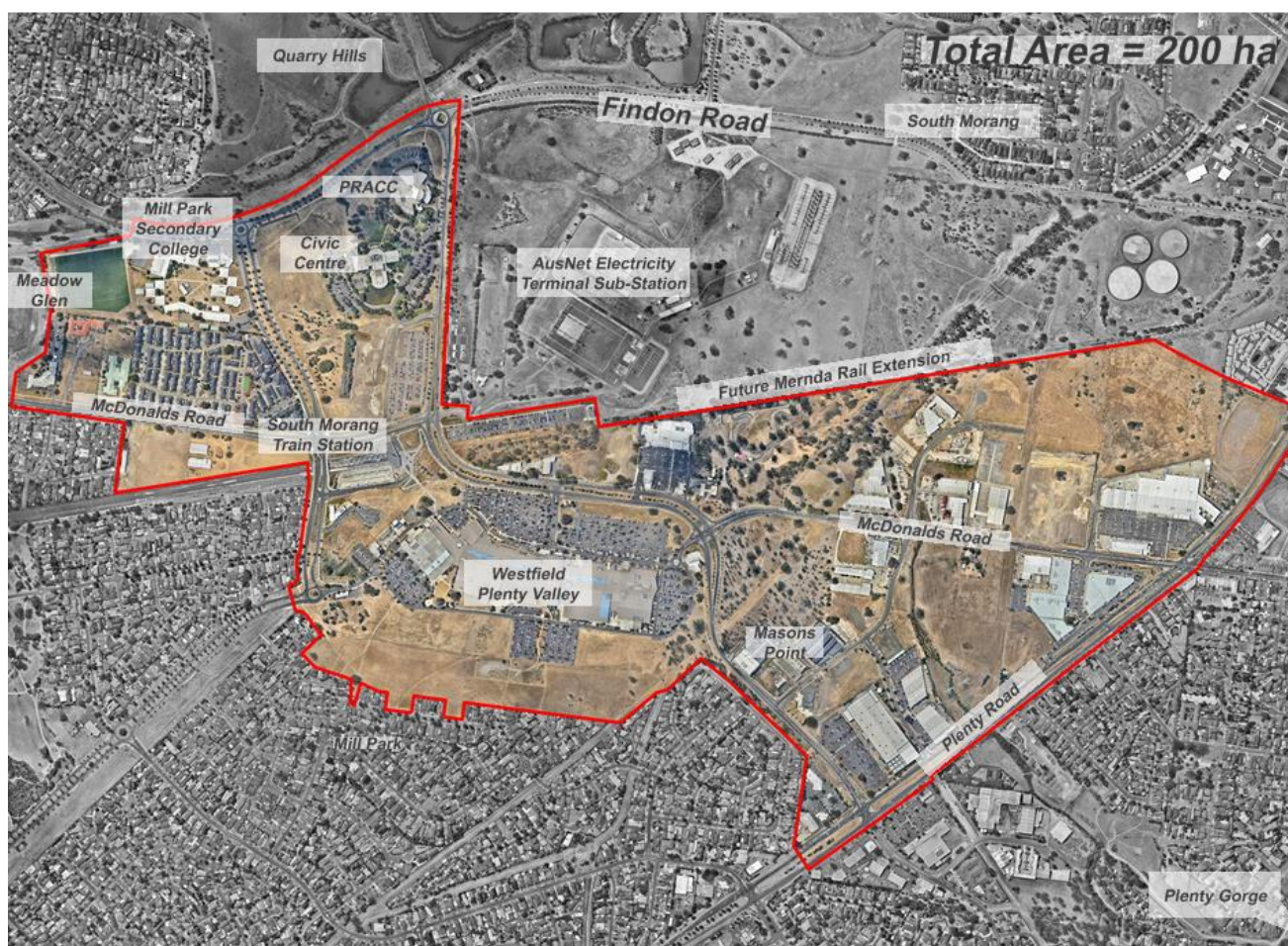


Figure 1 : Plenty Valley Town Centre Structure Plan area



## 2. Existing Car Parking

### 2.1 Parking Supply

As part of the Background Reports prepared for Plenty Valley Town Centre Structure Plan, a parking analysis was undertaken.

The analysis found that there are over 6,000 free car parks are available in off street at-grade car parks within Plenty Valley Town Centre.

Parking has been provided as part of developments, generally at the standard Planning Scheme rates. There are some individual cases of car-parking reductions be permitted.

The existing parking provision is shown in Figure 2.

Currently, large at grade car parks visually dominate the frontages of key streets through the Plenty Valley Town Centre which generally feature minimal landscaping and few pedestrian access paths. Extensive car parking comprising in excess of 2000 spaces currently exists around the Westfield Plenty Valley Shopping Centre. Other significant car parks (300+ spaces) currently exist to service Bunnings and homemaker/bulky good centres along McDonalds Road.

The Civic Precinct and Westfield Plenty Valley both have enforced time limits on their car parks to reduce the impact of commuter car parking on their operations. The commuter car park provided with the South Morang Train Station only has 450 spaces in a formal car park and an approximately additional 300 spaces in a temporary car park located north of McDonalds Road adjacent to the Train Station.

The existing core road network within Plenty Valley Town Centre has been designed as arterial and sub-arterial roads. These roads are not built to accommodate on street parking and have designated no standing zones. On street car parking is currently limited to connector and local streets including; Oleander, Wealthiland and Danaher Drives and parts of Civic Drive and Murdoch Road. The lack of on-street parking increases the amount of off-street car parking required and decreases the development potential for some land.



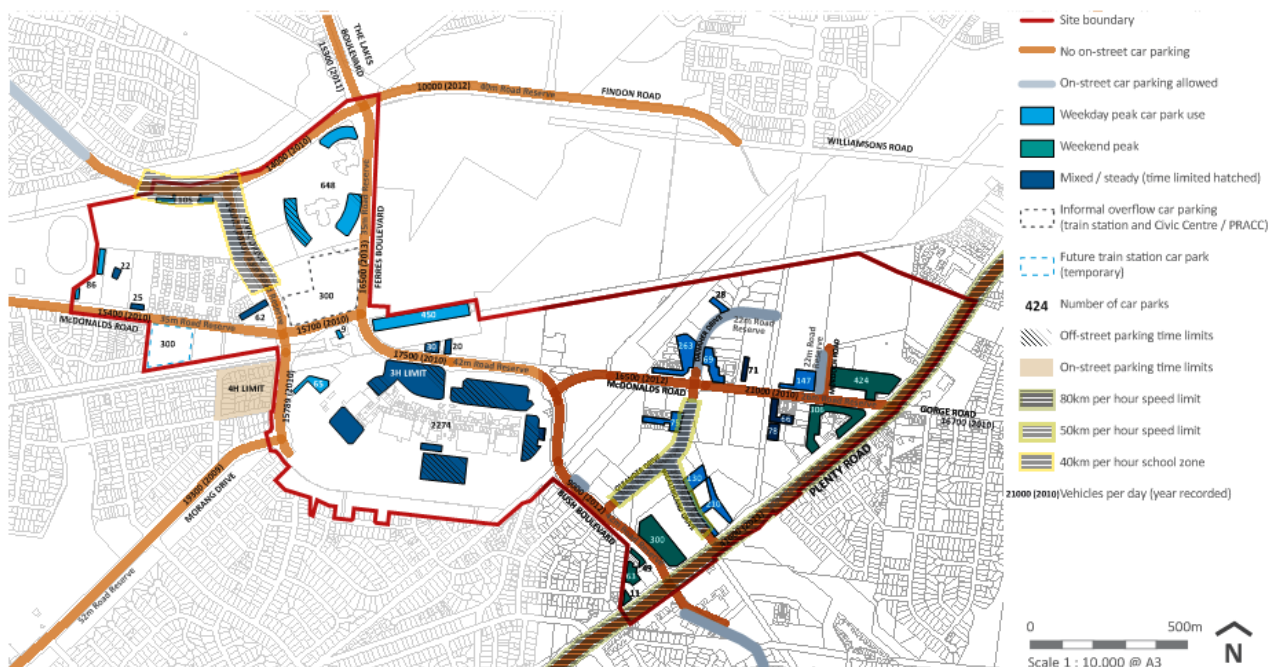


Figure 2 : Existing Parking Provision

Source: Plenty Valley Town Centre Structure Plan Background Report 2015



## 2.2 Parking Demand

### 2.2.1 Plenty Valley Town Centre Car Parking

As described above, the Plenty Valley Town Centre contains significant amount of car parking. None of the car parks within the Town Centre are used to capacity at all times. The car parking load is spread across the different uses and different times of the day, week and year.

Office and commuter car parks experience high demand on weekdays.

Retail car park demand is higher on the weekends, school holidays and close to calendar events including Christmas and Easter.

There are opportunities to make more efficient use of parking by having it utilised by different uses at different times of the day and week.

As part of the expansion of the Westfield Plenty Shopping Centre, 'a review of the car park occupancy levels across the site was undertaken by Cardno in April 2015. During the parking surveys indicates that the peak demand occurred at 1:00pm on the Saturday, when a total of 1,745 vehicles were recorded on the site. This equates to a parking demand for 3.56 spaces per 100 square metres for the centre.

On the Friday, the peak parking demand occurred at 12:00pm when 1,703 vehicles were observed on the site. This equates to a parking demand for 3.47 spaces per 100 square metres for the centre.

The above suggests approximately 3.5 spaces per 100 square metres of retail and supermarket space is required to meet peak demand.

### 2.2.2 Commuter Car Parking

There is significant demand for commuter car parking around the South Morang Train Station. Currently, 450 car parking spaces are provided in a sealed car park east of Ferres Boulevard and approximately an additional 300 spaces provided in a temporary car park located west of Ferres Boulevard. These parking areas are regularly filled during the morning commute.

The extension of the rail line to Mernda will likely ease the stress on the car parking in the short term. A number of strategies can be implemented to provide convenient access to the station and reduce the amount of strategically located land utilised for parking:

- Promote the use of new stations to be constructed as part of the Mernda Rail extension.
- Improve the pedestrian and cycle connections to the Train Station,
- Encourage greater patronage of buses which interchange at the Train Station,
- Advocate for the extension of the Route 86 Tram Route and for an interchange at the Train Station,
- Support the provision of multi-level parking with active uses at ground level.

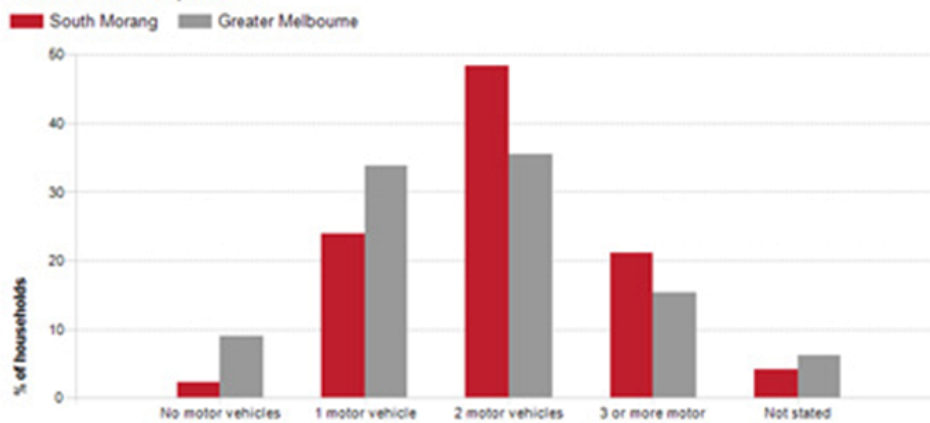
Ultimately, commuter parking is the responsibility of Public Transport Victoria and its Council's responsibility to provide a supporting role in respect to this issue.

### 2.2.3 Car Ownership

The level of car ownership per household provides an indication of private vehicle use and therefore the level of parking demand. Figure 3 shows that households in Plenty Valley are more likely to own two vehicles than Greater Melbourne. There are few households which do not own a car. This is reflective of the current vehicle dominated transport network and largely low density residential areas located some distance from public transport.

It is expected that car ownership will become more consistent with Greater Melbourne as alternative transport options improve and more housing options are provided closer to public transport similar to other more established parts of Melbourne. It would be expected that households located in the Town Centre with good access to public transport would generally only require one car.

Car ownership, 2011

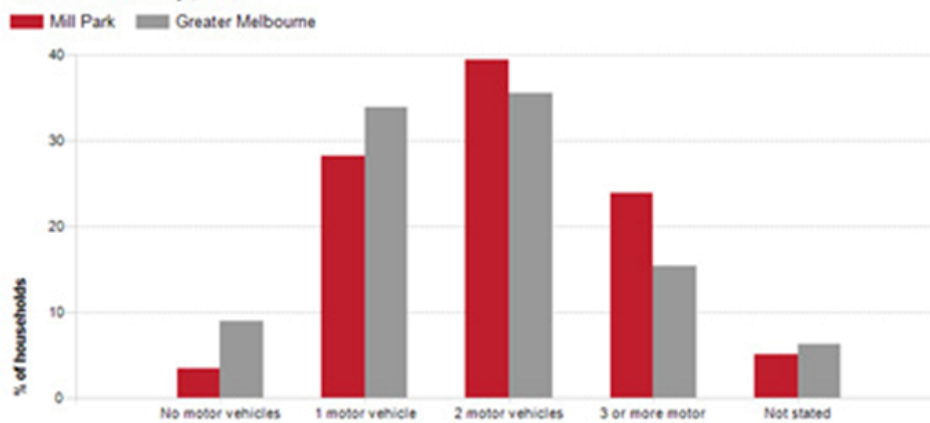


Number of cars

Source: Australian Bureau of Statistics, Census of Population and Housing, 2011 (Enumerated data)  
Compiled and presented in profile.id by .id, the population experts.

.id  
the population experts

Car ownership, 2011



Number of cars

Source: Australian Bureau of Statistics, Census of Population and Housing, 2011 (Enumerated data)  
Compiled and presented in profile.id by .id, the population experts.

.id  
the population experts

Figure 3 : Car Ownership in Plenty Valley  
Source: id. The Population Experts. 2016





For example, less than one third of households in proximity to the Reservoir and Preston activity centres areas have access to more than one vehicle.

The Structure Plan aims to provide the urban environment and infrastructure which supports households to function without multiple cars. This includes locating housing close to public transport and services and making the town centre more walkable.

Other initiatives such as Green Travel Plans (2.2.6) see can help people to live with less car dependency by guiding behaviour changes. The plan provides strategies and outlines practical steps to assist people moving into new developments to live in a more sustainable manner.

This in turn supports the Structure Plan's vision to create a more attractive, accessible and vibrant Town Centre which is less dominated by traffic and vehicles.

#### **2.2.4 Alternative Transport Options**

The Plenty Valley Town Centre is well serviced by public transport and sustainable transport modes. The South Morang Train Station is located centrally within the centre and links South Morang with the Melbourne CBD, Epping, Preston and the wider train network. The line is proposed to be extended in the near future to Mernda with future stations at Marymede, Hawkstowe and Mernda.

Ten bus routes currently service the Plenty Valley Town Centre which connect with the hinterland of residential areas and activity nodes further away. A bus interchange is located beside the South Morang Train Station with additional stops located throughout the centre. Recent new bus services in Plenty

Valley have improved frequency including Route 564 which runs every 10 minutes between South Morang and RMIT Bundoora.

A proposal exists to extend the Route 86 tram route from the current terminus at RMIT Bundoora along Plenty Road to the Plenty Valley Town Centre. The extension will likely run along Plenty Road, Bush Boulevard and McDonalds Road and head further north to The Lakes Boulevard, with an interchange at the South Morang Train Station. This would provide additional access and movement opportunities to and throughout the centre. Currently, the State Government is undertaking a feasibility study into the tram extension.

Plenty Valley is connected by pedestrian and cycle networks. Further, the Plenty Valley Town Centre Structure Plan identifies a number of improvements which will further enhance pedestrian and cycle connectivity and permeability throughout the centre.

Other alternative transport modes including carshare, electric vehicles and Uber are becoming more prominent and should be accommodated for in the Town Centre. Multiple transport options which exist now and will be enhanced into the future provide alternatives to the use of the car and decreases the need and demand for car parking. This can provide the opportunity for better utilisation of land currently used for parking. Reductions and restrictions in car parking also improves the attractiveness of using public transport which in turn provides the demand for improved services. It is anticipated that this trend will continue over time as the vision for the Town Centre is realised.



### 2.2.5 Green Travel Plan

A Green Travel Plan is a suite of onsite initiatives and offsite services to encourage residents and staff of large developments to use sustainable transport options.

The Travel Plan is a tool to assist in encouraging a mode of transport shift and reducing the demand or need for increased road capacity and parking supply.

A Travel Plan promotes greener, cleaner, and healthier travel choices with a specific emphasis on reducing single-occupancy car journeys plus encouraging active modes of transport such as walking and cycling.

Depending on the development type, a Green Travel Plan may include:

- parking facilities for bicycles, motor bikes, small cars, electric cars and onsite and nearby car share systems,
- end of trip facilities for staff, including the location of showers and the availability of personal lockers,
- bicycle and walking maps,
- nearby public transport stops,
- timetables for public transport services,
- availability of free or substituted public transport tickets through the employer or relevant Owners Corporation,
- an organisation's car-pooling scheme.

### 3. Parking Precinct Policy

The Parking Precinct Policy provides a response to specific issues relating to parking provision and development in the Plenty Valley Town Centre. It is proposed that the policy will be implemented through statutory controls including the Parking Overlay.

The policy is supported by further actions to address broader parking issues in the Town Centre

#### On-Site Parking Provision Rates

1. The Parking Provision Rates set-out in Table 1 below are to be applied in Plenty Valley via the Schedule to Clause 52.06-6. These rates are most suitable for a Town Centre environment.

Use	Rate	Measure
Dwelling	1	Space for each dwelling

For all other uses listed in Table 1 of Clause 52.06-5, the number of car parking spaces required for a Use shall be calculated by using the Rate in Column B of that Table.

2. Council will undertake an assessment of the appropriateness of allowing fewer spaces in accordance with 52.06-6. In assessing an application against 52.06-6, Council will consider applications to reduce parking in the Town Centre where it is demonstrated that alternative and sustainable transport option including public transport, cycling and walking will reduce the demand for car parking.
3. Developments will be encouraged to provide parking spaces for Carshare services.
4. Residential developments should make provision for electric vehicles charging in parking on their site.

#### Commuter Parking

5. Support the provision of sufficient commuter parking by Public Transport Victoria in proximity to the South Morang Station particularly in multi-level parking facilities with active ground floor uses.
6. Support the extension of the Route 86 tram route with a transport interchange at the South Morang Train Station.

#### Parking Design

7. Parking areas should be located in basement, screened undercroft and / or multi-level parking arrangements.
8. Developments are encouraged to use flexible design initiatives that enable parking areas to be used on a temporary basis for alternative uses such as community gatherings or markets.
9. Where parking is provide at grade, it should be sleeved by built form, high quality landscaping and provide trees and landscaping at the rate of one space for every eight spaces.
10. Support provision of at-grade parking beneath electricity transmission lines and within utility easements.
11. Multi-level parking should provide for alternative uses at street level. Developments should consider flexible design initiatives that enable multi-level parking areas to be transformed into office or residential space at a later time.



## **Green Travel Plans**

12. Council will require developments comprising 5,000 square metres or more of commercial or industrial floorspace and / or 30 or more dwellings to submit a Green Travel Plan. The Green Travel Plan is to demonstrate design and behaviour-change initiatives that are to be implemented over the life of the development that will assist to achieve the sustainable transport objectives for the Plenty Valley Town Centre.

## **Parking Use and Redevelopment**

13. Council will support the redevelopment of at-grade car parking into multi storey mixed use development including parking.



## 4. Implementation and Further Actions

As part of implementing this Parking Precinct Plan, Council will:

1. Apply a Parking Overlay (PO) to the Plenty Valley Town Centre in the Whittlesea Planning Scheme which implements the Parking Precinct Policy. A draft schedule is included in Appendix 1.
2. Monitor the supply and demand of car-parking and the patronage of public transport in the Town Centre.
3. Monitor and take appropriate action where necessary in respect to illegally parked vehicles, particularly where they undermine the intent of parking management tools.
4. Prepare a Parking Strategy for the municipality with specific strategies and actions for activity centres and around train stations. Review and update the Parking Overlay and other relevant controls in the Whittlesea Planning Scheme if necessary.
5. As part of the preparation of the Parking Strategy, consider parking management tools to including; pricing, permits, restrictions, physical changes to best manage parking within an activity centre environment.
6. Advocate for Public Transport Victoria to develop at-grade car parking near the South Morang Train Station into multi storey mixed use development including multi-level parking.
7. Advocate for the extension of the Route 86 tram route with a tram stop at the South Morang Train Station to provide greater transport alternatives to travel to and from the South Morang Train Station.

## Plenty Valley Town Centre Parking Precinct Plan 2017

**Appendix A: Parking Overlay Schedule****SCHEDULE 2 TO THE PARKING OVERLAY**

Shown on the planning scheme map as **PO2**

**1.0 Parking objectives to be achieved**

Provide for sufficient car parking as part of new development.

To encourage a mode shift towards the use of active and sustainable travel modes.

To improve the visual and pedestrian amenity within Plenty Valley Town Centre through the careful design and placement of car parking.

**2.0 Number of car parking spaces required**

The required number of car parking spaces is shown in Table 1. The requirement for a use listed in the table is the product of the *Rate* and the *Measure*.

**Table 1: Car parking spaces**

Use	Rate	Measure
Dwelling	1	Space for each dwelling

[For all other uses listed in Table 1 of Clause 52.06-5, the Rate in Column B and Measure in Column C of Table 1 in Clause 52.06-5 applies.](#)

**3.0 Application requirements**

Before a new use commences or any buildings or works associated with that use or an existing use is constructed, plans must be prepared to the satisfaction of the responsible authority. In addition to the application requirements set out in Clause 45.09-8, the plans must show:

- Pedestrian access ways through parking areas.
- The potential location of electric vehicle charging points.
- Details of how the parking spaces will be allocated to individual dwellings or tenancies and whether the parking spaces will be separate lots with their own Certificate of Title.

This information may be included in other plans submitted with an application.

A Green Travel Plan must be prepared for all applications use and / or for development comprising:

- 5,000 square metres or more of commercial or industrial floorspace, and / or
- 30 or more dwellings,

that includes design initiatives and actions to encourage the use of more sustainable transport options in the Plenty Valley Town Centre consistent with the objectives of this overlay.

The responsible authority may require a Green Travel Plan to be provided for smaller developments, depending on the location of the land, the proposed use and / or the proposed provision of parking.

**4.0 Design standards for car parking**



Plenty Valley Town Centre Parking Precinct Plan 2017

In addition to the design standards at Clause 52.06-8, the design of car parking spaces should meet the following:

- Parking areas should be located in basement, screened undercroft and / or multi-level parking arrangements.
- Flexible design initiatives should be provided to enable parking areas to be used on a temporary basis for alternative uses such as community gatherings or markets.
- Ensure the following design outcomes for at-grade car parks where basement, undercroft or multi-level parking is not feasible:
  - Locate car parking to the rear of developments and avoid the use of street frontages for car-parking, where possible;
  - Sleeve at-grade car parks with built form or screen them with extensive, high quality landscape treatments that reduce the visual dominance of the car park while contributing to the built form quality and active surveillance opportunities; and
  - Provide trees and landscaping at the rate of one space for every eight spaces in surface car parks, with engineered soils where required to ensure proper tree growth.
- Ensure the following design outcomes for car parking in multi-level developments:
  - Provide for alternative uses at street level which activates the street and creates visual interest; and
  - Flexible and adaptable design initiatives such as appropriate floor to ceiling heights should be provided that enable multi-level parking areas to be redeveloped into office or residential space at a later time if the circumstances are appropriate.

**5.0 Decision guidelines**

Before deciding on an application to reduce the number of car parking spaces required for a specified use, in addition to the application requirements and decision guidelines at Clause 52.06-09 and Clause 52.06-09, the Responsible Authority must consider as appropriate:

- The likely effectiveness of the proposed Green Travel Plan, if applicable.

**6.0 Reference document**

*Plenty Valley Town Centre Parking Precinct Plan 2017*  
*Plenty Valley Town Centre Structure Plan 2017*

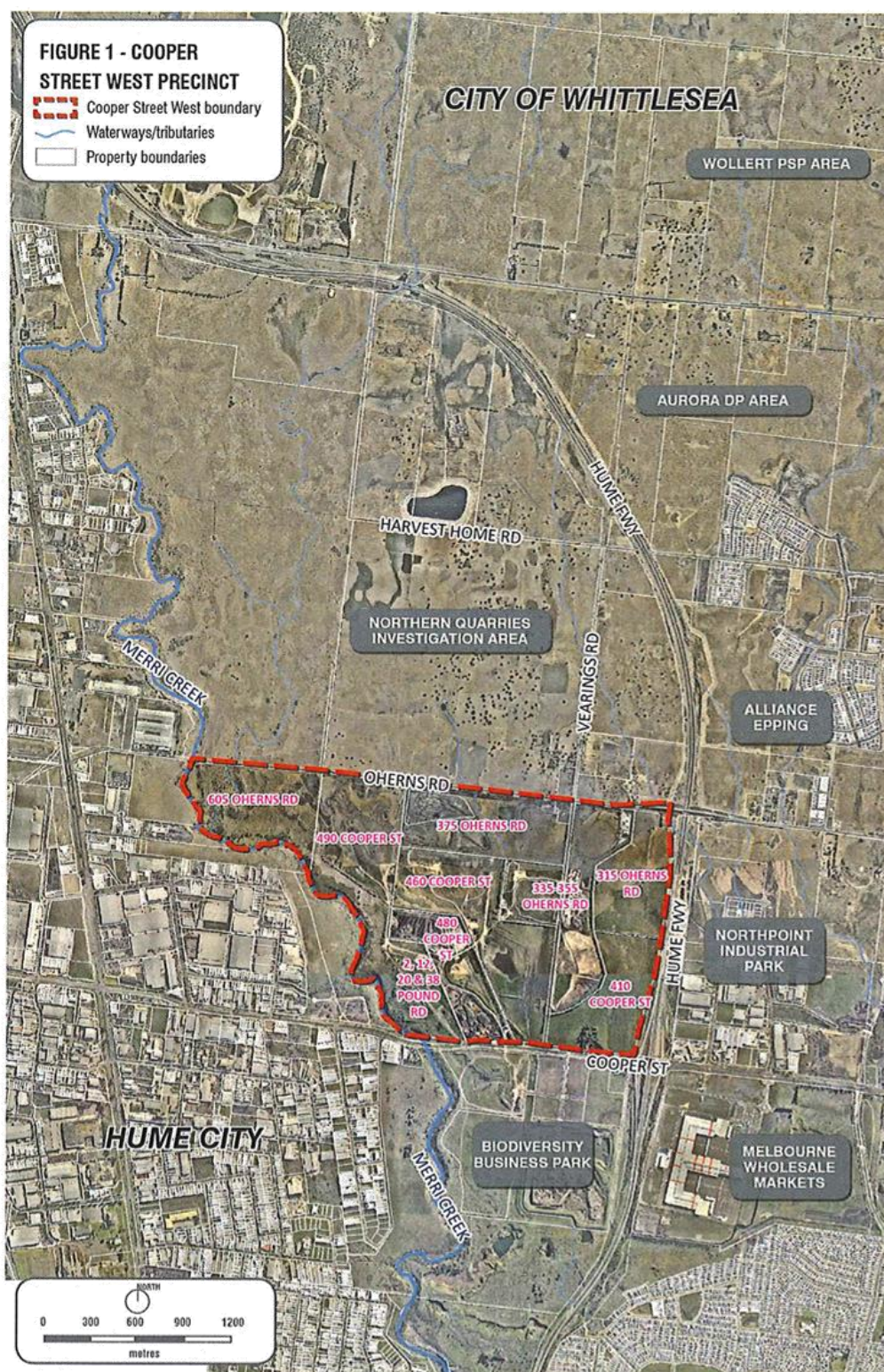


Locality Plan - Subject sites are outlined in blue





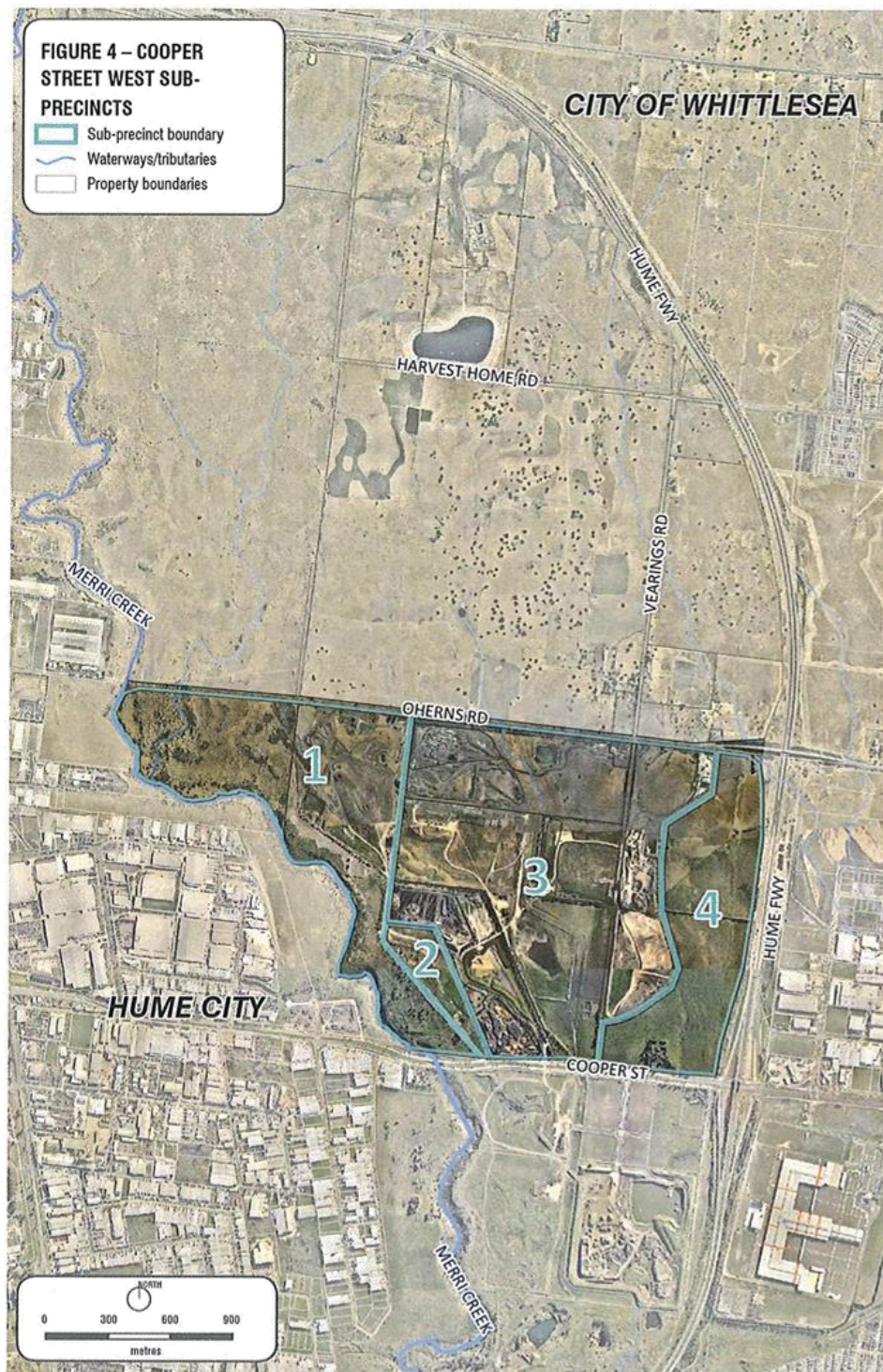
Cooper Street West Precinct – extract from the draft Cooper Street West Position Paper







Cooper Street West Sub Precinct Plan – extract from the draft Cooper Street West Position Paper

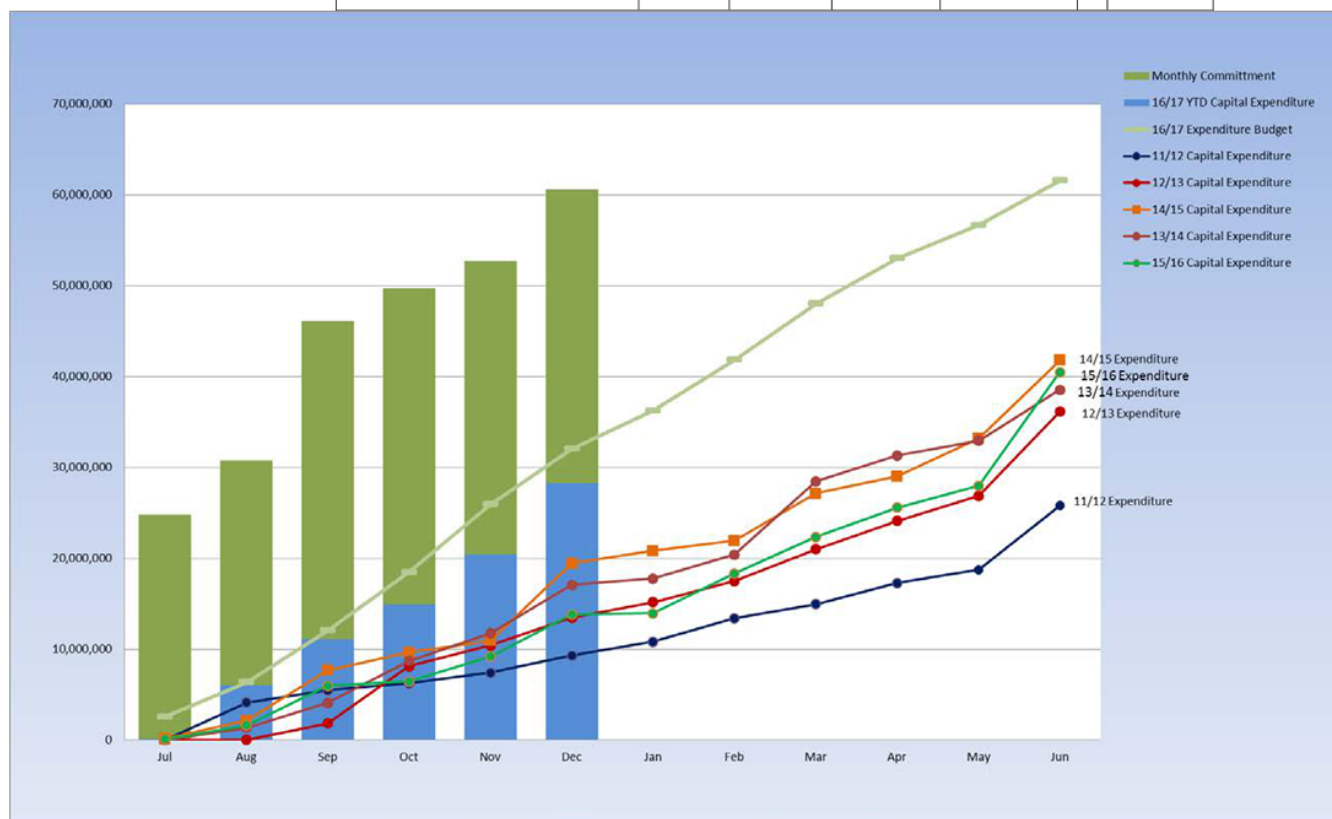






NEW WORKS PROGRAM - FINANCIAL PERFORMANCE - December 2016



















New Works	Year to Date \$'000s				Full Year Budget \$'000s
	Budget	Actuals	Variance	Commitments	
Total New Works Program	32,035	25,638	6,396	32,092	61,561






















The chart above provides a financial summary on the status of the New Works Program as at the end of December 2016. The value of work completed to the end of December is \$25,638,242 (blue column) with contracts and purchase orders committing another \$32,092,801 (green column). The green line indicates the accumulative budget for 2016/17 (The lines below show previous years' actual expenditure). This indicates that in 2016/17 the New Works Program has expended \$25.638m (41.65%) of the total Program budget to December 2016.












































Quarterly New Works Program Status Report for Council: <u>December 2016</u>							PHYSICAL PROGRESS LEGEND		
							Traffic Light Definitions		
							Favorable		Completed
							Favorable		Ahead Of schedule
Favorable		On Track							
Neutral		Not Yet Commenced							
Neutral		Behind Schedule							
Unfavorable		Project Not Proceeding							
Unfavorable		On-Hold							
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments		
BUILDINGS									
175. Disability Action Plan - Ongoing Program	1,677	0	100,000	26-50%	On Track		Construction works at Epping Depot in progress. Expected completion January 2017.		
177. Office Refurbishment and Alterations - Various Locations	130,779	135,000	380,000	0-25%	On Track		Works have been carried out in preparation for the main relocation of the West Wing once the first Stage of the CC HVAC project is completed.		
240. Animal Welfare Facility	6,233,072	5,400,000	5,499,216	76-100%	On Track		Shared Services Agreement executed with Moreland City Council and the City of Darebin for a Regional Animal Facility. Contract for construction awarded in December 2015 for the design and construction of the facility to replace an outdated facility. Construction was substantially completed in December 2016 including internal fit-out and finishes. Practical completion of works is expected in January 2017 with commissioning scheduled for February to March 2017. Handover planned for April 2017.		
484. Refurbish building - Wollert Hall	0	16,000	50,000	0-25%	On Track		Design Consultancy RFT drafted, Design Tender and detailed tender documentation for construction to follow second half of FY16/17. Construction completion planned for end FY17/18.		
671. Ashley Park Community Activity Centre - (Mernda Precinct 2B)	1,182,790	991,919	1,716,226	0-25%	Ahead Of Schedule		Ground works are continuing to expected program. Slab pour has now commenced and expected to be completed end November. Structural works will commence in early January. Program is on track for completion in July 2017.		
672. Mernda South (Hawkstowe Pde) ELC (Mernda Precinct 5)- Mernda South PPP	686,762	600,000	600,000	76-100%	Ahead Of Schedule		Construction work at Mernda South ELC is now complete with all documentation required for Practical Completion issued and Occupancy Permit for the new building received on 22/11 (three weeks ahead of program). The PWG is now working with the YMCA regarding arrangements for Council MCH services to move in and commence in March 2017.		
1010. Barry Road CAC - extension and refurbishment	1,390,278	1,767,000	3,352,874	51-75%	On Track		The building works are progressing well and are due for completion at the end of March 2017 with handover in April 2017. The external works tender was awarded in December 2016 and is due to commence in February 2017.		
1207. Galada (Aurora) Community Activity Centre (Harvest HomeTown Centre)	5,060	60,000	120,000	0-25%	Not Yet Commenced		Kitchen upgrade drawings and specifications completed. Request for Quotation documentation being prepared to send out to building contractors for quotations. Anticipate quotation award March 2017. Commencement of building works April 2017 which is also dependent on Barry Road CAC upgrade completion.		
1271. Upgrade pavilion - RGC Cook Reserve	1,282,014	810,000	1,374,225	76-100%	On Track		The pavilion is approximately 85% complete and due for completion March 2017. Internal stakeholders have been advised that once occupancy has been achieved for the new building upgrade works will commence for the existing building enabling the club to operate without any impact to the club.		
1311. Woodland Waters Recreation Reserve Sports Pavilion - Mernda (Precinct 4)	0	31,139	31,139	76-100%	Completed		Project Complete.		
1324. Construct pavilion and car park - Harvest Home Road soccer pitches	9,951	77,000	85,000	0-25%	On Track		Town Planning Permit Application referral queries addressed. Resident consultation and public advertising of the application completed. Application in final stages of assessment. Permit expected early in the New Year. Finalisation of construction documents to follow, along with Building Permit application.		



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	Neutral		Behind Schedule						
	Unfavorable		Project Not Proceeding						
	Unfavorable		On-Hold						
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments		
1382. Mernda Recreation and Leisure Centre Construction	0	10,000	110,000	0-25%	On Track		The tender analysis of the consultant submissions for the Feasibility Study and Business Case preparation for Mernda Recreation and Leisure Centre is currently in progress.		
1407. Jindi Family & Community Centre (Renaissance Rise)	0	20,000	20,000	0-25%	On Track		Facility is outside Defects Liability Period. No major operational issues to report. <input type="checkbox"/> Project participation recognition artwork (hand prints of children who participated in design development), as originally planned for the Social Gathering Space on the street side of facility. These works part of original project funding.		
1589. Redevelop Mill Park Leisure and Service Centre	207,344	335,000	1,041,158	0-25%	On-Hold		The schematic design was submitted to the Council Forum on the 29th November for feedback and the Council Meeting on the 13th December. The report was deferred at the Council meeting with workshops to be undertaken in January 2017 to provide further information to Council and look at the ramp and accessibility requirements.		
1590. Epping Depot upgrade and refurbishment	0	288,000	876,176	0-25%	On Track		Project is scheduled to complete by the end of the financial year.		
1616. Meadowglen Athletics Stadium - additional toilet facility	971	28,056	28,056	76-100%	On Track		Project complete and currently in Defects Liability Period.		
1701. Conversion of Janefield School Building into Community Facility - University Hill	12,213	12,214	392,486	0-25%	On Track		Contract established for detailed design architectural services, this work will start early in the new year. The project was also successful for a \$310,000 grant under the Growing Suburbs Fund.		
1849. Civic Precinct - HVAC and Refurbishment (Sustainable Climate Control Project Stage 1)	1,637,379	2,415,000	5,622,246	0-25%	On Track		Works have commenced for Stage 1 and are due for completion in April 2017. Stage 2 will be commence following staff movements and is scheduled for completion by end of 2017 whilst Stage 3 will be scheduled for completion mid 2018. <input type="checkbox"/>		
1899. Mill Park Lakes Reserve - Pavilion Expansion and Upgrade	891,803	933,926	933,926	76-100%	Completed		All works have been completed and Occupancy Permit received. <input type="checkbox"/> Clubs have started using the new extension area. <input type="checkbox"/> Building currently in defects liability period.		
1914. Energy Efficiency Program - Various	113,380	140,000	292,469	0-25%	On Track		Prepared for solar PV install on seven community centres, works to start next year. Lighting upgrades continued. New energy efficiency technologies explored for potential trial in 2017.		
1915. Refurbish building - Whittlesea Aquatic Facility	0	3,000	30,000	0-25%	Behind Schedule		Design Consultancy RFT drafted, Design Tender and detailed tender documentation for construction to follow to end 2017. Construction planned for the 2018 facility close period, between 1 April to 31 October 2018.		
1917. Korin Korin Children & Family Centre (Champions Pde), Epping Nth	20,259	115,000	115,000	76-100%	On Track		Minor new works (swing set, shade structure, plant & security fencing, signage, blinds) to be completed. Still under defects liability period. <input type="checkbox"/> Facility managers (Child Protection Society) have arranged (and funded): <input type="checkbox"/> - new telephone and ADSL lines, and <input type="checkbox"/> - new access control gate and fence enclosure outside to better control children's egress and main entry.		
1919. Leased Building/Property Renewal Program - Various	10,400	15,000	100,000	0-25%	On Track		Project Working Group continuing to develop a new program of works for implementation using the new building condition data collected by the consultant's completed audit work. Renewal program to be finalised in January with a view to works commencing in February as forecast.		





















Quarterly New Works Program Status Report for Council: <u>December 2016</u>							<b>PHYSICAL PROGRESS LEGEND</b> <b>Traffic Light Definitions</b> Favorable  Completed Favorable  Ahead Of schedule Favorable  On Track Neutral  Not Yet Commenced Neutral  Behind Schedule Unfavorable  Project Not Proceeding Unfavorable  On-Hold
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments
1920. Implementation of Signage Management Plan - Various	2,940	5,000	100,000	0-25%	On Track		The Signage Management Plan is currently being revised in line with feedback received at Council Forum in April 2016. As agreed in April, the revised draft will be re-presented to Council Forum early in 2017. It is anticipated that the rollout of new and replacement Council signage will commence in April/May 2017.
1923. Planned Renewal Projects (incl. Minor New Works)	33,160	60,000	200,000	26-50%	On Track		Various projects at Tender and Construction phase: Epping Memorial Hall - Construction Lalor Library Community Room - Design Spring St Hall - Complete French St Hall - Pre Construction Barry Rd Preschool - Tender Whittlesea Child Care - Tender Preschool playroom sinks - Tender Lalor Living and Learning Centre - Tender Laurimar pavilion - Design
2053. Sycamore BMX Facility Improvement - Sycamore Reserve, Mill Park	0	0	200,000	0-25%	On-Hold		Design issues have been encountered due to the original scope of project being changed. This has required the original design to be abandoned and commencement of a new design. Feedback based on the pavilion functional brief is required to proceed to design.
2056. Epping Services Hub - Upgrade of 713 High Street Epping	68,330	7,000	225,542	0-25%	On Track		Received only one tender submission and the tender price is significantly more than the estimated cost, the project will be retendered in early 2017.
2057. Peter Lalor Secondary College Redevelopment	2,760	0	200,000	0-25%	Behind Schedule		Design consultants appointed. Work on concept designs commenced and scheduled for delivery early in 2017. External grant applications have progressed to the second level of consideration.
2060. Refurbish Harbard Street Kindergarten	10,650	30,000	30,000	76-100%	Completed		Project is completed.
2109. 3 Phase Power (Design) – Civic Centre	302	0	70,000	26-50%	On Track		Construction Contract Awarded. Works scheduled to begin February 2017.
2123. Thomastown Library Refurbishment	117,261	133,270	133,270	76-100%	On Track		In defects liability period
2150. New external access door to Huskisson Reserve Pavilion Toilet	0	0	15,000	0-25%	On Track		Concept Design stage
2151. Ziebell's Farmhouse - Fire Detection Alarms	0	0	50,000	0-25%	Behind Schedule		Project being scoped.
2158. Epping Recreation Reserve – Tennis club room upgrade	0	0	30,000	0-25%	Behind Schedule		Project being scoped.
2162. Extension of verandah - Epping Memorial Hall	5,000	5,000	5,000	76-100%	Completed		Project complete.
2164. Portable buildings for female change rooms - Sycamore Reserve	0	20,000	50,000	0-25%	Not Yet Commenced		Options analysis being undertaken in preparation for modular buildings. Once a recommendation has been provided external stakeholder consultation will be organised. Anticipate option recommendation January 2017. Timeline is dependant on modular option.
2165. Cool room - Mill Park Lakes Reserve Pavilion	29,780	30,000	30,000	76-100%	Completed		Cool room has been installed as per the requirements. In defects liability period.
<b>Total BUILDINGS</b>	<b>14,086,315</b>	<b>14,493,524</b>	<b>24,209,009</b>				
<b>DRAINS</b>							

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Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments	
1064. Drainage improvement works	80,500	50,000	50,000	76-100%	On Track		Drainage systems at Bassetts Road / Cookes Road intersection, Doreen and 105 Janefield Drive, Bundoora completed. New project is to upgrade drainage at 56 Coulstock St, Epping. Approx cost is \$30k. Memo to request additional funding approved by the Projects Board in Sept 2016. Works completed prior to Christmas 2016 however not invoiced.	
1802. Hillcroft Estate Drainage Issues	0	47,500	47,500	0-25%	Not Yet Commenced		Agreement with the property owner has been delayed and is pending negotiations.	
1884. Queenscliff Road and Michigan Terrace intersection drainage issue	3,400	80,000	1,200,000	0-25%	Not Yet Commenced		Tender evaluation is in progress as per the original program for the project.	
1904. Construct end of pipe treatments - Darebin Creek	0	10,000	50,000	0-25%	On Track		Due diligence confirmed that mandatory Cultural Heritage Management(CHMP) is required. Tender for CHMP is in progress	
2147. Depot Fuel Facility Environmental Improvements	0	0	56,000	0-25%	On Track			
<b>Total DRAINS</b>	<b>83,900</b>	<b>187,500</b>	<b>1,403,500</b>					
<b>FEASIBILITY &amp; PLANNING</b>								
1847. Planning and Feasibility Studies for Future Projects	27,245	140,000	300,000	0-25%	On Track		The project planning undertaken under this budget allocation allows for the conduct of due diligence investigations to ensure the project is in a better position for delivery when budget allocations are made in the forward New Works Program. Identified works are proceeding in accordance with the endorsed Program timelines. Revised Program included in Q2 NWP Report to Council.	
<b>Total FEASIBILITY &amp; PLANNING</b>	<b>27,245</b>	<b>140,000</b>	<b>300,000</b>					
<b>OPEN SPACE</b>								
2. Upgrade open space - Implement Sportsfield Strategy	991,150	1,285,000	1,285,000	76-100%	Completed		Synthetic sportsfield completed, now under maintenance. Final invoices to be received.	
118. Renewal of playgrounds and general landscape improvements	389,786	800,000	1,606,251	0-25%	On Track		Butcherbird Park is complete and currently on contractor maintenance. Main Street Rec Reserve, Thomastown, Victoria Dr Park, Thomastown, Wenden Drive Park, Mill Park and Kalara Close Park, Lalor are all awarded to contractors and are scheduled to commence construction in Jan 2017.	
252. Street tree renewal - Ongoing Program	14,380	0	300,000	0-25%	On Track		2 extra streets added to 2017 planting program & consultation underway for those 2 streets, tree stock ordered for the entire project. Contract awarded for tree removal which will begin Feb 2017.	
405. Develop landscape - Whittlesea Gardens	1,765	0	50,000	0-25%	On Track		Landscape architectural consultancy appointed. Preliminary assessment, site investigation underway and consultation program is aiming to be undertaken during February 2017.	
408. Develop landscape - Quarry Hills Park	5,648	0	160,000	0-25%	On Track		Cultural Heritage Management Plan tenders closed and being evaluated, Flora and Fauna tenders awarded, RFQ for Survey and Geotechnical investigations underway.	
























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Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments
1717. Establishment of additional dog off leash areas - Various Locations	12,422	0	55,000	76-100%	Behind Schedule	●	New drinking fountain installed at Whittlesea dog off leash area, with additional landscape works to be undertaken at this location as well as Creek Park (Mernda) and Fitzwilliam Park (Doreen) around March 2017. A new unfenced dog off leash area is also planned for Hill Top Park (Doreen) however this location is being reviewed following resident objections during consultation.
1834. Cricket Wickets Upgrade (various locations)	37,742	45,000	55,000	76-100%	On Track	●	Works complete
1938. Woodland Waters Sportsfields Stormwater Harvesting Project	3,040	0	23,270	0-25%	Project Not Proceeding	●	Project not proceeding. Balance of unspent funds from Melb Water to be returned, awaiting invoice from Melb Water.
1939. Playground McMillan Run	5,869	13,166	13,166	76-100%	Completed	●	Works Completed, in defects liability period. DLP expires on 25/07/2017
2052. Sycamore BMX Track Development	0	50,000	300,000	0-25%	On-Hold	●	Project scope is currently revised based on service need determination. A revision of project schedule and budget will be performed.
2054. Vehicle Exclusion Fencing	38,235	50,000	50,000	76-100%	Completed	●	Vehicle exclusion fencing works at Foothills Park Stage 1 and 3 completed. Practical completion was granted on 19/12/2016
2067. Renewal of track - Meadowglen Athletics Reserve	622,046	525,000	1,003,470	51-75%	On Track	●	Works commenced 18th Oct & on schedule and budget. □ Due to be completed late Jan./early Feb 2017
2068. Doreen Recreation Reserve Master Plan	1,765	0	50,000	0-25%	On Track	●	Next Stage of Project - Detail Design to commence 3rd quarter of 2016/17 (JAN)
2115. Sports Lighting Upgrades - Various Sites	15,026	10,000	220,000	0-25%	On Track	●	Sports lighting infrastructure upgrade at Main Street Recreation Reserve, Thomastown □ Detailed design of new lighting infrastructure ongoing. Electrical engineer is having issues finding source of power to the pavilion. Design anticipated for completion towards the end of February, construction tender to be issued immediately after.
2119. Street Light bulk replacement program - various locations	0	20,000	300,000	0-25%	On Track	●	Preparing to install approx 200x 37W LED lights, and assessing feasibility of replacing higher output 150W sodium lights on main roads with LEDs.
2125. Huskisson Recreation Reserve Master Plan	1,765	15,000	50,000	0-25%	On Track	●	Master Plan design contract awarded, public consultation phase planned for Feb / March 2017. Site survey and investigations underway.
2145. Norris Bank Reserve Master Plan Development (as part of the Open Space Strategy Implementation)	5,840	20,000	50,000	0-25%	On Track	●	Master plan design services contract (including community consultation) awarded and community consultation completed on site. Currently waiting on report of consultation findings before master plan process continues
2146. A.F. Walker Reserve Sportsfield Lighting	35,737	0	200,000	0-25%	On Track	●	Construction contract awarded. Works to begin on site early 2017. Currently working with the Cricket and Football clubs to get sign off on proposed tower locations to ensure they are constructed outside the fields of play.
2148. Half Court Basketball Court in University Hill	2,648	75,000	75,000	26-50%	On Track	●	Liaison with Nillumbik Council ongoing, final designs approved and construction pending.
2149. Scoreboard for Main Street Reserve Thomastown	0	0	35,000	0-25%	On Track	●	Concept design stage.
2152. Playground Upgrade - Botanica Park	10,943	20,000	310,000	0-25%	Behind Schedule	●	Design services contract (including consultation) has been awarded and community consultation has occurred on site. Currently waiting on report from consultation process before design proceeds any further. Construction will not be completed this financial year.

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							Neutral	Behind Schedule
							Unfavorable	Project Not Proceeding
							Unfavorable	On-Hold
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments	
2153. Waterview Reserve – Additional Shelter	0	50,000	50,000	0-25%	Not Yet Commenced		Original architect appointment to commence design of all weather shelters has withdrawn from project. A new architect has been appointed. Initial site visit with internal stakeholder and Architect to discuss expectations. Design services will consist of concept which is due January 2017. Building Design and Construct contract will commence in February 2017. Anticipate completion by end of June 2017.	
2154. Sycamore Reserve – Temporary lighting	0	15,000	30,000	0-25%	On Track		Lighting feasibility and risk report pending with Project Board	
2155. Butcher Bird Reserve Playground	50,000	35,000	50,000	76-100%	On Track		Project complete, on maintenance	
2156. Norris Bank Reserve - Hot Shot Courts	0	20,000	90,000	0-25%	Not Yet Commenced		Scoping of project with Club ongoing.	
2157. West Lalor Tennis Club (Huskisson) – New Sports Lighting	0	10,000	85,000	0-25%	Ahead Of Schedule		Detailed design and supply investigations underway	
2161. Mesh fencing around the Cricket ground - Leslie Street Reserve	0	0	30,000	0-25%	On Track		RFQ being sourced with completion of project due by March 2017.	
2163. Ball catching fencing - HR Uren Recreation Reserve	0	0	10,000	0-25%	Behind Schedule		Design only in 2016/17, with construction in 2017/18.	
Total OPEN SPACE	3,273,960	5,296,772	11,464,034					
PLANT & EQUIPMENT								
104. Replacement of Council fleet - Ongoing Program	1,579,788	850,000	2,338,000	51-75%	On Track		Project on track for 2016/17 financial year.	
106. Furniture and equipment purchases	23,714	30,000	60,000	26-50%	On Track		Purchases made to provide furniture for OH&S requirements. Supply of new furniture to replace existing due to wear and tear.	
225. IT - Various computer system hardware and other IT equipment	7,827	30,000	85,000	0-25%	On Track		A number of older mobile phones are being upgraded as the mobile network no longer supports 2G. Awaiting order to be filled from supplier.	
241. Acquisition of Visual Art - Civic Centre	1,150	0	10,000	26-50%	On Track		3 artworks were acquired for the Cultural Collection. It is expected that more artwork will be acquired in the next 6 months with the input of the Acquisitions Advisory Group.	
369. Furniture and fittings for halls and CACs - Various Locations	22,692	40,000	65,000	51-75%	On Track		Furniture has been purchased for May Road Seniors Centre and French Street Hall.	
419. Management of the Public Art Collection	10,818	11,181	50,181	26-50%	On Track		Projects is progressing well according to timelines. Tenders will be assessed in mid Jan and designs developed by two shortlisted applicants. More site investigations were carried out. Community consultation was held with 6 community groups. On track.	
1281. IT - Councillor IT Equipment Replacement	16,544	50,000	55,000	0-25%	On Track		IT requirements for new Council have been purchased. Awaiting invoices.	
1874. IT - ICT Infrastructure Asset Program	34,128	40,000	115,000	0-25%	On Track		New core switch has arrived but has not yet been invoiced. Works are progressing.	
1877. IT - Mobility	28,664	71,000	129,794	0-25%	On Track		Mobility hardware installation ontrack for delivery in 2016/17.	
1881. IT - Meeting Room IT Improvements	0	5,000	10,000	0-25%	On Track		Trials for new hardware and software in meeting rooms are currently underway.	
1943. IT - Aged Care Rostering	9,584	60,000	134,510	0-25%	On Track		Project on track for software and hardware completion by June 2017.	






















Quarterly New Works Program Status Report for Council: <u>December 2016</u>							<b>PHYSICAL PROGRESS LEGEND</b> <b>Traffic Light Definitions</b> Favorable  Completed Favorable  Ahead Of schedule Favorable  On Track Neutral  Not Yet Commenced Neutral  Behind Schedule Unfavorable  Project Not Proceeding Unfavorable  On-Hold
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments
2141. EPMO System Implementation	92,310	0	150,000	51-75%	On Track		Survey Monkey circulated to determine a new name, with "iProject" being the most popular choice. Solution is being developed based on the approved Requirements Design Document from Revolution IT. The Requirements Design Document was based on the seven workshops held with key project stakeholders. UAT scheduled for 16/01/2017 and will run for two weeks. Staged roll-out planned, as documented in the Business Deployment Plan that has been drafted for the project.
<b>Total PLANT &amp; EQUIPMENT</b>	<b>1,900,275</b>	<b>1,443,181</b>	<b>4,242,702</b>				
<b>ROADS &amp; PATHS</b>							
76. Local road resurfacing works - Ongoing Program	1,800,000	730,000	2,000,000	76-100%	Ahead Of Schedule		Total budget committed. Currently on track to complete works by the end of Jan 2017 (weather permitting and contractors availability).
77. Local road reconstruction/rehabilitation	1,333,887	2,000,000	2,000,000	76-100%	Ahead Of Schedule		Road rehabilitation works commenced in Lalor and Thomastown. Budget committed. Anticipated completion by January 2017.
361. Car Park rehabilitation - various	0	60,000	60,000	0-25%	On Track		Lalor Football Club carpark has been selected. Concept design completed by Major Projects. Project underway for delivery in third quarter of the financial year.
450. Scott Street Beautification works	3,932	300,000	300,000	0-25%	Behind Schedule		The construction contract has been awarded to Metro Asphalt and due to commence on site early in the new year. Awarding delayed due to Council election. Construction is scheduled for completion in April 2017.
813. Road Safety Improvements - Install roundabout at Hurlstone Crescent	0	0	150,000	0-25%	On-Hold		Project on hold until PIDs 1986 and 1987 are complete, and a new report is sent to Council.
1113. Bicycle facilities - provide new on-road & off-road paths	5,267	94,000	114,000	0-25%	On Track		Design s being finalised. 4 small projects have been completed and are awaiting invoicing
1258. Upgrade disabled parking bays to DDA requirements - Various locations - DDA Works	1,631	17,500	35,000	51-75%	On Track		5 of 6 sites completed using Annual Supply Contractors, with the balance to be constructed in January 2017. Invoicing anticipated in February 2017.
1265. Kerb Ramp DDA Upgrades	47,460	40,000	40,000	76-100%	On Track		2016/17 Program of Works for kerb ramp upgrades completed, with invoices paid.
1592. Part constructed roads improvements	1,754	0	100,000	0-25%	On Track		Tree removal planning permits submitted and awaiting approval. Civil designs completed, and to be constructed using Annual Supply Contractors in March 2017.
1641. Church Street Shopping Strip Redevelopment	34,614	37,640	37,640	76-100%	Completed		Construction Completed, soft landscape maintenance period completed and handed over to Council maintenance contractors. Minor Defects complete and TGSI installation complete.
1672. Provide kerb and channel and drainage - both sides of Laurel Street - Forest Street to Whittlesea CAC	2,088	3,000	3,000	76-100%	On Track		Due diligence assessment completed and confirmed, there is no requirement for Cultural Heritage Management Plan. Project is ready for construction when funds are available.

Quarterly New Works Program Status Report for Council: <u>December 2016</u>							PHYSICAL PROGRESS LEGEND
							Traffic Light Definitions
							Favorable
							Completed
							Ahead Of schedule
							On Track
							Not Yet Commenced
							Behind Schedule
							Project Not Proceeding
							On-Hold
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments
1789. Safe Routes to Schools	0	0	25,000	0-25%	On Track	●	Works have been awarded for completion in late December 2016 and January 2017. □ It has been determined that additional funds will be required to be offset from other Transport Engineering accounts.
1809. Urbanise Road - McKimmies Road - Darebin Creek to Garden Grove Drive	750,129	879,881	879,881	76-100%	Completed	●	All construction and rectification works completed, and awaiting some invoices.
1812. Upgrade shared path - Darebin Creek - Metropolitan Ring Road to Findon Road	177,359	590,000	1,356,675	0-25%	Behind Schedule	●	Stages 3-8 works have been delayed due to wet weather making the creek bank inaccessible for construction access. Works to commence in January 2017.
1815. Bundoora Shopping Centre Upgrade	191,987	200,115	200,115	76-100%	On Track	●	Stage 2 - Raised pedestrian crossing Construction works complete □ - Additional Landscaping planned for early 2017 □ Stage 3 - Award of Design Consultant for Dennison Mall currently in progress. □ - Construction expected to commence in May, 2017
1841. Thomastown-Lalor Masterplan Streetscape Improvements	8,005	8,050	64,544	0-25%	On Track	●	In the planning Stages - finalising Project Working Group. □ □ Finalising Consultancy Brief for Urban Designer to plan and design improvements at Thomastown
1867. Footpath Widening north side of Henderson's Road Bridge	0	65,837	65,837	76-100%	Completed	●	Works complete and infrastructure is in DLP
1875. Construct Road - Regent Street - Sackville Street to Grafton Street	8,100	30,000	400,000	0-25%	Behind Schedule	●	Design is in progress for completion by February 2016.
1896. Retail Activity Centres - Streetscape Condition Audit & Improvements - Various Locations	18,800	100,000	100,000	0-25%	On Track	●	Project is scheduled for completion at end of 16/17 financial year.
1951. Construct shared path - Bush Boulevard - Plenty Road to Westfield Shopping Centre	815	15,000	20,000	26-50%	On Track	●	Feature survey and 2D Detailed Design have been completed and circulated for internal consultation. 3D Detail design is in progress.
1956. Construct shared path - McDonalds Road (north side) - Darebin Creek to Civic Drive	0	20,000	35,000	26-50%	On Track	●	The Council is assessing the best fit horizontal alignment as it's highly subject to the future development of McDonalds Road. VicRoads opinion is this regard is awaited.
1959. Construct shared path - Yan Yean Pipe Track - Gordons Road to Hawkstowe Parade	88,720	125,000	125,000	76-100%	On Track	●	Works 95% complete and awaiting top dressing of batters adjacent to the path and signage installation for completion. Invoicing to be finalised.
1968. Construction of 2 lane undivided road and roundabout - Brush Road	5,603	50,000	850,000	0-25%	Not Yet Commenced	●	Tender has been awarded to Metro Asphalt. Contract document is being prepared. Works to commence in mid February 2017.
1992. Construct shared path - Plenty Road (west side) - McKimmies Drive to Weathilland Drive	2,900	30,000	324,500	0-25%	On-Hold	●	Design in progress. Awaiting confirmation of VicRoads business case for Plenty Road upgrade before proceeding with works.
2072. Streetscape improvements program - Lalor High Street Shops (Thomastown and Lalor Masterplan)	0	10,000	316,480	0-25%	On-Hold	●	Site Improvement Plan presented to Council in July 2016 for the urban streetscape improvement of the Lalor Shopping Centre. Other associated projects were identified by Council (such as provision of a toilet amenity, traffic congestion and safety on Station Street), progress of the Community Hub are being further investigated. The total project cost is \$2M with \$1M funded from the State Government Growing Suburbs Fund. The current budget is to undertake design.





Quarterly New Works Program Status Report for Council: <u>December 2016</u>							<b>PHYSICAL PROGRESS LEGEND</b> <b>Traffic Light Definitions</b> Favorable  Completed Favorable  Ahead Of schedule Favorable  On Track Neutral  Not Yet Commenced Neutral  Behind Schedule Unfavorable  Project Not Proceeding Unfavorable  On-Hold
Project Name	YTD Actuals	YTD Budgets	Annual Budget	% Complete	Status	Physical Status	Reporting Comments
2166. Additional Parking - Lalor	0	10,000	107,000	0-25%	On Track		Project design and community consultation have been completed. <input type="checkbox"/> Works have been awarded to a Council annual supply contractor with delivery scheduled for February 2017. <input type="checkbox"/> <input type="checkbox"/> Note that 15 bays are to be installed in William Street between Ash Street and Lorne Street.
2168. Black Length - Glenburnie Road	0	0	0	0-25%	On Track		Works to be undertaken in early 2017. Design and community consultation are complete.
<b>Total ROADS &amp; PATHS</b>	<b>5,054,367</b>	<b>6,819,074</b>	<b>14,034,473</b>				
<b>TRANSPORT</b>							
304. LATM Schemes various treatments	106,878	80,245	400,000	0-25%	Behind Schedule		Community consultation of the proposed TMP has been completed, with report to be prepared for Council at its meeting in February 2017. Subject to Council endorsement, outstanding works expected to be awarded for construction, with commencement in March 2017.
439. Improve disability access (DDA) to public transport	55,000	30,000	60,000	51-75%	On Track		Construction complete by Annual Supply Contractors, with some minor rectification works scheduled for January 2017. Invoicing to be completed in February 2017.
443. Traffic control devices - un-programmed works	13,363	175,000	235,000	26-50%	On Track		Gillwell Road - Alternative treatment selected with Annual Supply Contractors commencing January 2017. Works are scheduled for completion by February 2017, with invoicing to follow. Jardier Terrace - Construction to be completed by January 2017, with invoicing in February 2017.
445. Collector Road traffic management - Various locations	80,387	70,000	140,000	26-50%	On Track		Consultation, investigation and treatment recommendation due in February 2017. Report to prepared for Council at its meeting in March 2017. Invoices anticipated for March 2017.
446. Installation bus shelters	9,500	10,000	20,000	51-75%	On Track		4 of 9 sites completed, with balance to be constructed in January 2017. Invoicing anticipated in February 2017.
1448. Roadside hazard protection	114,165	0	75,000	76-100%	Completed		Roadside Hazard Protection upgrades on Arthus Creek Road complete. 2016/17 Program of Works complete.
1986. Roundabout Modification - Centenary Drive/Bradley Drive	4,531	167,864	167,864	0-25%	Behind Schedule		The approval for public lighting plans from Ausnet were received on 15 Nov 2016. Tenders for construction were called among Panel Contractors (under Head Contract 2015-21) on 22 November 2016 and closed on 14 Dec 2016. The tender evaluation for the construction works in progress.
1987. Modify Roundabout - Centenary Drive and Hinkler Drive	4,298	200,000	200,000	0-25%	Behind Schedule		The approval for public lighting plans from Ausnet were received on 15 Nov 2016. Tenders for construction were called among Panel Contractors (under Head Contract 2015-21) on 22 November 2016 and closed on 14 Dec 2016. The tender evaluation for the construction works in progress.



**2016/17 New Works Program Second Quarter Project Progress Report**

**ASHLEY PARK COMMUNITY ACTIVITY CENTRE- MERNDA**





ANIMAL WELFARE FACILITY – EPPING







HAWKESTOWE PARADE EARLY LEARNING CENTRE – MERNDA SOUTH





RGC COOK RESERVE PAVILION – THOMASTOWN



REDEVELOPMENT OF BARRY ROAD COMMUNITY ACTIVITY CENTRE

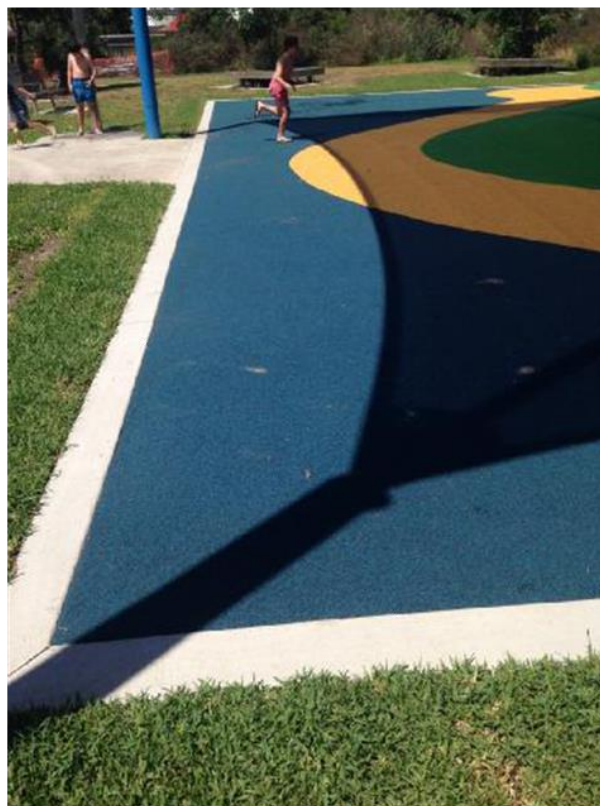


MEADOWGLEN ATHLETICS TRACK- RESURFACING





THOMASTOWN RECREATION & AQUATIC CENTRE (TRAC) OUTDOOR SPACE



CIVIC PRECINCT – HVAC AND REFURBISHMENT (SUSTAINABLE CLIMATE CONTROL PROJECT SATGE1)



LALOR RECREATION TENNIS CLUB UPGRADE - LALOR





ROADSIDE HAZARD PROTECTION – ARTHURS CREEK ROAD



PAINTED HILLS ROAD/BRIDGE INN ROAD – SIGNALISE INTERSECTION



BUNDOORA SHOPPING CENTRE UPGRADE





CONSTRUCT SHARED PATH – YAN YEAN PIPE TRACK



VEHICLE EXCLUSION FENCING





BUTCHER BIRD RESERVE PLAYGROUND







### 2016-2017 Planning and Feasibility Program – Q2 Report

To provide a Q2 report on the Planning and Feasibility Program in the New Works Program.

#### Background

Planning for future infrastructure projects is vital for the accurate scoping and costing of works and efficient delivery on time. The previous budget allocations for this program has enabled the forward planning of many projects resulting in the significant improvement in the delivery performance of the New Works Program and attraction of significant external grant funding.

The project planning undertaken under this budget allocation allows for the conduct of due diligence investigations to ensure the project is in a better position for delivery when budget allocations are made in the forward New Works Program. This often requires:

- Investigative works such as feature and level surveys, geotechnical assessments, condition assessments, assessment of existing utilities, etc.
- Development of concepts or possible solutions.
- Development of schedules for the delivery of the project including a resource plan.
- Cost estimation of the project including quantity estimates.
- Preparation of project briefs and business cases, including risk assessments, stakeholder and user consultation, and options analysis.

Project planning reduces the overall project risk by defining the variable items, discovering latent issues and reducing uncertainties. In turn, confidence in the delivery of the project on time and within budget is optimised.

Good project planning will also enable greater accuracy and confidence in applications for external funding grants and increase in external revenue. This is essential to ensure sustainable investment in Council's infrastructure through the 4/15 Year Long Term Infrastructure Program.

#### 2016/2017 Program Summary

The Planning and Feasibility Program approved by Council in June 2016 identified a total project budget of \$300,000. A report was presented to Forum on 31 May 2016 outlining the detailed program of works for the 2016/17 Planning and Feasibility Program with an updated Program presented to Council as part of the First Quarter New Works Program Report to Council on 22 November 2016.

Recent further refinement of the program has identified the following changes:

Project	Task	Allocation	Comment/Update
Reconstruct courts - Dr Harry Jenkins Reserve, Mill Park - Dr Harry Jenkins Reserve, Mill Park	Undertake site investigations and concept planning (including costings). Prepare business case. Potential grant funding opportunities (e.g.. SRV).	\$30,000	Included in program for 2016/17

Upgrade pavilion - Main Street Reserve Thomastown	Finalisation of business case and detailed design for pavilion upgrade	\$30,000	Included in program for 2016/17
Electrical Services Investigation at Meadowglen International Athletics Stadium to determine compliance demands requirements	Works to include the following: 1. Review of current electrical circuitry and switchboard to determine compliance 2. Engage a consultant to determine exact extent of any works required	\$15,000	Included in program for 2016/17
Epping North Section 173 Review – Agreement with Places Victoria	Places Victoria is required to deliver 22 community facilities of which they have delivered 3 so far. Agreement requires review at least every 5 years to ensure development progress is consistent with that predicted in 2007. Confirm current needs analysis and schedule of facilities to be provided by Places Victoria in the next 5 year period to ensure community facility delivery meets community demand.	(\$10,000)	Reduce budget from \$40,000 to \$30,000
<b>TOTAL</b>		<b>\$65,000</b>	

These have now been included in the updated program (see **Attach listing of projects**).

#### Financial Summary

2016/2017 Budget	2016/17 Expected Expenditure	Expected Variance
\$300,000	\$300,000	\$0

Council allocated \$300,000 funding in 2016/17 budget to undertake planning works for projects identified in the 4 Year New Works Program.

Project expenditure is expected to be within the allocated budget as at the second quarter, however this Program will continue to be monitored as more detailed costings become available and any necessary adjustments will be reported in future reports to Council.

#### Recommendation:

That Council approve the revised 2016/17 Planning and Feasibility Program with a further update to be provided at the end of the third quarter as projects progress and more details become known.

PID 1847 - PLANNING FEASIBILITY PROGRAM FOR 2016/2017 NWP

ID	Project Name	Project Description	Potential External Grant Opportunities	Supporting Information	Planning & Feasibility Program (PID 1847)				Comment
					Q1 Project Budget Allocation	Q2 Proposed Project Budget Allocation	Start	Due for Completion	
HIGH PRIORITY									
484	Wollert Hall Refurbishment - Epping Road Wollert	Wollert Hall - Extension (inc. Public Toilet)	Growing Suburbs Fund (IGF Round 2 - DEWLP - State)	Extension and refurbishment of hall to accommodate additional future use in Wollert growth corridor. The use and popularity of the facility has increased since its management was handed over to Council.	\$ 15,000	\$ 15,000	Q1	Q3	Feasibility works (concept design and scope of works) have been completed by external consultant. RFQ for Design to be developed.
1400 & 1486	Edgars Creek Community-Education Precinct (Aurora)	Master plan and concept design	Community Sports Fund (SRV - State)	Responding to State Government school funding accouchement and opportunity for joint delivery of project. DET has brought forward the school masterplaning process which provides opportunity for integrated ELC. PID 1400 - Construct 2 Football/Cricket Ovals and Pavilion adjacent to P-12 school (near Steen Avenue - Aurora Precinct 3). PID1486 - Construct Tennis Courts and Pavilion adjacent to P-12 school (near Steen Avenue - Aurora Precinct 3).	\$ 20,000	\$ 20,000	Q1	Q4	Preliminary masterplanning works to establish the feasibility of delivering all Council facilities on the available land is scheduled to commence January 2017 with a view to a draft preliminary masterplan by the end of the month. Consultants have been appointed to commence process.
1419	Construct Community Activity Centre - Epping North - adjacent to P-12 school (near Steen Avenue - Aurora Precinct 3)	Epping North - Precinct 3 (Aurora) - Construction of Community Activity Centre	Children's Facilities Program (DET - State)	Community Centre is co-located with the P-12 school site which contains two kindergarten rooms, maternal and child health, meeting rooms and hall. Subject to finalisation of Development Contributions Agreement with Lend Lease and Minister's approval of amendment. DET has brought forward the school masterplaning process which provides opportunity for integrated ELC.	\$ 20,000	\$ 20,000	Q1	Q4	Preliminary masterplanning works to establish the feasibility of delivering all Council facilities on the available land is scheduled to commence January 2017 with a view to a draft preliminary masterplan by the end of the month. Lend lease are responsible for the design and delivery of the CAC. Discussions with Lend Lease about the design work will commence when the preliminary
1462	Duffy Street Pavilion	Upgrade existing Pavilion	Community Sports Fund (SRV - State)	Upgrade to the existing pavilion to meet the needs of the growing resident soccer club. Club is expanding and attracting girls to the club. Upgrade would include remodelling the internal layout to provide for 2 change facilities, additional storage, improving the viewing capacity to the ground, providing an appropriate area for umpires change facilities and making the pavilion accessible. Project has been identified in the draft Sports Pavilion Strategy.	\$ 35,000	\$ 35,000	Q2	Q3	Condition audit currently being undertaken by consultant. Draft RFQ for concept design has been prepared for internal consultation with a view to appointing consultant before the end of January. Condition audit and concept plan will inform the Business Case options and evaluation, and in turn the future funding allocation(s) for this project. There is currently\$300k allocated in 2017/18.
1996	Extension to Laurimar Community Centre	Concept design work for planned extension	Growing Suburbs Fund (IGF Round 2 - DEWLP - State)	Construction of Laurimar CAC Hall. Site investigation commenced in 2015/16.	\$ 25,000	\$ 25,000	Q2	Q4	Draft RFQ for concept design has been prepared for internal consultation with a view to appointing a consultant in February.
1999	Install stormwater harvesting - Melbourne Markets (Stage 4)	Melbourne Markets Stormwater Harvesting Project - Extension to HR Uren Reserve	National Stronger Regions Fund (Dep of Inf - Federal)	As part of the Melbourne Markets Stormwater Harvesting Concept Study and option for the extension of the recycled water main to HR Uren sports facility was considered. Appropriate equipment and functional design elements were provided in the initial design and construction of the Stage 2 and 3 of the project.	\$ 30,000	\$ 30,000	Q2	Q4	Concept planning and cost estimate work currently underway to ascertain viability of project.
TBA	Epping North Section 173 (2007) Review - Agreement with Places Victoria	Epping North - (Aurora) -	Priority projects likely to be community centres, pavilions and ovals	Places Victoria is required to deliver 22 community facilities of which they have only delivered 3 so far. Agreement requires review at least every 5 years to ensure development progress is consistent with that predicted in 2007. Confirm current needs analysis and schedule of facilities to be provided by Places Victoria in the next 5 year period to ensure community facility delivery meets community demand. Linked to review of Section 173 Epping North agreement with Places Victoria..	\$ 40,000	\$ 30,000	Q2	Q4	Project Brief completed.  RFQ has been prepared for internal consultation with a view to appointing a consultant in February.
Var	Mernda Town Centre	Planning for Mernda Town Centre	TBA. Likely to be Growing Suburbs Fund (IGF Round 2 - DEWLP - State) and Growing Suburbs Fund (IGF Round 2 - DEWLP - State)	Commencement of Masteplans and urban design concepts for: PID 669 - Youth Facility PID 1408 - Living & Learning Centre PID 1409 - Performing Arts Space PID 1410 - Outreach Centre PID 1411 - Maternal Child Health Centre PID 1454 - Library	\$ 20,000	\$ 20,000	Q2	Q4	Site Massing design work has commenced  Business Case to commence once site massing and heights work is completed. Commencement expected start February 2017.
Var	Project Brief and Business Case developments	Municipal wide	TBA. Likely to be Growing Suburbs Fund (IGF Round 2 - DEWLP - State) and Growing Suburbs Fund (IGF Round 2 - DEWLP - State)	Other priority project investigations, Project Briefs and Business Cases for: - PID 1291 - Business Case - Regional All Abilities Playspace - including Economic Study - Lalor Community Hub-May Rd - Condition Report and Business Case - Mill Park Library Refurbishment - Engage consultant to inform Grant application - Mernda Recreation Reserve Future Plan	\$ 30,000	\$ 30,000	Q1	Q3	Works on identified projects has commenced
TBA	BALANCE	Project allocation to be advised by the New Works Program Board and reported to Council. See Project list below			\$ 65,000	\$ -			See Project list below
259	Reconstruct courts - Dr Harry Jenkins Reserve, Mill Park - Dr Harry Jenkins Reserve, Mill Park	Harry Jenkins Reserve - Reconstruct courts	Community Sports Fund (SRV - State)	The tennis courts are to be reconstructed in stages with a concrete base and sand-filled artificial grass surface as per the Tennis Facility Strategy. Project continues beyond Year 4.	\$ -	\$ 30,000	Q3	Q4	Draft RFQ has been prepared for internal consultation with a view to appointing consultant in February. Appointment of consultant is subject to Council endorsement of the inclusion of this work in the Planning and Feasibility Program.



## PID 1847 - PLANNING FEASIBILITY PROGRAM FOR 2016/2017 NWP

ID	Project Name	Project Description	Potential External Grant Opportunities	Supporting Information	Planning & Feasibility Program (PID 1847)				Comment
					Q1 Project Budget Allocation	Q2 Proposed Project Budget Allocation	Start	Due for Completion	
1698	Upgrade pavilion - Main Street Reserve Thomastown	Detailed design work for planned extension	Community Sports Fund (SRV - State)	Finalise Business Case for redevelopment and commence detailed design for Main Street Reserve Pavilion upgrade	\$ -	\$ 30,000	Q2	Q4	Business Case details three options. Memo circulated to Councillors in August 2016 requested feedback re: preferred option. No feedback was received. Recommendation to proceed with Option 3. Concept designs to be developed.
2191	Electrical Services Investigation at Meadowglen International Athletics Stadium to determine compliance demands requirements	Undertake investigation works to ascertain the electrical compliance of the switchboard and circuitry to meet the needs of the facility.	Nil	Works to include the following: 1. Review of current electrical circuitry and switchboard to determine compliance 2. Engage a consultant to determine exact extent of any works required.	\$ -	\$ 15,000	Q3	Q4	Indications from initial inspections are that the electrical circuitry and switchboard are non compliant and an OH&S issue. Works are to engage a consultant to determine the exact extent of the works required.
					\$ 300,000	\$ 300,000			

18/01/2017

2016/2017 Q2 New Works Program Grants Status Report

Project ID	Project Name	Grant Body	Grant Name	Grant Type	Funding Requested / Granted
<b>Application in Progress</b>					
1142	TH Hurrey - Upgrade of tennis courts and pavilion	Department of Infrastructure and Regional Development	Building Better Regions Fund - Infrastructure Project Stream	Federal	\$ 1,800,000.00
2072/2077	Thomastown and Lalor Urban Upgrade Streetscape Projects	VicRoads	Safe System Road Infrastructure Program	State	\$ 500,000.00
<b>Application Pending Notification</b>					
N/A	Mill Park Library Refurbishment <i>*Project is contingent on receiving grant funding</i>	Department of Environment, Land, Water & Planning	Living Libraries Infrastructure Program 2016 - <i>Expression of Interest</i>	State	\$ 660,000.00
2057	Peter Lalor Secondary College Redevelopment	Department of Premier & Cabinet	Community Infrastructure and Cultural Precincts Program 2016-17	State	\$ 600,000.00
N/A	Laurimar Primary School Outdoor Netball Court Lighting and Cover <i>*Project is contingent on receiving grant funding</i>	Department of Education & Training	Shared Facilities Fund - <i>Expression of Interest</i>	State	\$ 225,000.00
N/A	Edgars Creek Education Community Precinct - Indoor Sports Hall <i>*Project is contingent on receiving grant funding</i>	Department of Education & Training	Shared Facilities Fund - <i>Expression of Interest</i>	State	\$ 2,000,000.00
N/A	Edgars Creek Education Community Precinct - Sports Oval <i>*Project is contingent on receiving grant funding</i>	Department of Education & Training	Shared Facilities Fund - <i>Expression of Interest</i>	State	\$ 4,000,000.00
2057	Peter Lalor Secondary College Redevelopment	Department of Education & Training	Shared Facilities Fund - <i>Expression of Interest</i>	State	\$ 850,000.00
2072	Urban Upgrade Streetscape Portfolio - Lalor	Department of Infrastructure and Regional Development	Black Spot Programme	Federal	\$ 530,000.00
<b>Successful Applications</b>					
1142	Yan Yean Community Tennis Club Court Development	Sport and Recreation Victoria	Community Sports Infrastructure Fund 2017/2018 Major Facilities	State	\$ 100,000.00
696	Mosaic Recreation Reserve and Community Soccer Facility	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 2,950,000.00
703	Painted Hills Recreation Reserve and Community Soccer Centre Development	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 3,275,000.00
1701	Janefield Community Centre Refurbishment	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 310,000.00
118	Oakbank Boulevard Playground and Township Link	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 220,000.00
118	Playground Renewal Program, Statesman Crescent Park and Hampstead Court Park	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 167,000.00
1914	Community Facilities Solar Photovoltaic Roll-out	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 200,000.00





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## 2016/2017 Q2 New Works Program Grants Status Report

Project ID	Project Name	Grant Body	Grant Name	Grant Type	Funding Requested / Granted
2072	Urban Upgrade Streetscape Portfolio - Lalor - Planning component of project not funded	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 880,000.00
1815	Urban Upgrade Streetscape Portfolio - Bundoora (1815) - Planning component of project not funded	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 125,000.00
703	Painted Hills Recreation Reserve and Community Soccer Fields	Sport and Recreation Victoria	Community Sports Infrastructure Fund 2017/2018 Minor Facilities	State	\$ 650,000.00
2150	West Lalor Tennis Clubroom Access Upgrade	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 2	Federal	\$ 10,000.00
2149	Main Street Reserve Electronic Scoreboard	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 2	Federal	\$ 15,000.00
2168	Road Safety Works, Glenburnie Road, Whittlesea	Department of Infrastructure and Regional Development	Black Spot Programme	Federal	\$ 880,000.00
1010	Barry Road Community Activity Centre	Department of Environment, Land, Water & Planning	2015/2016 Interface Growth Fund	State	\$ 2,600,000.00
2056	Epping Services Hub	Department of Environment, Land, Water & Planning	2015/2016 Interface Growth Fund	State	\$ 781,000.00
1627	Lalor Tennis Club Rec Reserve	Department of Environment, Land, Water & Planning	2015/2016 Interface Growth Fund	State	\$ 1,250,000.00
1899	Mill Park Lakes Community Pavilion	Department of Environment, Land, Water & Planning	2015/2016 Interface Growth Fund	State	\$ 1,118,750.00
118	Playground Improvement Program	Department of Environment, Land, Water & Planning	2015/2016 Interface Growth Fund	State	\$ 260,000.00
2123	Thomastown Library Refurbishment	Department of Environment, Land, Water & Planning	Living Libraries Infrastructure Program	State	\$ 150,000.00
118	Main Street Reserve Play Space Redevelopment	Department of Health & Human Services	Community Sports Infrastructure Fund 2016/2017	State	\$ 100,000.00
1847	Memda Aquatic and Indoor Centre Feasibility Study	Department of Health & Human Services	Community Sports Infrastructure Fund 2016/2017	State	\$ 30,000.00
1923	Waterview Scoreboard - Woodland Waters Sportsfield Development	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 1	Federal	\$ 20,000.00
1627	Lalor Tennis Club Social Space Development	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 1	Federal	\$ 15,000.00
1113	Main Street Thomastown Cycling Link	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 1	Federal	\$ 8,000.00
304	Findon Primary School Bicycle Links	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 1	Federal	\$ 19,000.00
1923	Epping Tennis Club Kitchen Upgrade	Department of Infrastructure and Regional Development	Stronger Communities Program - Round 1	Federal	\$ 20,000.00
2146	A.F Walker Reserve Sportsfield Lighting	Department of Health & Human Services	Sport and Recreation Victoria - Country Football and Netball Program	State	\$ 100,000.00

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2016/2017 Q2 New Works Program Grants Status Report

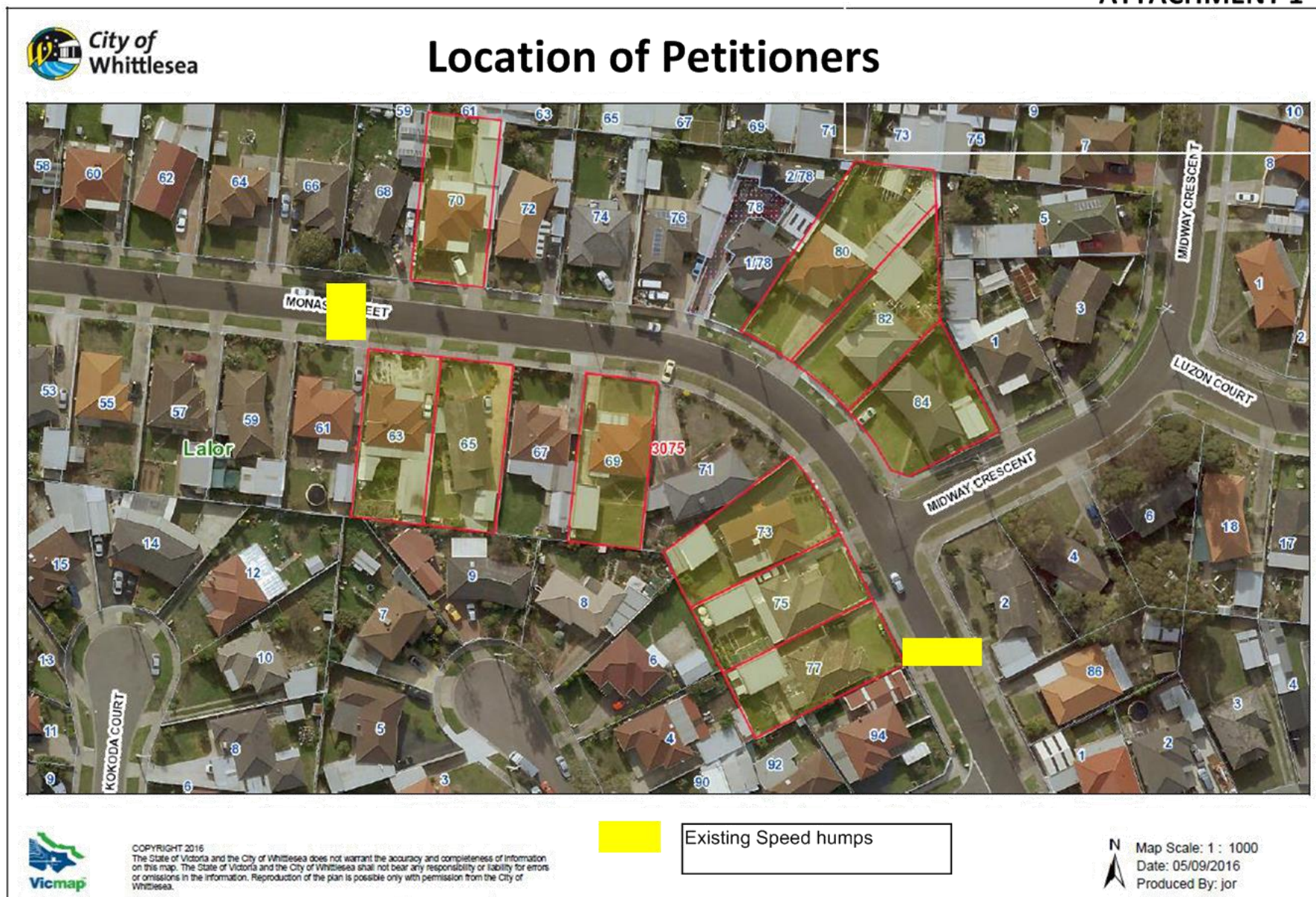
Project ID	Project Name	Grant Body	Grant Name	Grant Type	Funding Requested / Granted
Unsuccessful Applications					
1419	Edgars Creek Children's Community Centre	Department of Education & Training	2016-17 Integrated Children's Centre Grant * Funding from Redgum Kindergarten will be redirected to this project	State	\$ 1,600,000.00
1917	Korin Korin Children & Family Centre	Department of Health & Human Services	Community Shade Grants Program - Round 2 - <i>Expression of Interest</i>	State	\$ 8,000.00
1717	Whittlesea Dog Off Leash Park	Department of Health & Human Services	Community Shade Grants Program - Round 2 - <i>Expression of Interest</i>	State	\$ 20,000.00
1627	Lalor Tennis Club	Department of Health & Human Services	Community Shade Grants Program - Round 2 - <i>Expression of Interest</i>	State	\$ 20,000.00
484	Wollert Community Centre Refurbishment <b>*Project is contingent on receiving grant funding</b>	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 250,000.00
TBA	Findon Road Path Link and Safe Pedestrian Crossing	Transport Accident Commission	Local Government Grants Program	State	\$ 78,925.00
2077	Urban Upgrade Streetscape Portfolio - Thomastown - Planning Projects not funded	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 100,000.00
1142	Yan Yean Community Tennis Pavilion Development	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 1,800,000.00
1324	Harvest Home Road Community Pavilion Development	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 2,500,000.00
1812	Darebin Creek Cultural Heritage Trail	Department of Environment, Land, Water & Planning	2016/2017 Growing Suburbs Fund	State	\$ 1,121,000.00
1010	Barry Road Community Activity Centre	Department of Justice & Regulation	2016/2017 Public Safety Infrastructure Fund	State	\$ 250,000.00
1627	Lalor Tennis Club Social Space Development	Department of Premier & Cabinet	Community Infrastructure and Cultural Precincts Program 2016-17 - EOI	State	\$ 141,000.00
1701	Janefield Community Centre Refurbishment	Department of Premier & Cabinet	Community Infrastructure and Cultural Precincts Program 2016-17 - EOI	State	\$ 200,000.00
2119	Streetlight Bulk Replacement Program - Stage 2	Department of Transport, Planning and Local Infrastructure	Collaborative Council - Sustainability Fund Partnerships Program	State	\$ 75,000.00
2118	Findon Road Extension, South Morang	Department of Infrastructure & Regional Development	National Stronger Regions Fund - Round 3	Federal	\$ 5,216,471.00







ATTACHMENT 1







ATTACHMENT 2

## Existing Speed Humps: Monash Street, Lalor







**ATTACHMENT 1 – SITE PLAN & PHOTO**



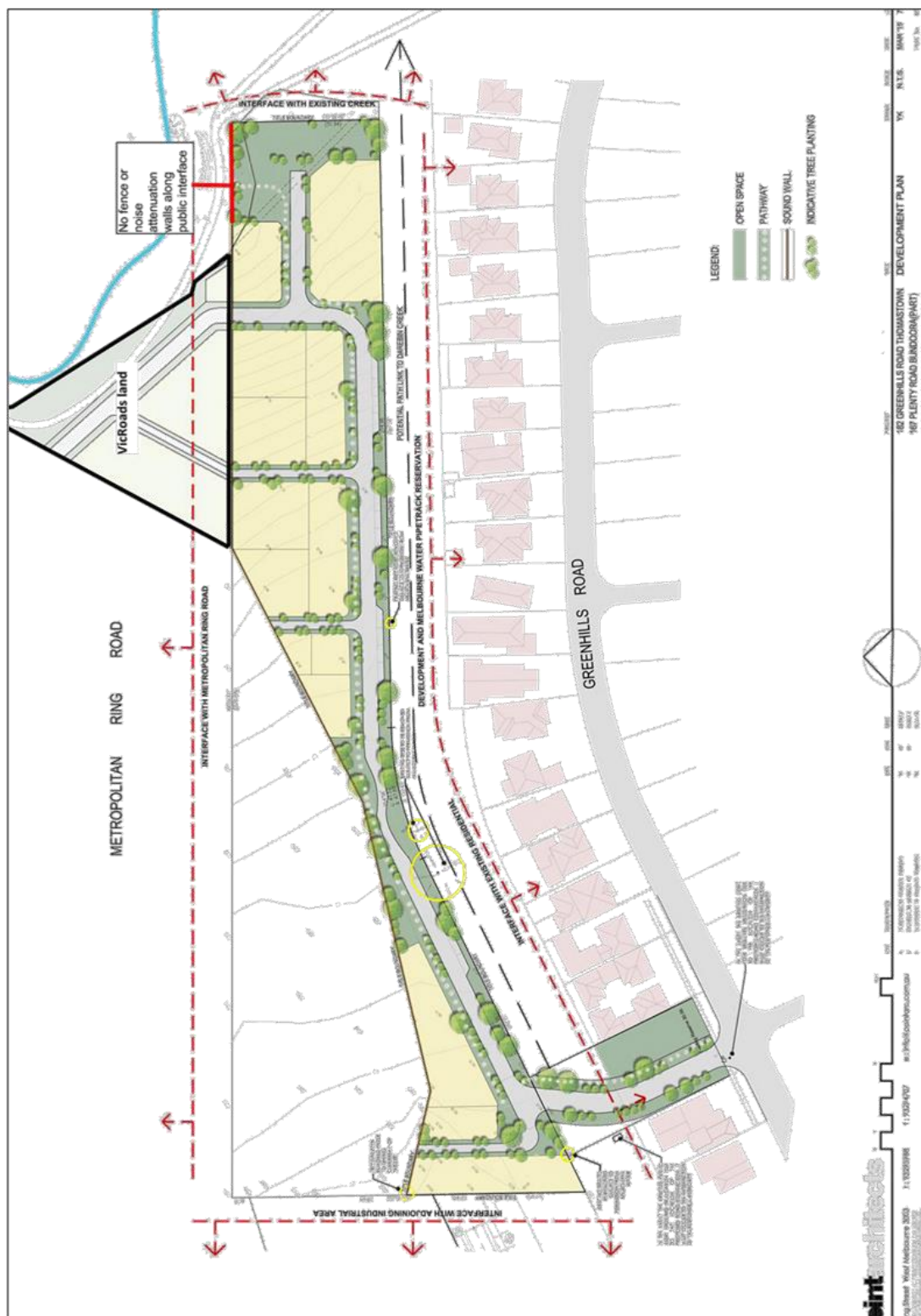
**Council land – 214W Greenhills Rd  
Bundoora**

**Development site – 182 Greenhills Rd  
Bundoora**













**ATTACHMENT 1 – SITE PLAN & PHOTO**



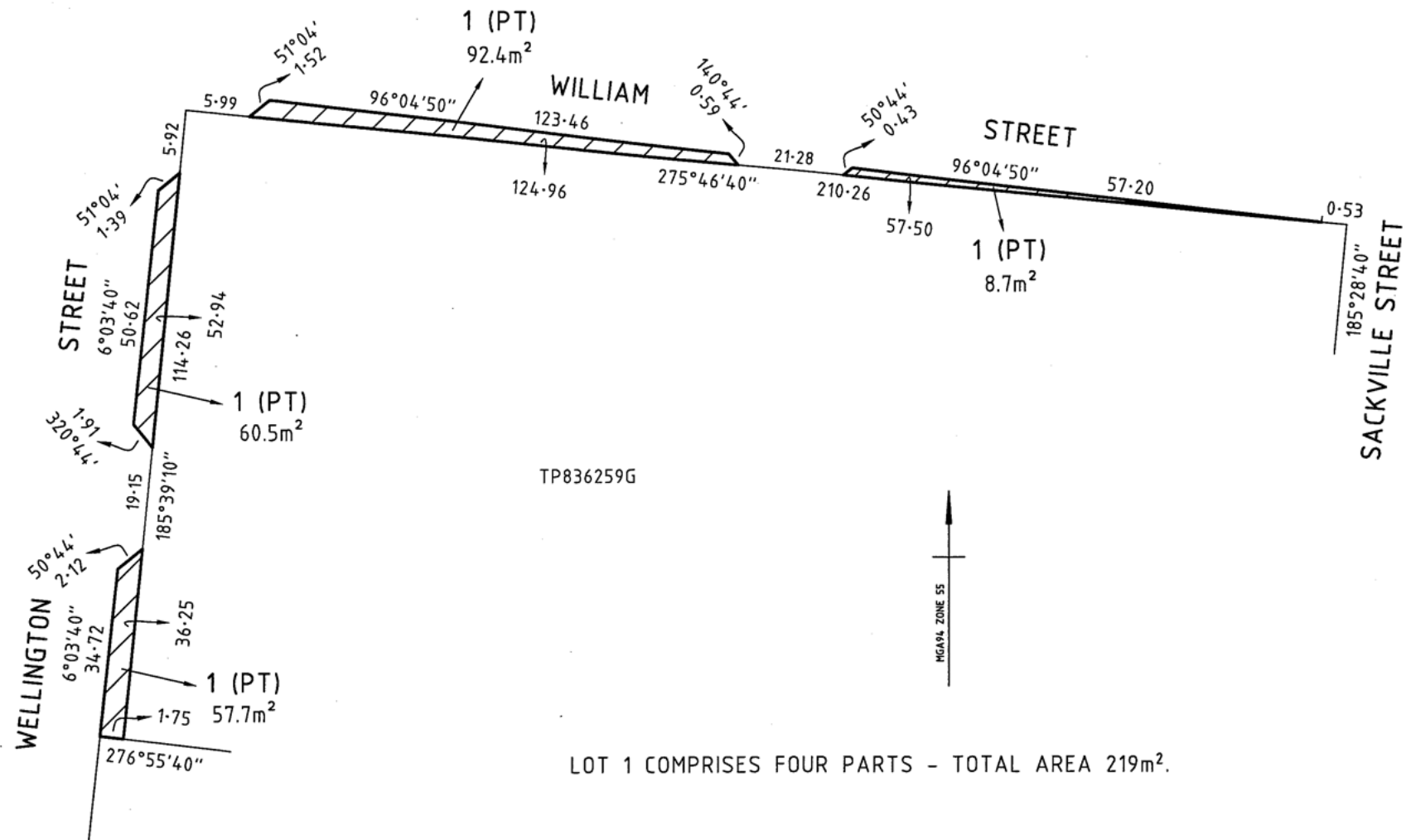
**Roads to be discontinued and transferred to  
60 Sackville Street Mernda**

**Land to be acquired by  
Council**









LOT 1 COMPRISES FOUR PARTS - TOTAL AREA 219m<sup>2</sup>.







Unconfirmed Audit & Risk Committee  
Minutes  
5pm Thursday  
24 November 2016

**Location:** Council Chamber, Civic Centre

	Present	Apology
<b>Members:</b>		
<b>Independents</b>		
Michael Said, Chairperson	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Homi Burjorjee	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Michael Ulbrick	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>Councillors</b>		
Cr Stevan Kozmevski	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Cr Ricky Kirkham, Mayor	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>Officers:</b>		
Michael Wootten, Chief Executive Officer	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Helen Sui, Director Corporate Services	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Amy Montalti, Manager Finance & Assets	<input checked="" type="checkbox"/>	<input type="checkbox"/>
David Gauci, Internal Compliance Officer	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Nick Mann, Director City Transport & Presentation (Item 9.2)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Liana Thompson, Director Partnerships & Engagement (Item 12.2)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>In attendance:</b>		
Pat Farrell, Partner, PWC	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Natalie Mu, Manager, PWC	<input checked="" type="checkbox"/>	<input type="checkbox"/>





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Thursday 24 November 2016



**City of  
Whittlesea**

## **Matters in Discussion**

### **1. DISCLOSURE OF CONFLICTS OF INTEREST**

No declarations were made.

### **2. CONFIRMATION OF MINUTES OF PREVIOUS MEETINGS**

Resolution:

**That the minutes of the Audit & Risk Committee meeting held on 25 August 2016 be noted and confirmed.**

Moved by: Cr Kozmevski

Seconded by: Mr Burjorjee

CARRIED

Resolution:

**That the minutes of the Audit & Risk Committee meeting held on 1 September 2016 be noted and confirmed.**

Moved by: Mr Ulbrick

Seconded by: Cr Kozmevski

CARRIED

### **3. MATTERS ARISING FROM PREVIOUS MEETING(S)**

The Outstanding Items Report from Previous Meetings was presented for discussion. The Committee Chairperson noted that the Victorian Auditor General (VAGO) had recently released a report – *Financial Systems Controls Report: 2015-16*, in which five Councils are referenced, as well as information regarding Cyber Security. The Committee requested this report be referred to the Information Services Department for consideration.

Resolution:

**That the report of Outstanding Action Items from Previous Meetings be received and noted.**

Moved by: Mr Burjorjee

Seconded by: Mr Ulbrick

CARRIED

### **4. AUDIT & RISK COMMITTEE WORK PLAN**

The Annual Work Plan Matrix was presented for general discussion. The Committee Chairperson gave an overview of the process undertaken to develop the Work Plan for the 2016-17 year.

The Committee briefly discussed its expectations in relation to what it would like to receive regarding certain items listed on the Work Plan. In particular, when a policy is presented, it should be accompanied by a statement of compliance / non compliance.

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**City of  
Whittlesea**

Discussion concluded with the Committee noting that VAGO now has increased powers which allows it to review the operations of a third party (i.e. contractors). It was noted some Councils have alerted their contractors to this increased power, whilst others have amended their Procurement Policies and Procedures.

Resolution:

**That the Annual Work Plan Matrix be noted.**

Moved by: Mr Ulbrick

Seconded by: Cr Kozmevski

CARRIED

**5. CEO'S REPORT ON COMPLIANCE/NON-COMPLIANCE WITH LEGISLATION AND POLICIES INCLUDING TENDERING AND PROCUREMENT**

The CEO advised that there were no issues of compliance/non-compliance to report.

Resolution:

**That the CEO's verbal report on Compliance/Non-Compliance with Laws/Legislation and Policies be noted.**

Moved by: Mr Burjorjee

Seconded by: Mr Ulbrick

CARRIED

**6. FINANCIAL REPORT**

**ITEM 6.1 Financial Report**

A copy of the Financial Performance Report for the period ended 30 September 2016 was circulated to Committee members.

The Director Corporate Services advised the Committee that Amy Montalti had been appointed to the position of Manager Finance & Assets.

An update on the progress of the Capital Works Program against budget was provided with officers responding to questions from the Committee.

The Committee also discussed the difference in definitions provided in the Model Financial Report versus those included in the Local Government Performance Reporting Framework and the need for greater consistency with definitions. The Committee felt that the Local Government industry should be working as a whole to advocate greater consistency. The Manager Finance & Assets advised she would raise this issue with FinPro.

Resolution:

**That the Financial Report be noted.**

Moved by: Cr Kozmevski

Seconded by: Mr Burjorjee

CARRIED

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## 7. INTERNAL CONTROL

Refer Item 11.1

## 8. RISK MANAGEMENT

No items for this meeting.

## 9. INTERNAL AUDIT

### ITEM 9.1 Internal Audit Status Report

PwC presented the Internal Audit Status Report and responded to questions from the Committee. The Committee noted the '*Internal Audit Hot Topic – Data Assurance – Big Data (Impacts and Benefits)?*' and briefly discussed the performance results received in relation to the Better Practice for Procurement review.

#### Resolution:

**That the Internal Audit Status report be received and noted.**

Moved by: Mr Ulbrick  
Seconded by: Cr Kozmevski

CARRIED

### ITEM 9.2 Internal Audit Reviews

- Verification of Vested Assets

PwC presented the Verification of Vested Assets report and gave an overview of the findings. Officers responded to the report and questions from the Committee.

The Committee noted the timelines associated with each of the proposed management actions and their appropriateness. The Committee also encouraged management to continue utilising its current practices whilst management actions are being implemented.

#### Resolution:

**That the Verification of Vested Assets internal audit report be received and noted.**

Moved by: Mr Ulbrick  
Seconded by: Cr Kozmevski

CARRIED

### ITEM 9.3 Outstanding Action Items From Internal Audit Reports

The Outstanding Action Items Report from Previous Internal Audits was presented



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for discussion.

Resolution:

**That the report of Outstanding Action Items from Previous Internal Audit Reports be received and noted.**

Moved by: Mr Burjorjee

Seconded by: Mr Ulbrick

CARRIED

**ITEM 9.4 Review of Internal Auditor's Performance**

The performance of the Internal Auditor was discussed.

The Committee also discussed the performance of the external auditor. The Committee asked if management had received any request from VAGO to provide an assessment of the 2015-16 external audit process. Management advised no request had been received.

General discussion ensued regarding the recent external audit. Concerns were raised in relation to the timeliness of the auditor. These issues will be discussed with the auditor at the Committee meeting to be held on 2 March 2017.

Resolution:

**That the performance of the Internal and External Auditors be noted.**

Moved by: Cr Kozmevski

Seconded by: Mr Burjorjee

CARRIED

**10. EXTERNAL AUDIT**

No items for this meeting.

**11. COMPLIANCE**

**ITEM 11.1 Internal Compliance Reviews**

An update on the status of internal compliance reviews undertaken since the last Committee meeting was provided with officers responding to questions from the Committee.

The Committee discussed the outcomes from the Purchase Card review and requested that the updated Purchase Card Policy be provided to the Committee once completed.

Resolution:

**That:**

- 1. The report on the status of Internal Compliance Reviews be received and noted.**

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Whittlesea**

2. The updated Purchase Card Policy be provided to the Committee once completed.

Moved by: Mr Ulbrick  
Seconded by: Cr Kozmevski

CARRIED

## 12. OTHER RESPONSIBILITIES

### ITEM 12.1 Update on Significant Legal Matters

The Director Corporate Services noted that there were no significant issues to discuss.

Resolution:

**That the update of significant legal matters by the Director Corporate Services be noted.**

Moved by: Mr Burjorjee  
Seconded by: Mr Ulbrick

CARRIED

### ITEM 12.2 Service Planning & Review Program

An Update on the Service Planning & Review Program was provided. Management advised the Committee that Nous Group had been engaged to undertake a strategic review of services that are core, locally critical and their efficiency and effectiveness. The Committee requested it be provided with a copy of the outcomes from this review at a future meeting.

Resolution:

**That:**

- 1. The Committee note the update on the Service Planning & Review Program and the complementary work conducted externally.**
- 2. The Committee be provided with the results of the Strategic Review being undertaken by Nous Group by their 25 May 2017 meeting.**

Moved by: Mr Burjorjee  
Seconded by: Mr Ulbrick

CARRIED

### ITEM 12.3 Response to VAGO Report – Audit Committee Governance

Management's response to the VAGO Report – Audit Committee Governance was provided.

Resolution:

**That the Committee note the VAGO Audit Summary Report: Audit Committee Governance and management's response to the recommendations included in the report.**

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Whittlesea**

Moved by: Cr Kozmevski  
Seconded by: Mr Burjorjee

CARRIED

**ITEM 12.4 Other Audits**

An update on the status of recent audits undertaken by external parties was provided. Audit opinions were provided for the:

- Roads to Recovery Program.
- Inclusion and Professional Support Program.

Resolution:

**That the Committee note the Audited Financial Acquittal Report for the Roads to Recovery and Inclusion and Professional Support Program.**

Moved by: Mr Ulbrick  
Seconded by: Cr Kozmevski

CARRIED

**ITEM 12.5 Review of Audit & Risk Committee's Performance**

A summary of results from the Committee members' responses to the Self-Assessment questionnaire were presented for discussion.

Resolution:

**That the Committee note the summary of the Audit & Risk Committee's Self-Assessment Performance results including comments provided by members.**

Moved by: Mr Burjorjee  
Seconded by: Mr Ulbrick

CARRIED

**13. CORRESPONDENCE**

Nil

**14. GENERAL BUSINESS ITEMS**

Nil

**15. CONFIRMATION OF DATE OF NEXT MEETING:**

- 2 March 2017
- 25 May 2017
- 24 August 2017 (*Financial Statements Review*)
- 31 August 2017
- 23 November 2017

Meeting Closed: 6.50pm



